

City Plan Commission Briefing #2

August 2, 2016



Image Source – City of Richardson



MAIN STREET / CENTRAL - REZONING INITIATIVE | PHASE III SOUTH END

Agenda

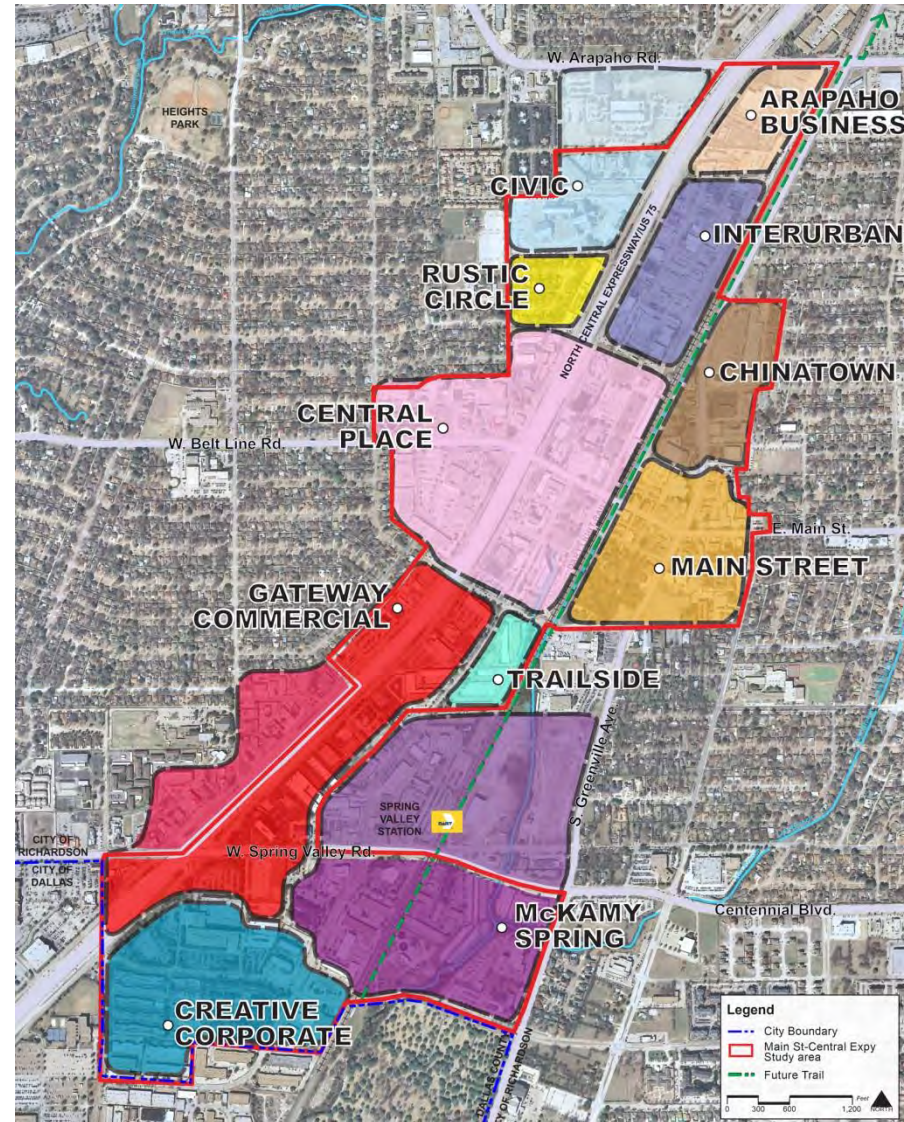
- Project Overview / Progress to Date
- Property and Business Owner Meeting Series 2
- Community Workshop 2
- Belt Line Road / Main Street – Alternative Cross-Section
- Update – Information Requested at 6/20 CPC / City Council Joint Session
- Next Steps



Project Overview / Progress to Date

Main Street / Central Expressway Project

- **Phase I** created a vision for the study area and for each of 11 sub-districts based on community goals and market realities (2013)
- **Phase II** created a Form Based code for the 4 Central Sub-districts (2015)
- **Phase III** focuses on the 4 South End Sub-districts and will create a Form Based Code for these areas (2016); includes refinements to the existing Code in the Central Sub-districts



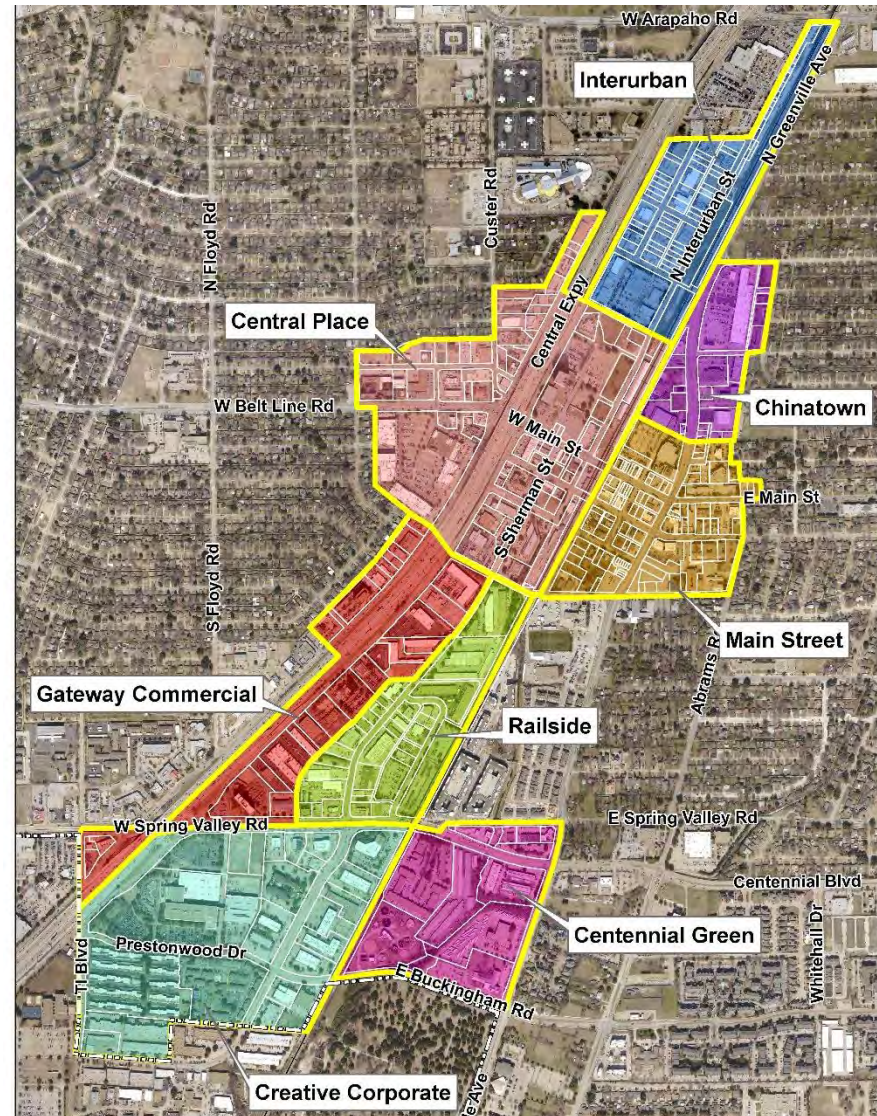
Phase III – Progress to Date

- Community and Stakeholder Involvement, Round 1
 - Property and business owner meetings on March 2 and 3, 2016 – 62 participants
 - Community Workshop on March 29, 2016 – 65 participants
 - Online Survey during April 2016 – 295 responses
- City Council (CC) and City Plan Commission (CPC) Involvement and Direction
 - Briefings - January 4 and 19, 2016
 - Tours - April 30, 2016 and May 17, 2016
 - Work Sessions - May 2 and 3, 2016
 - CC/CPC Joint Work Session - June 20, 2016
- Community and Stakeholder Involvement, Round 2
 - Property and business owner meetings on July 11 and 12, 2016 – 27 participants
 - Community Workshop on July 12, 2016 – 35 participants



Sub-districts for July Discussions

- **South End Sub-districts (New Code)**
 - Gateway Commercial
 - Creative Corporate
 - Centennial Green
 - Railside
- **Central Sub-districts (Refinements to Existing Code)**
 - Main Street
 - Central Place
 - Chinatown
 - Interurban



**Property and Business
Owner Meetings Series 2**

Property and Business Owner Meetings Series 2 - Overview

- Meetings were held with property and business owners from each of the southern sub-districts and from the central sub-districts on July 11 and 12, 2016
- Two opportunities were provided for property and business owners from each sub-district to attend
- These meetings accommodated feedback from 27 participants



Property and Business Owner Meetings Series 2 – Synopsis

- Gateway Commercial
 - Building heights seem appropriate – good for business
 - The proposed uses are appropriate
 - The proposed street connection from the US 75 Frontage Road to Sherman is a great idea
- Central Sub-districts
 - Would like to see additional materials allowed for patio enclosures
 - Discussion / clarification was provided related to proposed on-premise alcoholic beverage consumption exemption
 - The alternate Belt Line Road / Main Street cross-section was supported
- Creative Corporate / Centennial Green
 - Support shown for green aspects (envisioned creek / open space amenities) of Centennial Green
 - Proposed building heights are appropriate
 - Proposed uses are reasonable – should consider adding High Tech Manufacturing (stand alone) to Creative Corporate with a Special Permit
 - A strong connection between Creative Corporate and the DART station is needed



Property and Business Owner Meetings Series 2 – Synopsis

▪ Rainside

- Achieving the allowed building heights would be difficult due to existing parcel sizes – would like to see the proposed heights achieved
- The proposed street connection (US 75 to Sherman) is a positive addition
- Would like to have the ability to have landscaping in the area
- Many questions / concerns expressed related to non-conformities
 - Need to be able to expand building in order to stay in place
- Non conformity questions / concerns continued:
 - Concerned that small business owners will be forced-out if a big developer comes in and wants to develop
 - Body shops on same campus with new car dealerships should be accommodated
 - Auto related activity inside of a building should be allowed
 - Concerned that if a fire occurs, existing businesses may not be able to rebuild or continue their use
 - Employees from auto related uses support other businesses in town



Community Workshop 2

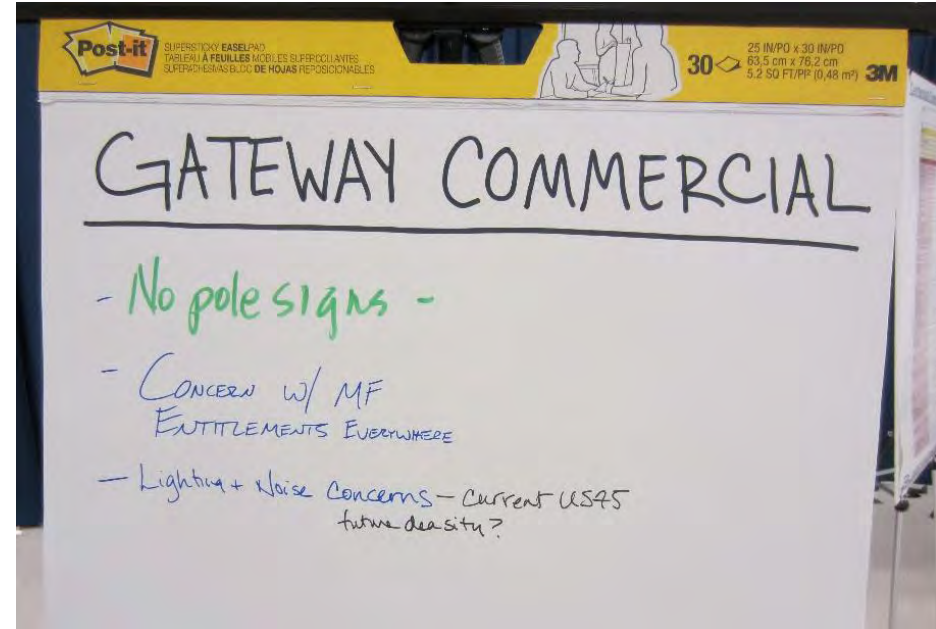
Community Workshop – Station Activities

- Station activities allowed participants to share their thoughts related to several topics that will provide additional detail for the rezoning initiative
- Information was gathered for each of the southern sub-districts related to desired uses, building heights and street types
- Information was gathered at a central sub-district station related to an alternate street cross-section for Belt Line Road / Main Street (west of Greenville) and proposed code amendments



Community Workshop – Gateway Commercial Feedback

- General comments:
 - No pole signs
 - Concern with multi-family entitlements everywhere (was proposed to be allowed with Special Permit only)
 - Lighting and noise concerns – current US 75 future density



Community Workshop – Centennial Green Feedback

- General comments:
 - *Floyd Branch as an amenity – potential for improvements through Brick Row*
- Proposed uses:
 - *No boarding kennel (was not proposed as a permitted use)*
 - *No helipad (was not proposed as a permitted use)*
 - *No smoking establishments (was proposed to be allowed with Special Permit only)*

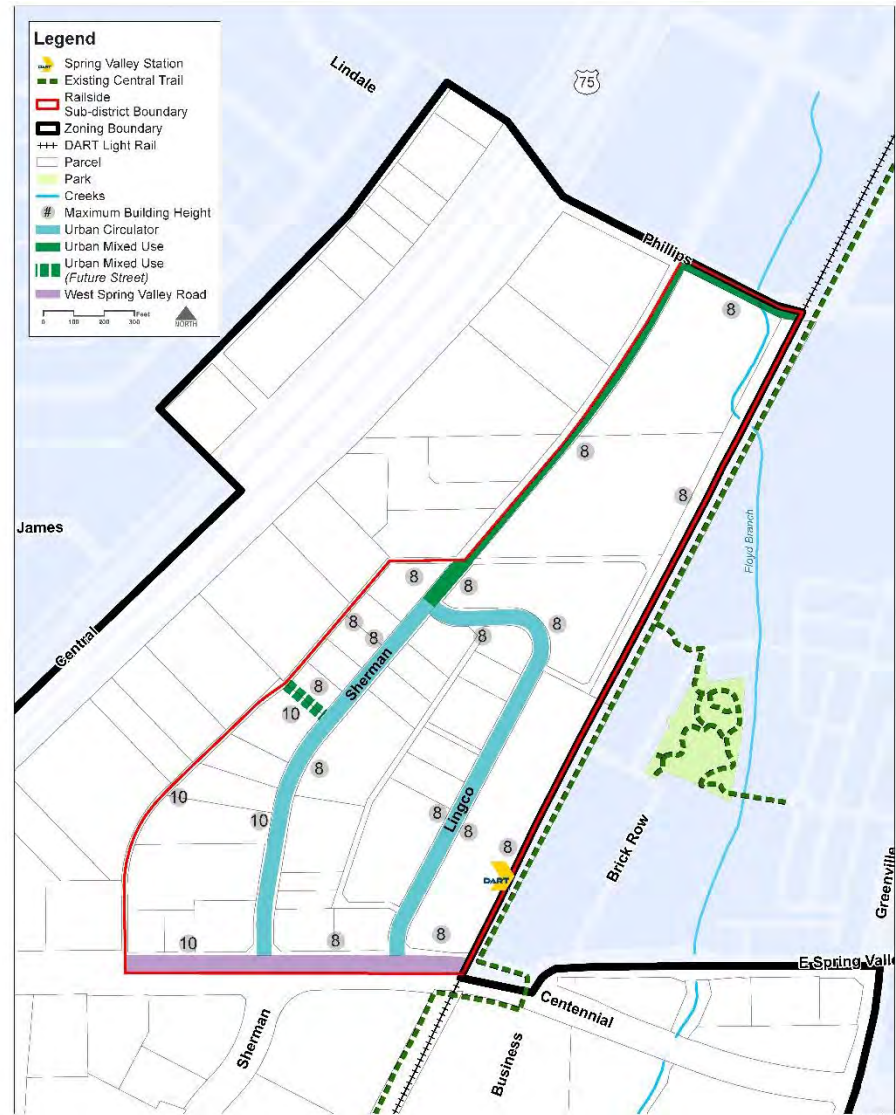
CENTENNIAL GREEN

-Floyd Branch as amenity
-potential for improvements through Brick Row



Community Workshop – Railside Feedback

- No comments received



Community Workshop – Central Sub-districts Feedback

- General comments:
 - Canvas carport structures okay in front of building setback? (was not proposed)
 - Fences and screening walls: allowable materials – consider adding vinyl

Existing Code Amendments and Refinements

DUMPSTERS

Through the application of the dumpster regulations over the last year, several issues have arisen. The following proposed changes reflect a response to the issues:

- Definition of dumpster has been modified to include "recycling dumpsters"
- Regulations have been modified to:
 - Clarify that the required masonry screening wall is included within the site placement requirements for the dumpster
 - Require that dumpster siting be in accordance with the City's Dumpster/Compactor Pad Detail for consistency with other City policies
 - Remove the minimum setback requirement from rear/side interior lot lines
 - Allow for a minor modification process to permit alternate container placement standards to accommodate servicing in unique situations

OUTDOOR SEATING AREAS AND PATIOS

New proposed standards to accommodate outdoor seating/patio enclosures for residential and non-residential uses were created based on property and business owner concerns of existing and future outdoor seating and patios:

- Outdoor seating areas (including patios and roof-top areas) are allowed, and may be partially or fully enclosed, subject to site plan approval
- Are considered as an accessory structure and shall be architecturally consistent with the primary building it is serving
- Shall not encroach into the minimum required sidewalk
- Non-residential and residential outdoor seating areas may be enclosed by a perimeter fence and/or wall; however, if located within a front yard or Required Front Build-to Zone then it shall:
 - Be constructed of tubular steel, wood, masonry and/or a combination thereof
 - Shall not exceed 42-inches in height
 - Shall be a minimum 50% open or transparent (residential applications only)
- Residential (rooftop only) and nonresidential outdoor seating areas may be enclosed temporarily if constructed of canvas, screen mesh, sun shade fabric, clear vinyl materials or other materials as approved by the City Manager or designee intended for outdoor use
- Residential outdoor seating areas when located in a front yard or within the Required Front Build-to Zone shall not be partially or fully enclosed (exception: rooftop)
- Shall comply with building and fire codes and shall be maintained in good condition

FENCES AND SCREENING WALLS

The following proposed regulations were developed to address fences and screening walls which were not addressed in the previous code:

- Fences and screening walls:
 - Shall not be placed between the existing building and street right-of-way line (except in Interurban Sub-district for contracting operations and specified motor vehicle related uses)
 - Shall not be placed within the Required Front Build-to Zone or within the front yard building setback (except in Interurban Sub-district for contracting operations and specified motor vehicle related uses)
 - May have an inward opening vehicle ingress/egress gate setback a minimum 20 feet from the street right-of-way line
 - Allowed materials - tubular steel, wood, masonry, and other materials as approved by the City Manager or designee. Vinyl, chain-link (including but not limited to galvanized, powder or vinyl coated), barbed wire, and/or razor wire are prohibited
 - When located in front of the existing building and/or within the Required Front Build-to Zone in the Interurban Sub-district, shall be tubular steel (or similar material) and minimum 50% open or transparent
 - Allowed maximum eight feet in height; maximum five feet in height for contracting operations and motor vehicle related uses if located in front of the existing building and/or within the Required Front Build-to Zone in the Interurban Sub-district
 - Finished side of the fence or screening wall visible from streets and/or public open spaces will face outward
 - Shall comply with additional permitting and placement requirements within the City's Code of Ordinances (Chapters 6 and 22)

Recall issue Vinyl

Community Workshop – Other Comments

- Other comments:
 - *Please don't let there be too many "big box" apartment developments. A monoculture supply leaves the City more susceptible to economic cycles and future shift in preferences. Witness what happened with Nortel, Countrywide, etc. Boom and Bust (i.e. fads don't last)*
 - *Monolithic ownership / management can expose the City to a big chunk (of properties) going downhill all at once. [Cited that Vickery Meadows (Park Lane at Greenville in Dallas) was once marketing young, single professionals]*
 - *A wider range of owners, big and small, can respond to market forces and be more resilient to changes in the economy*



Recommendations for City Council Consideration

Recommendations for Refinement of Direction

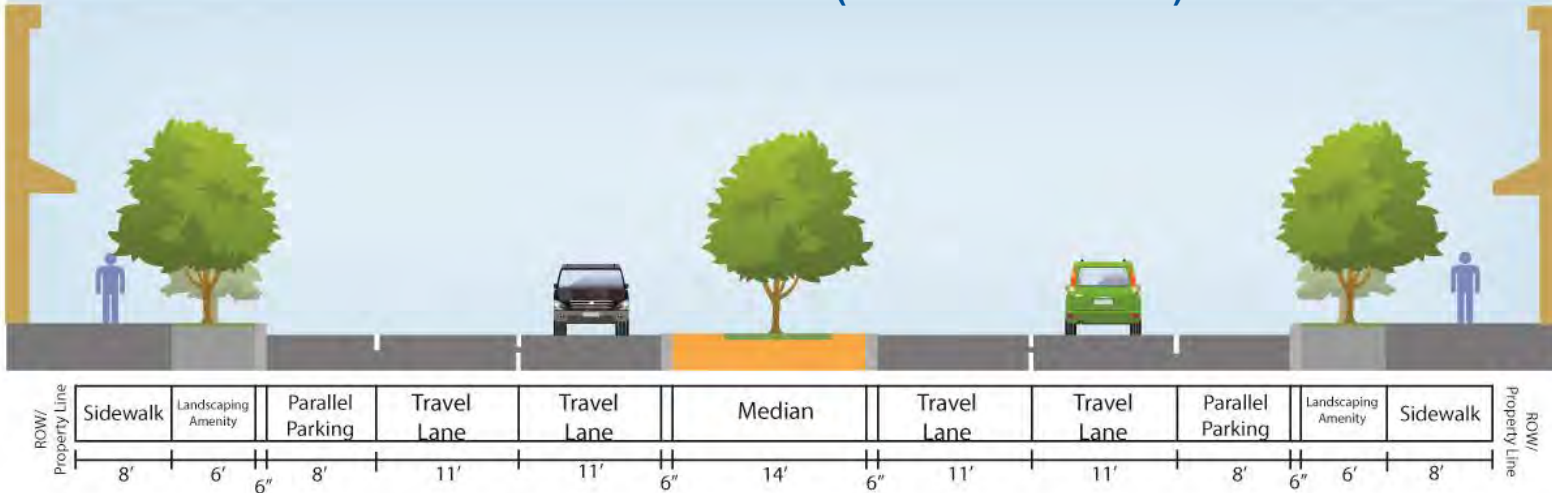
- Gateway Commercial
 - Maintain direction that multi-family will be allowed in this district, but only with a special permit
- Railside
 - Maintain current direction related to previously approved non-conformities for the corridor
- Creative Corporate
 - Provide a bike / pedestrian trail connection to DART that is comfortable for nighttime use (West Spring Valley and Prestonwood / Buckingham)
- Centennial Green
 - Create a specialty site designation along the Floyd Branch and Lois Branch, and add supporting code language to enable creek enhancement as an amenity for adjacent residential uses
- Central Sub-districts
 - Maintain current code direction



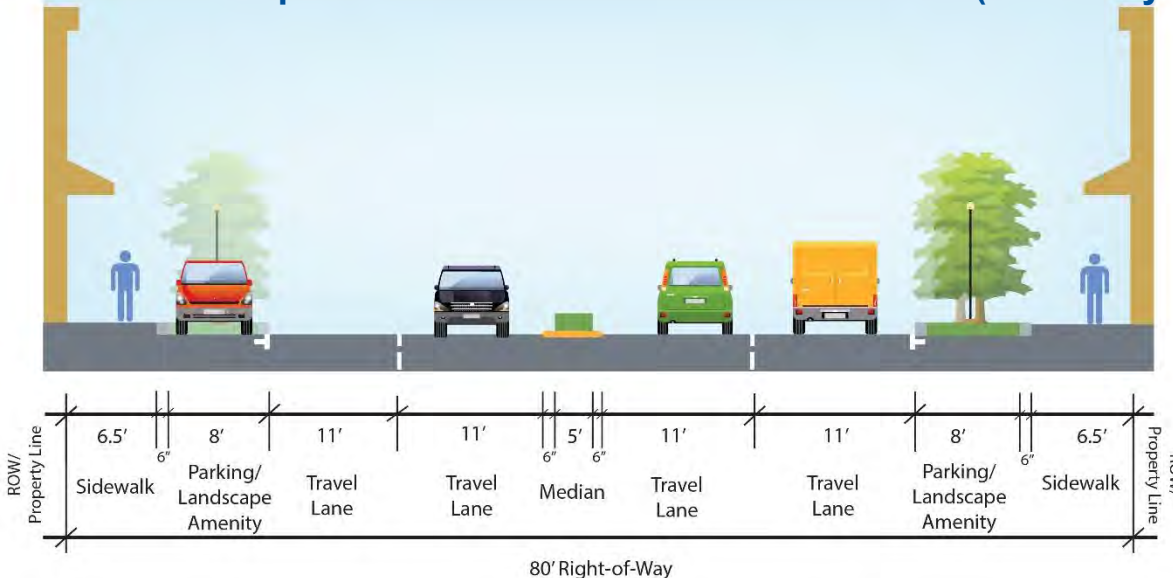
Belt Line Road / Main Street – Alternative Cross Section

Belt Line Road / Main Street Cross-sections

Current Code Cross-Section – Belt Line / Main (West of Greenville) 104' R.O.W.



Alternate Proposed Cross-Section – Belt Line / Main (McKinney to Texas Street) 80' R.O.W.



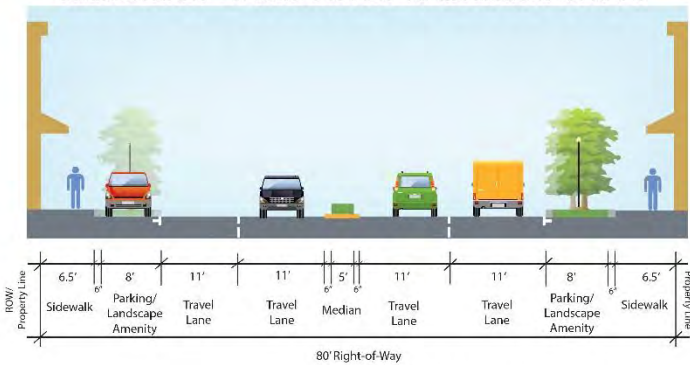
Comparison – Coded vs. Alternate Street Cross-section

Site Element	Coded Street Cross-section (at McKinney St.)	Alternate Street Cross-section (at McKinney St.)
Sidewalks (both sides)	8 ft.	6.5 ft.
Landscape/Amenity Zones (both sides)	6 ft.	8 ft. – Combined into same lane (i.e. parking lane with landscape islands)
Parallel Parking (both sides)	8 ft. (dedicated lane)	
Travel Lanes (all)	11 ft. (44 ft. total)	11 ft. (44 ft. total)
Median	14 ft.	5 ft.
Total ROW Width	104 ft.	80 ft. (existing varies)

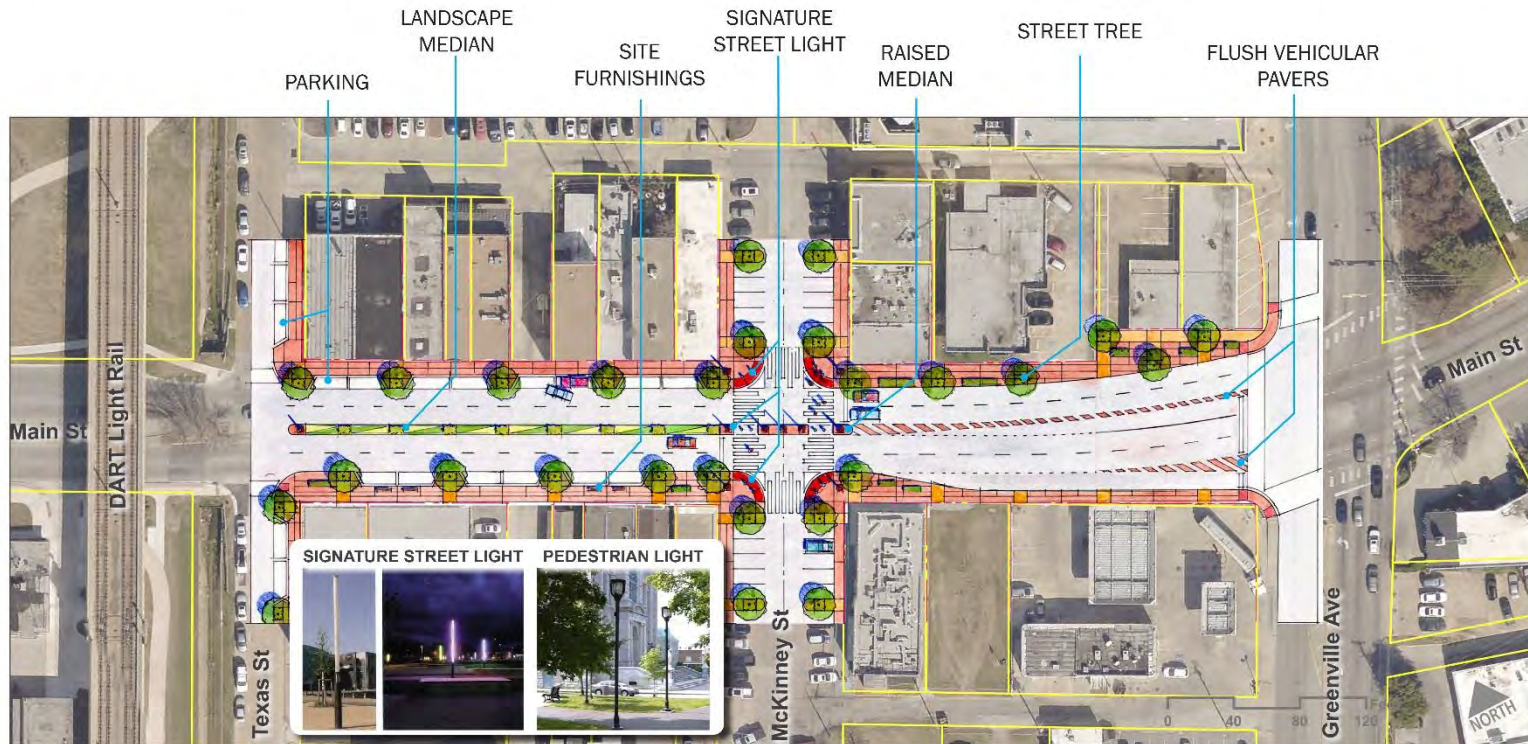
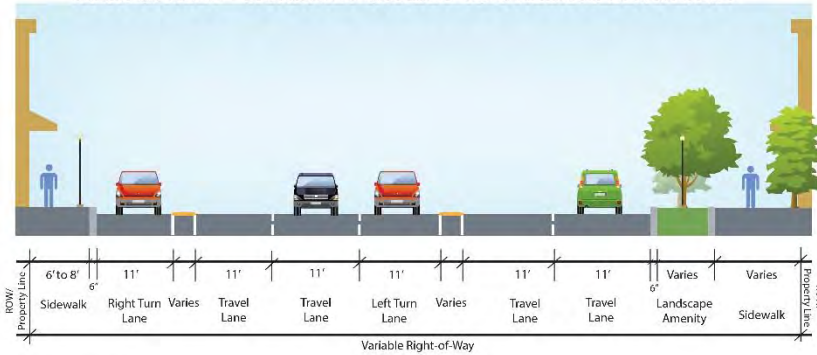


Belt Line Road / Main Street – Alt. Proposed Cross-section

MAIN STREET - TEXAS STREET TO MCKINNEY STREET



MAIN STREET - MCKINNEY STREET TO GREENVILLE AVENUE



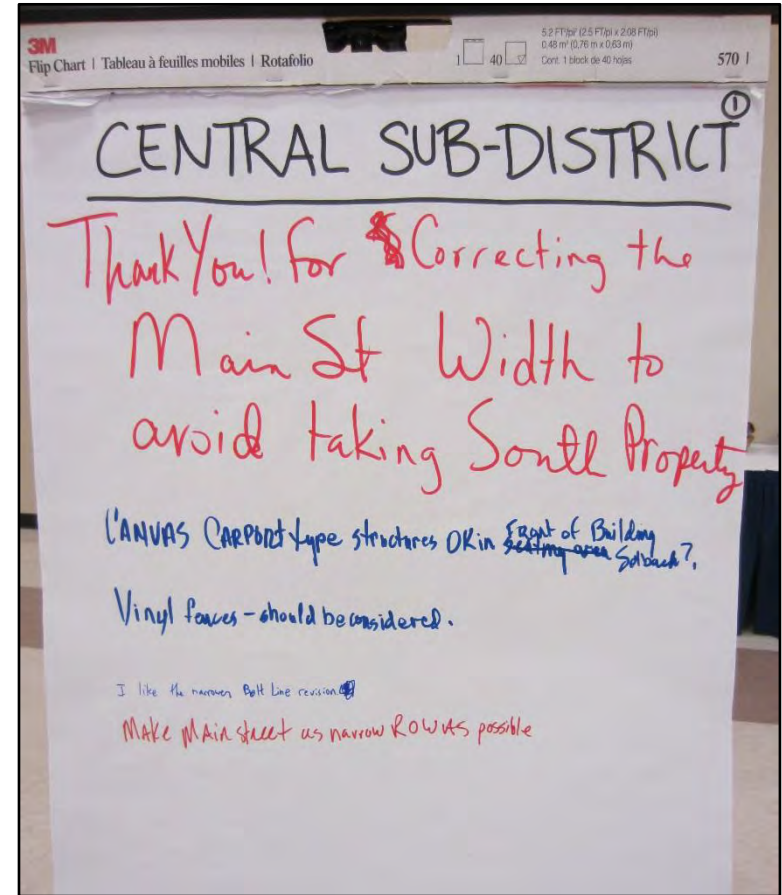
Alternate Cross-section – Additional Observations

- Contained within existing ROW except at NWC of Belt Line / Main and Greenville – ROW needed for intersection realignment
- Accommodates dedicated left turn lanes Belt Line / Main at Greenville (NB and SB)
- Accommodates right turn lane for EB Belt Line / Main to SB Greenville
- Between McKinney and Greenville – no on-street parking and landscaping areas minimal
- Sidewalk widths could become less
- NWC Belt Line / Main at Greenville – opportunity to create small open space area with balance of property
- Should properties redevelop, owners could choose to set back buildings farther away from ROW up to 10 ft. (as currently coded) – allows for wider sidewalk area and/or small landscape areas



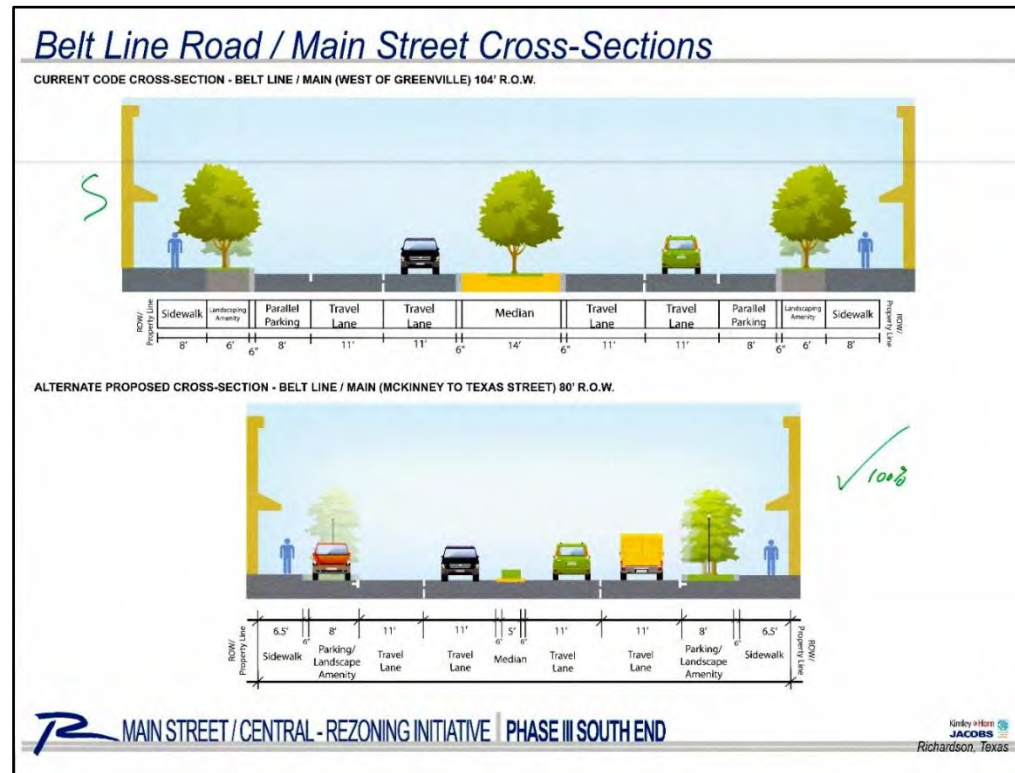
Alternate Cross-section – Community Workshop Comments

- Thank you for correcting the Main Street width to avoid taking south property
- I like the narrower Belt Line revision
- Make Main Street as narrow right-of-way as possible



Recommendation

- Replace the existing coded Belt Line Road / Main Street cross-section with the new alternate cross-section



**Update - Information Requested at 6/20
CPC / City Council Joint Session**

Auto-Related Uses – Additional Information

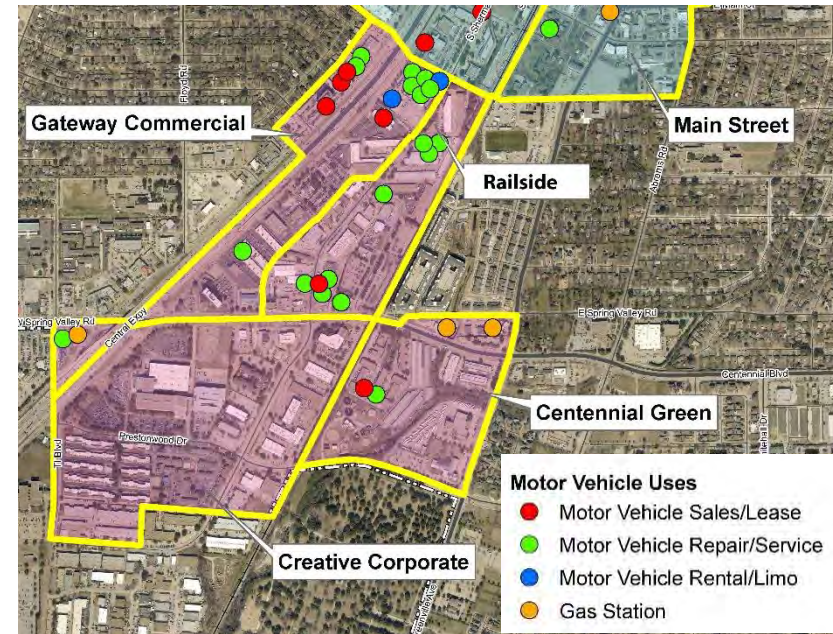
Requested by Council on 6/20

- Existing number of auto-related uses in the southern sub-districts
- Existing number of auto-related uses in the Central Place and Interurban sub-districts
- Where are additional areas that these uses can locate within Richardson?



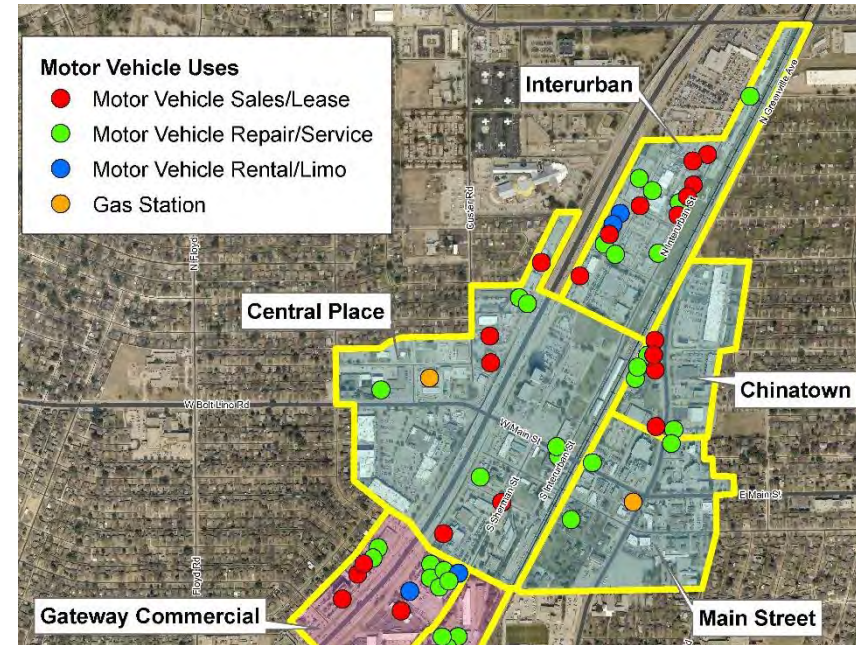
Existing Auto-Related Uses – Southern Sub-districts

- The largest concentration of existing auto-related uses occurs within the Gateway Commercial Sub-district (17 businesses)
- These businesses are primarily within the area recommended to allow the uses with special permit (west side of Central Expressway, and on the east side of Central Expressway within 1,000 feet south of Phillips Street)
- A majority of these uses are non-conforming today
- Allowing the uses with special permit provides existing businesses the opportunity to request a special permit to legalize their use, otherwise they can continue operating as a non-conforming use



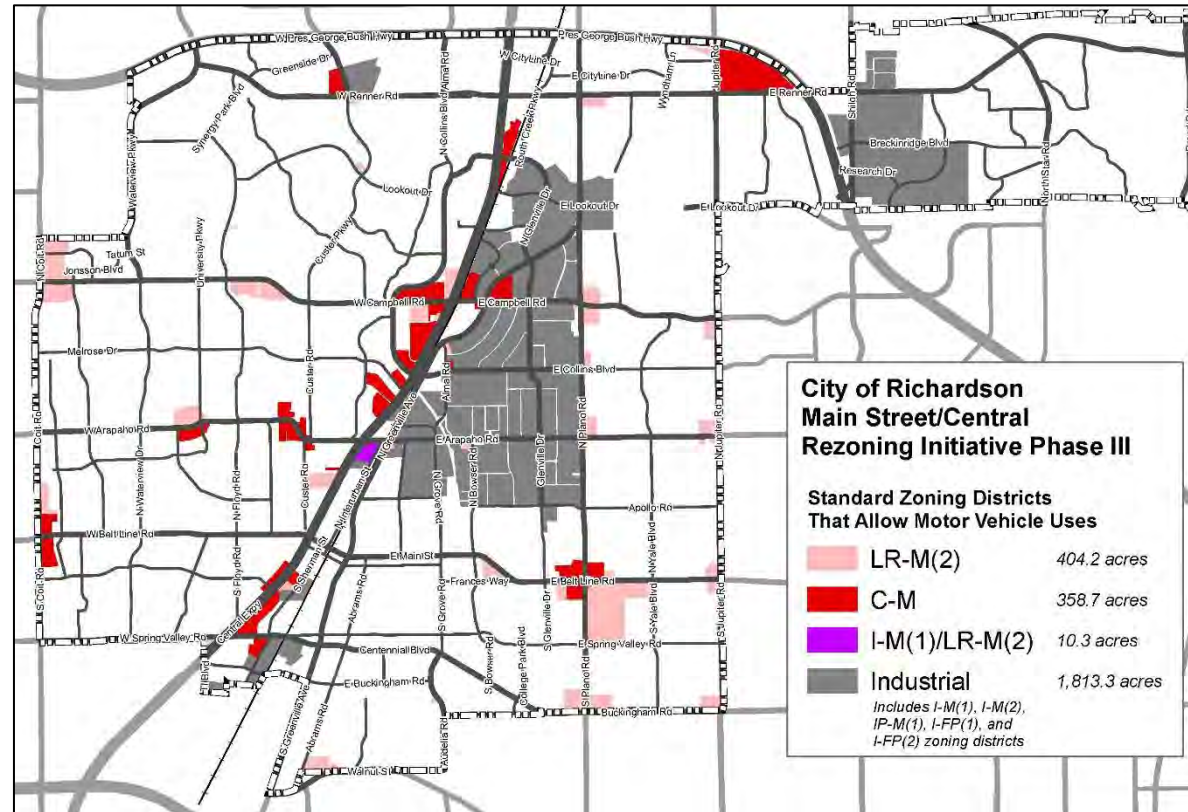
Existing Auto-Related Uses – Central Sub-districts

- The largest concentration of existing motor vehicle related uses occurs with the Interurban Sub-district (22 businesses)
- These uses are all non-conforming uses today due to the Main Street/Central Expressway Form Based Code adoption (many if not all uses were non-conforming prior to the Code adoption due to the 2008 CZO amendments)
- Motor vehicle parts and accessory sales, motor vehicle rental (limit ten vehicles), and motor vehicle sales/leasing (new) are allowed by right in the Central Place and Interurban Sub-districts
- All other motor vehicle related uses require approval of a special permit in the Central Place and Interurban sub-districts



Existing Auto-Related Uses – Where they can Locate

- There are standard zoning districts available throughout the city for businesses to consider locating within, the largest concentration being an industrial zoned area east of Central Expressway



Auto-Related Uses – Main Street/Central Corridor

▪ Central Sub-districts (Existing)

- All existing uses allowed to continue operating as non-conforming uses
- Subject to non-conforming provisions within Code
- Central Place and Interurban Sub-districts: allowed with Special Permit
- Main Street and Chinatown Sub-districts: prohibited

▪ Southern Sub-districts (Recommendation – 6/20 Joint Session)

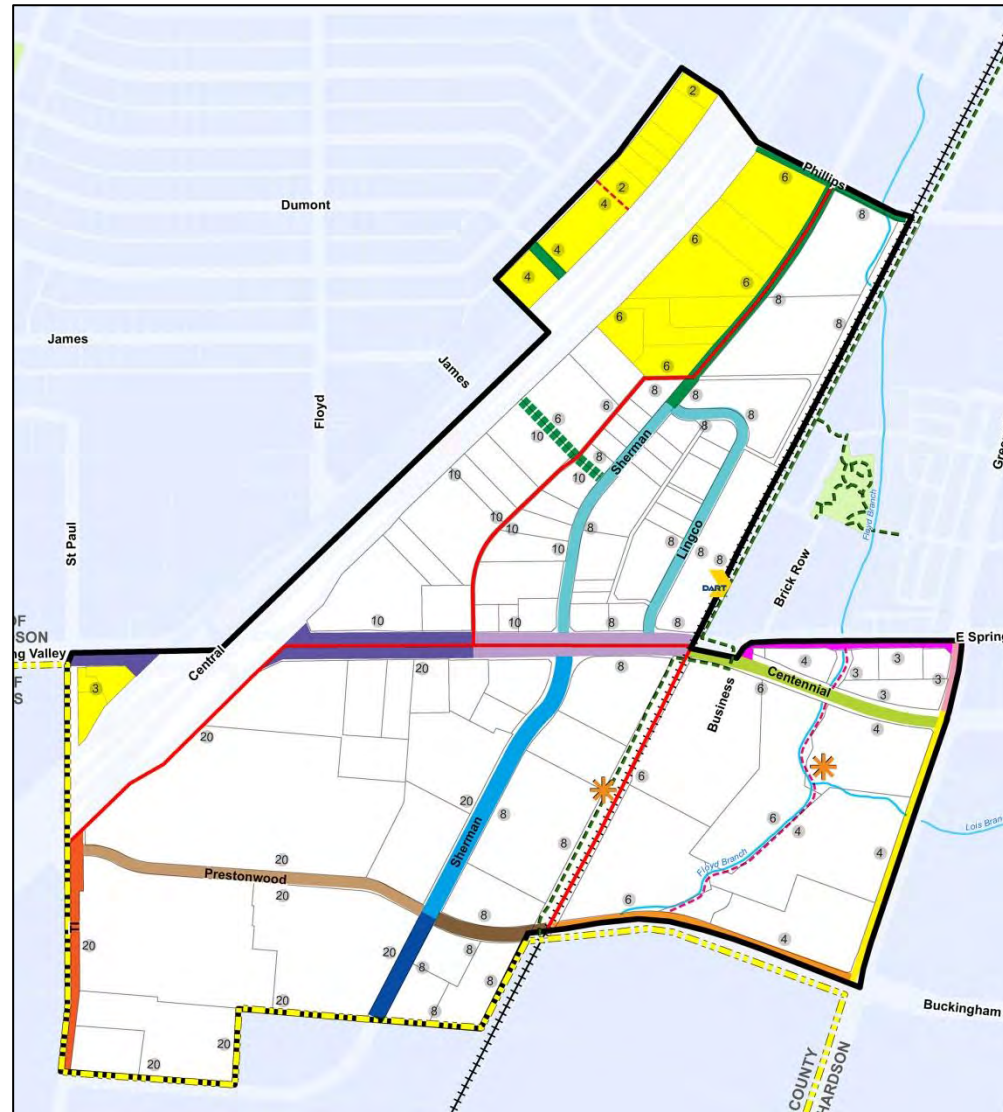
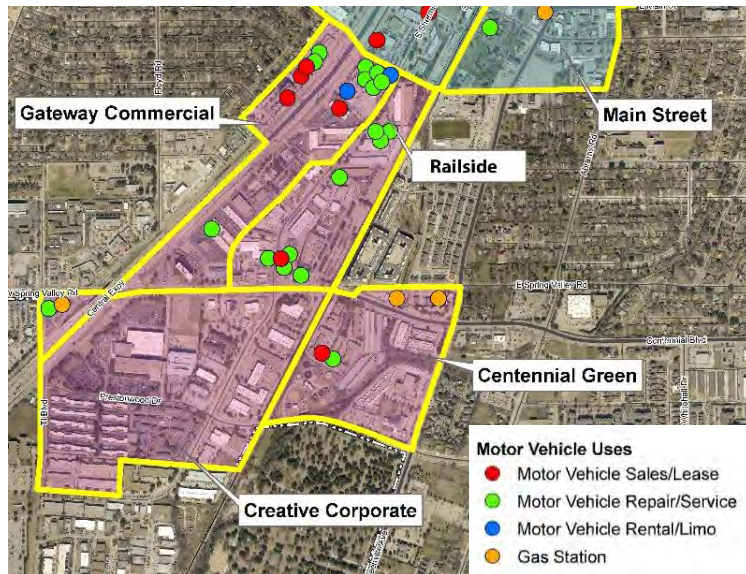
- All existing uses allowed to continue operating as non-conforming uses
- Subject to non-conforming provisions within Code
- Gateway Commercial Sub-district: allow with Special Permit on west side of US 75 and on east side of US 75 within 1000 ft of Phillips St
- Creative Corporate, Railside and Centennial Green Sub-districts: prohibit
 - Existing uses non-conforming today due to no Special Permit; or uses not allowed under current zoning (SVSD)



Auto-related Uses – 6/20 Joint Session Recommendation

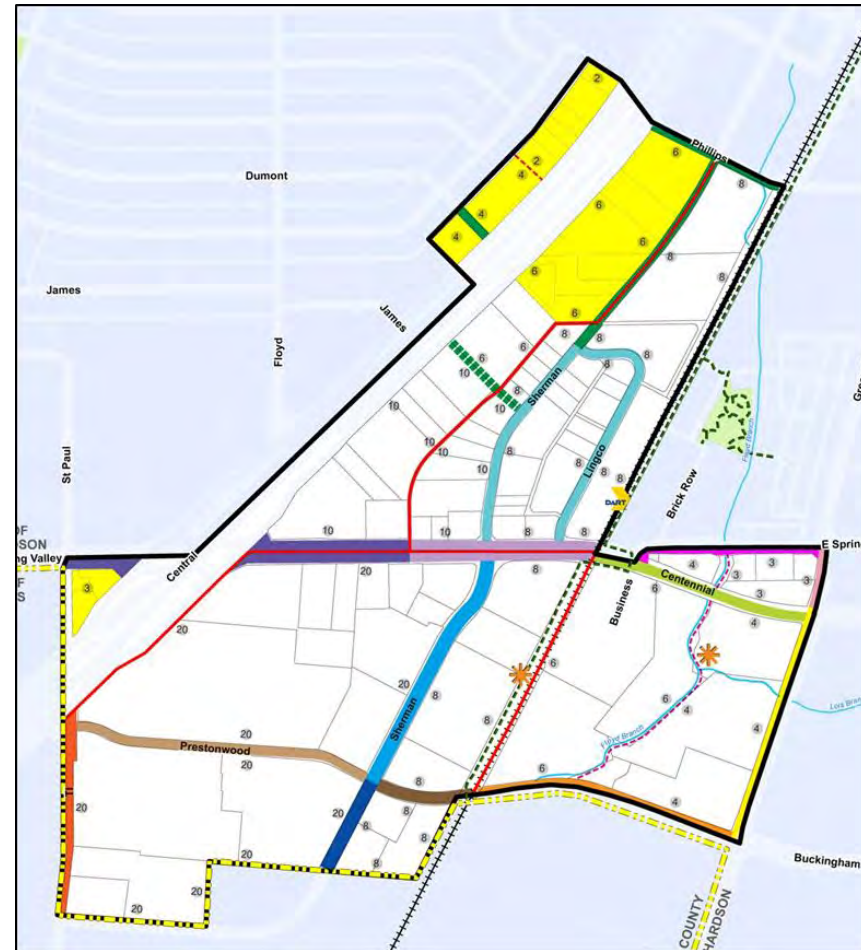
Gateway Commercial

- Allow with Special Permit on west side of US 75 and on east side of US 75 within 1000 ft of Phillips St (yellow areas on map)
- Prohibited elsewhere in district



Auto-related Uses – Post Code Adoption

- All existing Special Permits will cease to exist with the adoption of the new Code for the southern sub-districts
 - Current Special Permits were connected to the standard zoning districts under which they were granted [(e.g. C-M, I-M(1), SVSD)]
 - Similar approach as central sub-districts
- Uses are allowed to continue operating as non-conforming uses in accordance with the non-conforming provisions for the corridor
- Properties only within the yellow shaded area are allowed to request a Special Permit to become legal conforming uses
- Could include text within the new Code to retain any previously approved Special Permits



Next Steps

Summary

Timeframe	Task
August 2016	<ul style="list-style-type: none">• Prepare code for southern sub-districts• Prepare code amendments for central sub-districts
September/October 2016	<ul style="list-style-type: none">• Public hearing notification (mailed notice)• CPC code consideration<ul style="list-style-type: none">– 1st meeting Tues. Sept. 20– 2nd meeting Thurs. Oct 6 (<i>rescheduled from Tues. Oct. 4</i>)
October/November 2016	<ul style="list-style-type: none">• Public hearing notification (newspaper notice)• Council code consideration and adoption – Mon. Oct. 24



Next Steps

- Based on feedback received from tonight's meeting, the team will incorporate comments into the draft zoning ordinance for each of the southern four sub-districts, and the proposed code amendments to the existing Main Street/Central Expressway Form Based Code.
- The draft zoning ordinance for the southern sub-districts and the amendments for the central sub-districts will be presented to the City Plan Commission in public hearings in September and October for potential recommendation for adoption.
- The draft zoning ordinance for the southern sub-districts and the amendments for the central sub-districts will be presented to the City Council in public hearings in October and November for potential adoption.



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MAIN STREET / CENTRAL - REZONING INITIATIVE | PHASE III SOUTH END

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