

City Council Work Session Handouts

October 24, 2016

- I. Review and Discuss Zoning File 16-24 Main Street/Central Expressway Form Based Code (PD Development) (Work Session – Richardson Room)
- II. Review and Discuss Zoning File 16-24 Main Street/Central Expressway Form Based Code (PD Development) (Public Hearing – Council Chambers)

City Council Work Session

October 24, 2016



Image Source – City of Richardson



MAIN STREET / CENTRAL - REZONING INITIATIVE | PHASE III SOUTH END

Kimley»Horn
JACOBS
Richardson, Texas

Agenda

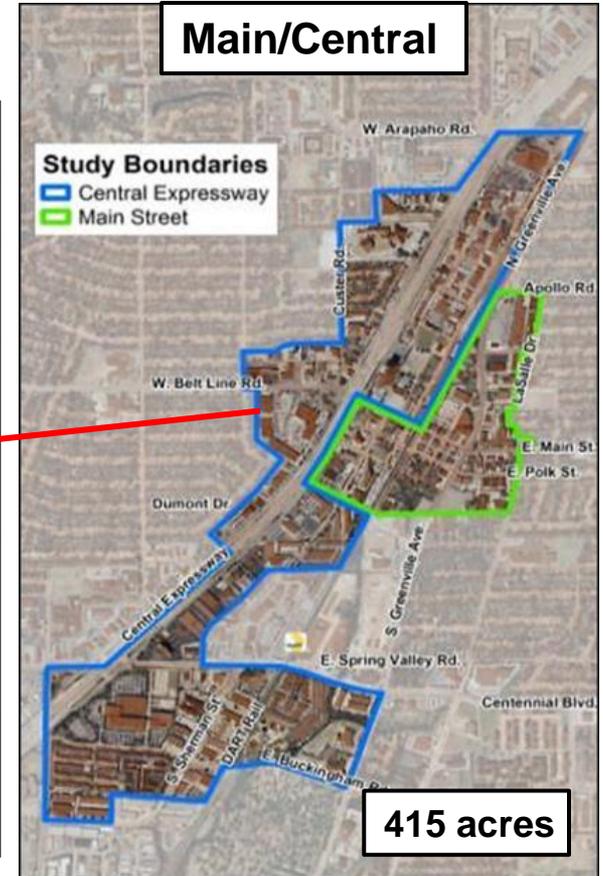
- Project Overview
- Community Input Process
- Key Direction from City Plan Commission and City Council
- Code Layout, Organization and Content
- Next Steps



Project Overview

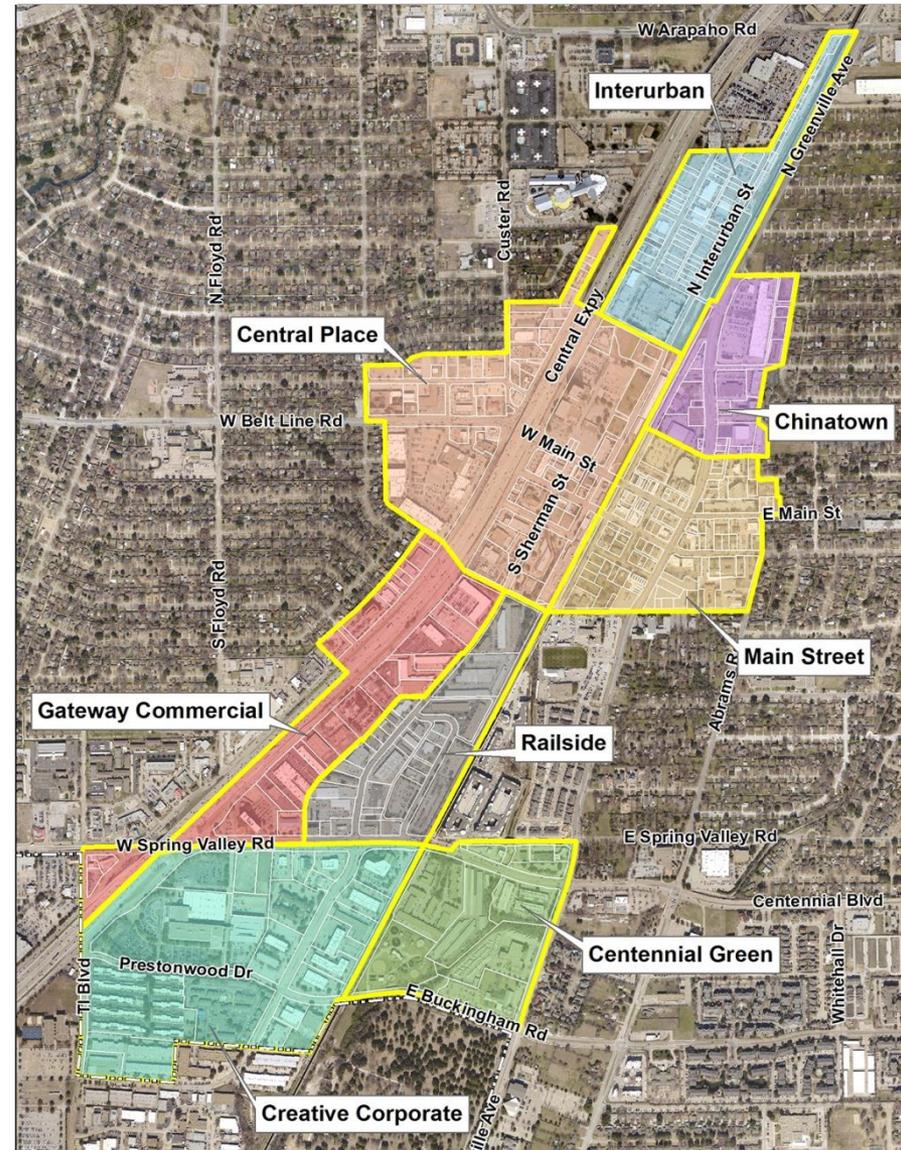
2009 Comprehensive Plan – Six Enhancement Areas

- West Spring Valley (Implementation)
- East Arapaho/Collins (Implementation)
- West Arapaho
- Coit
- Old Town/Main Street (Implementation)
- **Central** (Implementation – portion of corridor)



Main Street / Central Expressway Project

- **Phase I** created a vision for the study area and for each of 11 sub-districts based on community goals and market realities (2013)
- **Phase II** created a Form Based Code for the 4 Central Sub-districts (2015)
- **Phase III** focuses on the 4 Southern Sub-districts and will create a Form Based Code for these areas (2016); includes refinements to the existing Code in the Central Sub-districts



Community Input Process

Community Input

The project has used a variety of techniques to gain input from property and business owners, Richardson residents, and other stakeholders:

- Community and Stakeholder Involvement, Round 1
 - Property and business owner meetings on March 2 and 3, 2016 – 62 participants
 - Community Workshop on March 29, 2016 – 65 participants
 - Online Survey during April 2016 – 295 responses
- Community and Stakeholder Involvement, Round 2
 - Property and business owner meetings on July 11 and 12, 2016 – 27 participants
 - Community Workshop on July 12, 2016 – 35 participants
- Input from stakeholders was presented to City Plan Commission/City Council at key points in process for feedback and direction
- City Plan Commission, by a vote of 6-0, recommended approval of the Code as presented on September 20, 2016



Community Awareness

A variety of techniques have been used to promote this project throughout the community:

- Richardson Today
- Week In Review
- Project website
- Project Facebook page
- HOA Presidents Meeting
- Realtors Workshop
- Neighborhood Leadership Workshop
- Zoning signs posted throughout project area
- Letters to property and business owners, residents and other interested persons (workshops and stakeholder meetings)

- Public notification including courtesy letters
- Email distributions

Workshop seeks public input on Main Street/US 75 redevelopment

A community workshop to discuss the next phase for implementing the Main Street/Central Expressway Enhancement/Redevelopment Strategy will be held Tuesday, March 29 at 6:30-9 p.m. in the Grand Hall at the Richardson Civic Center/City Hall.

The workshop will focus on Phase 3 of the project that consists of rezoning four sub-districts located in the southern portion of the Corridor—Gateway Commercial, Trailside, Creative Corporate and McKamy Spring. Complete new land use and development regulations will be created for the sub-districts, incorporating the sub-districts into the Main Street/Central Expressway Form Based Code.

These four sub-districts are in an area that stretches along the east and west sides of US 75 from Phillips Street to south of Spring Valley Road.

Concurrently with rezoning the southern end of the Corridor, the City will be making refinements to the existing Code that apply to the central portion of the Corridor—the Interurban, Central Place, Chinatown and Main Street sub-districts—which was rezoned January 2015.

Throughout the course of this project, there will be more opportunities for input from property owners, business owners and other interested persons during stakeholder meetings, community workshops, and public hearings, as well as online.

Questions regarding the workshop and/or the overall strategy for redevelopment and revitalization of Main Street and the Central Expressway Corridor may be directed to Tina Firsens, Planning Projects Manager, at tina.firsens@cor.gov or mainstreetcentral@cor.gov. More information may also be found at www.cor.net/msc.



Key Direction from City Plan Commission and City Council

Key Topics

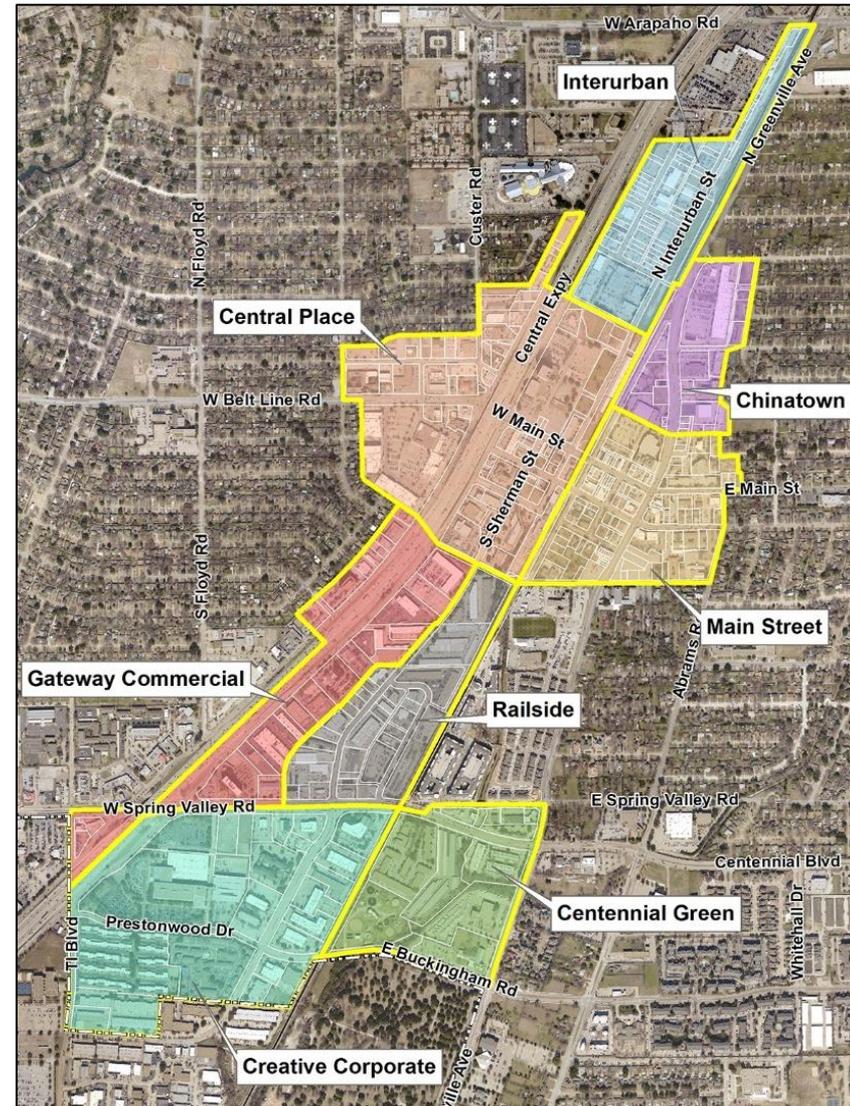
- City Plan Commission and City Council provided direction related to key topics for developing the draft Code:
 - Allowable uses (including auto-related uses)
 - Building heights
 - Development adjacent to US 75
 - Non-conforming properties
 - Future streets
 - Bicycle/pedestrian connections to DART Rail Line
 - Removal of development caps
 - Belt Line Road/Main Street (West of Greenville Avenue) cross-section
 - Loading operations within streets



Code Layout, Organization and Content

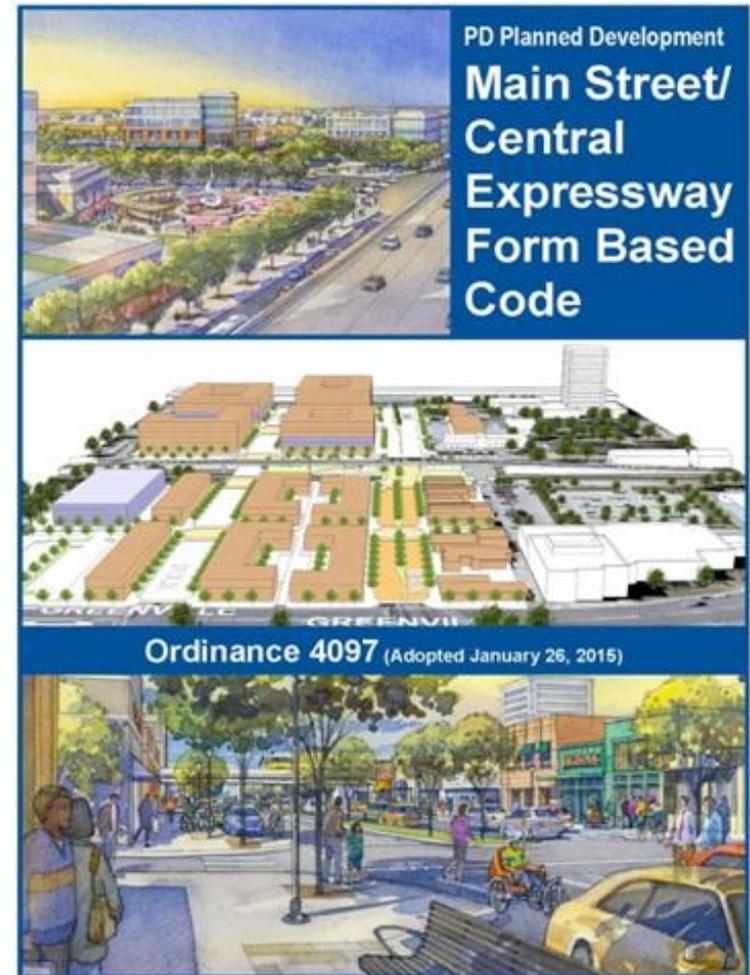
Form Based Code Coverage

- Covers eight sub-districts in Richardson identified in the *Main Street/Central Expressway Study*:
- Southern Sub-districts (**new Code**)
 - Gateway Commercial
 - Creative Corporate
 - Railside (*formerly Trailside*)
 - Centennial Green (*formerly McKamy Spring*)
- Central Sub-districts (**refinements to existing Code**)
 - Interurban
 - Central Place
 - Chinatown
 - Main Street



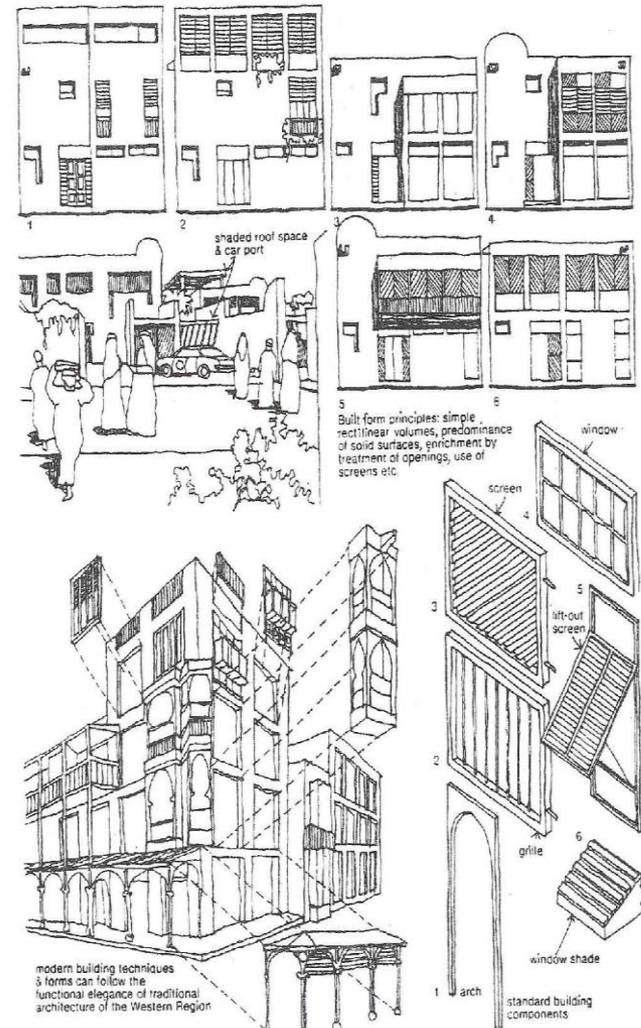
Working Assumptions for Southern Sub-districts

- Continue to use same Code format for Southern Sub-districts that was previously approved for Central Sub-districts
- Overall, a consistent use list will apply to properties in each sub-district; however, may limit uses in certain instances
- Continue to use the same approach for regulating non-conforming uses, structures, signs and site elements
- City Council and City Plan Commission concurred with these working assumptions (June 2016)



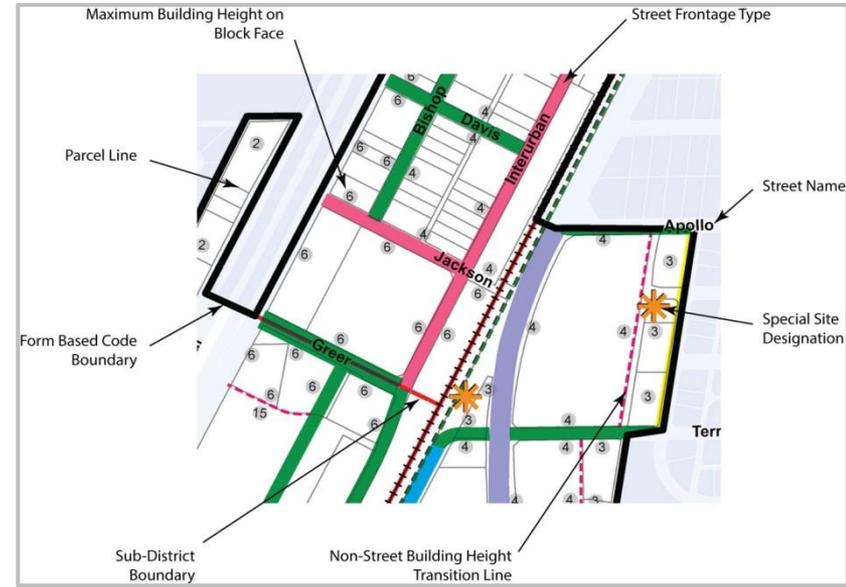
Form Based Code Components

- Overview of Code
- Sub-districts
- Signage
- Administration
- Definitions
- Appendices



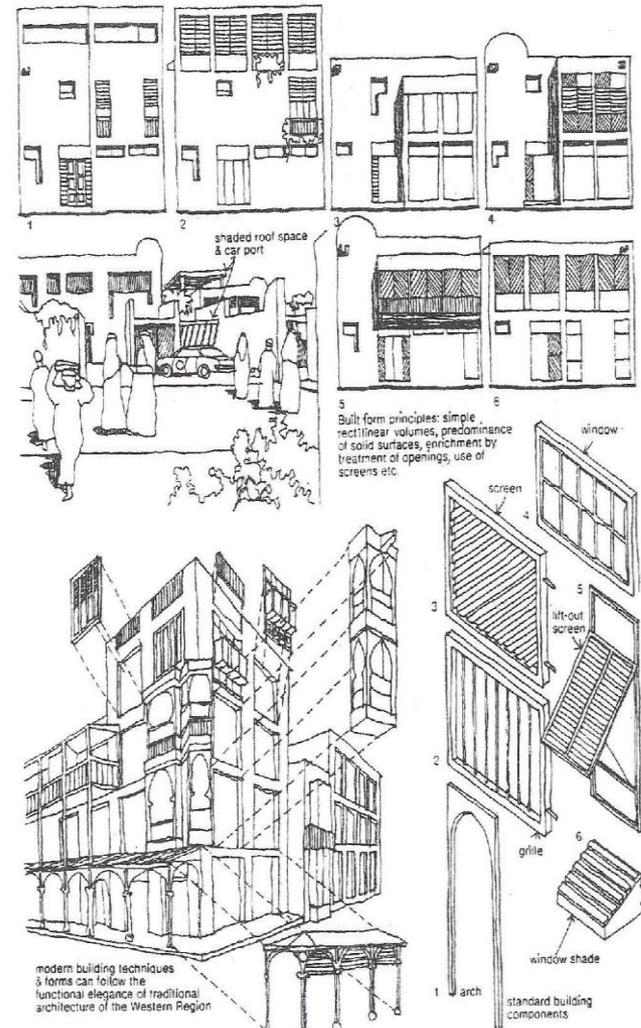
Form Based Code Components

- Section I – Overview of Code
 - Intent/Purpose
 - Components of Code
 - How to Use the Code
 - Understanding the Regulating Plan
 - Regulating Plan/Sub-district Location



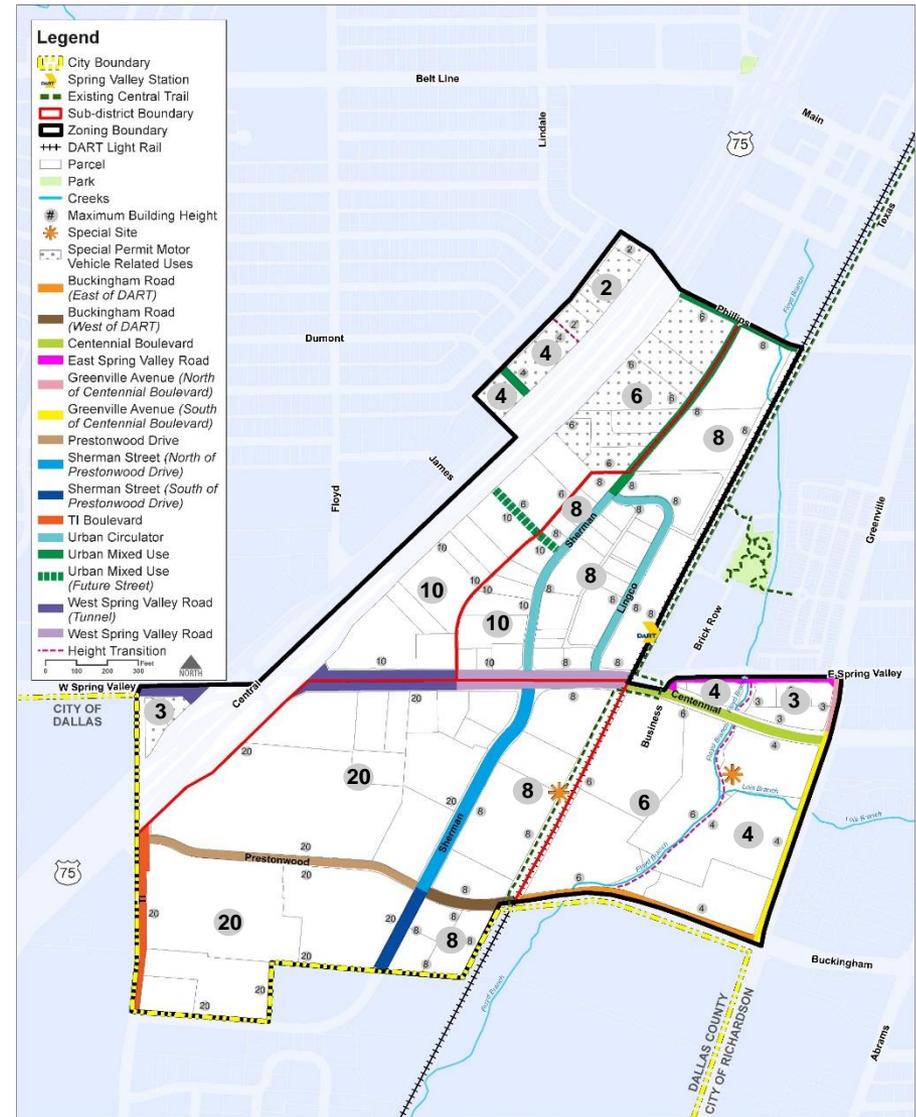
Form Based Code Components

- Section II - Sub-districts
 - Introduction
 - Regulating Plan
 - Public Open Space Plan
 - Public Parking Plan (where applicable)
 - Building and Envelope Standards
 - Street Typology and Streetscape Standards
 - Architectural Standards
 - Mechanical, Service Areas and Utilities
 - Thoroughfare Screening (where applicable)
 - Residential Zoning District Adjacency (where applicable)
 - Signage



Overall Sub-district Regulating Plan – Southern Sub-districts

- Identifies how all 4 Southern Sub-districts relate to each other
- Shows the street type designations and how the streets relate to each other
- Basis for which all other Code standards are applied
- Shows maximum building heights and land use area restrictions



Individual Regulating Plans – Southern Sub-districts

Gateway Commercial

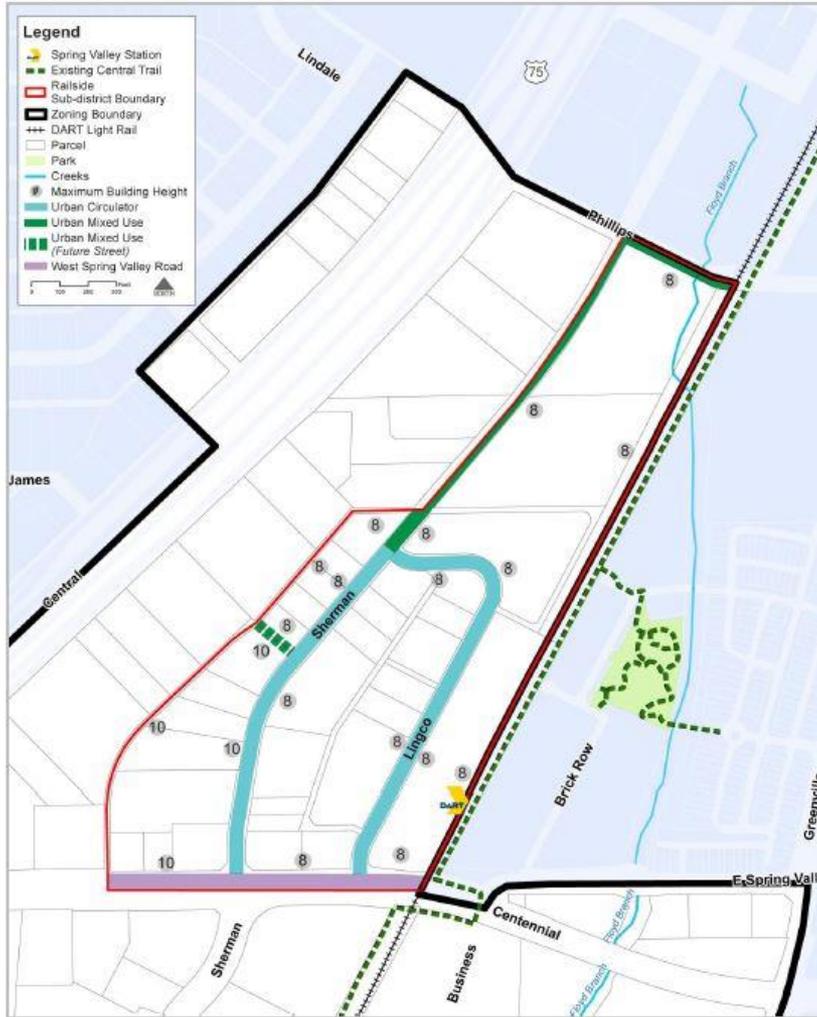


Creative Corporate

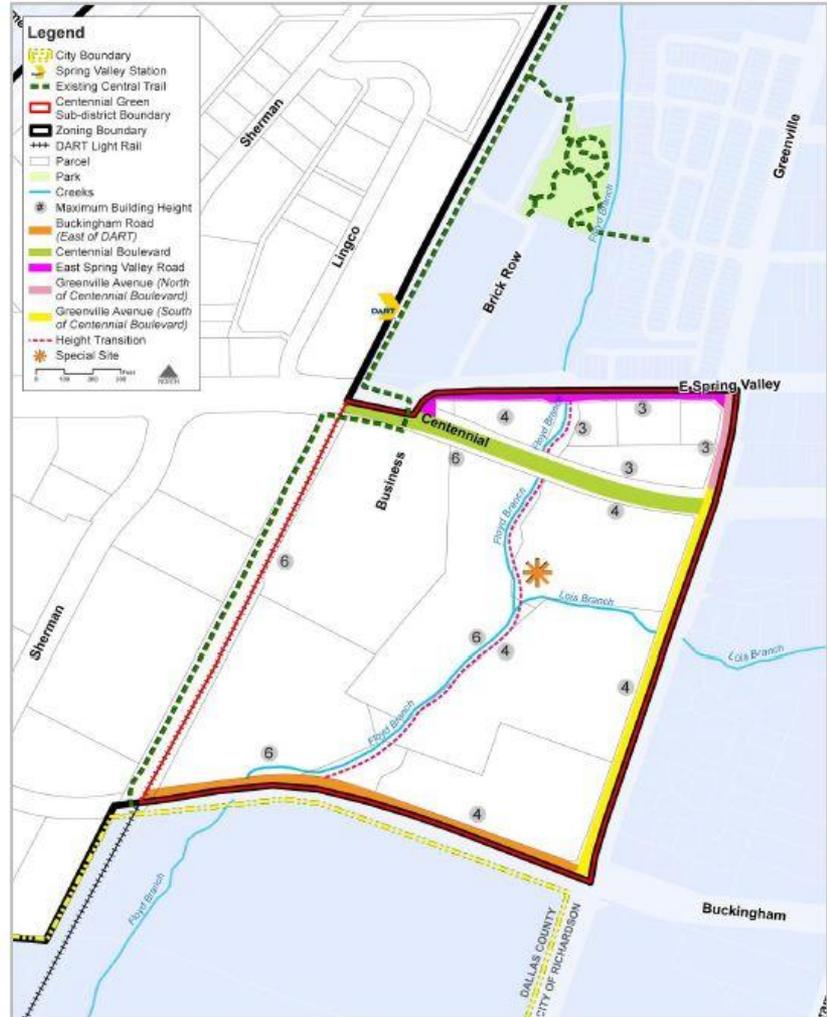


Individual Regulating Plans – Southern Sub-districts

Railside

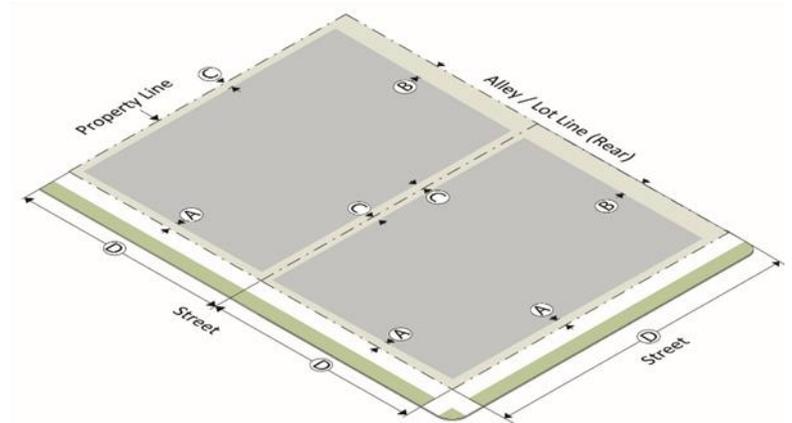
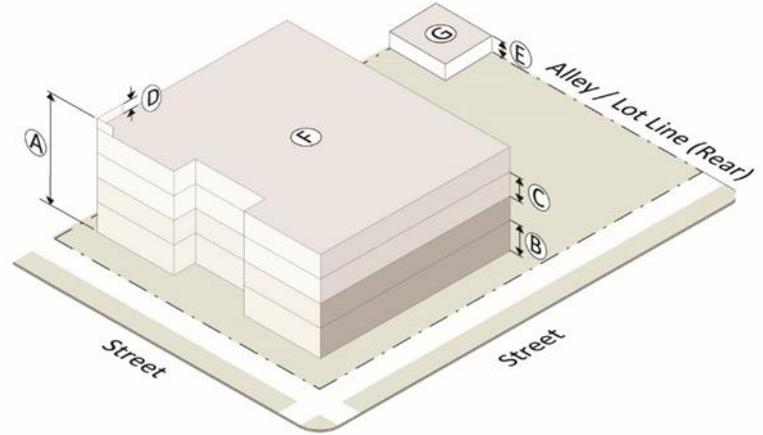


Centennial Green



Building Envelope Standards

- Establishes a list of allowable uses for each sub-district
- Establishes lot development standards based on street frontage for:
 - Building height
 - Maximum building ground floor
 - Required front build-to zones or minimum front yard building setbacks
 - Required rear and interior setbacks
 - Building frontage buildout
 - Parking



Building Envelope Standards – Schedule of Uses

- Use lists specific to each sub-district based on **intent** and **purpose** of sub-district
- Uses are allowed throughout each sub-district (unless otherwise specified) by right or with a special permit
- Some uses include supplemental use requirements to further regulate location and type of use

Railside Use Type	How Permitted P – Permitted S – Special Permit	Supplemental Use Requirements
Residential		
Multi-Family	P	Minimum Density of forty dwelling units per acre.
Townhome	P	Minimum Density of ten dwelling units per acre.
Non-Residential		
Adult Day Care	S	
Antenna, Accessory	P	See Article XXII-E, Supplemental Regulations for Certain Uses, Richardson Comprehensive Zoning Ordinance, as amended or its successors.
Antenna, Commercial	P	
Antenna, Commercial, In Excess of Three Antennas	S	
Antenna, Freestanding	S	
Antenna, Mounted	P	
Art Gallery	P	
Assisted Living Facility	S	
Bakery	P	
Bank or Financial Institution	P	
Barber or Beauty Salon	P	Skin and nail care as accessory uses only.
Beer and Wine Package Sales - 75% or more Revenue from Sales	S	
Beer and Wine Package Sales – Less than 75% Revenue from Sales	P	
Catering Service	P	
Child Care Center	S	See Article XXII-E, Supplemental Regulations for Certain Uses, Richardson Comprehensive Zoning Ordinance, as amended or its successors.
Church	P	
Commercial Amusement Center	S	



Building Envelope Standards – Building Height

■ Building Height

- Regulating plan establishes the maximum building heights for an area
- Code includes maximum height dimensions in feet, which correspond with the building stories – the feet dimensions for the Southern Sub-districts are the same dimensions adopted for the Central Sub-districts
- Single family residential adjacency reflected in building heights shown on regulating plans

Building Stories	Maximum Building Height (in feet)	Maximum Height with Parapet/ Architectural Feature
1 story	15 See also II.E.3.e.i.g)	21 See also II.E.3.e.i.g)
2 stories	27	33
3 stories	39	45
4 stories	51	57
5 stories	63	69
6 stories	75	81
7 stories	87	93
8 stories	99	105
9 stories	111	117
10 stories	123	129



Building Envelope Standards – Building Ground Floor

- Maximum building ground floor
 - Ranges from 10,000 – 100,000 square feet
 - Regulates building footprint proportionality in relation to the properties and adjacent street types

Maximum Building Heights		
(A)	Principal Building fronting on:	
	Urban Mixed Use	4-10 stories max.
	West Spring Valley Road (Tunnel)	3-10 stories max. ²⁸
	Central	2-10 stories max. ²⁸
(B)	Ground Story	See Maximum Allowable Building Height Table in Subsection II.E.3.e.i.f)
(C)	Subsequent Stories	See Maximum Allowable Building Height Table in Subsection II.E.3.e.i.f)
(D)	Parapet Wall, or Architectural Feature	6' max.
(E)	Accessory Building	15' max.
Maximum Building Ground Floor		
(F)	Principal Building fronting on:	
	Urban Mixed Use	10,000 sq. ft.
	West Spring Valley Road (Tunnel)	15,000 sq. ft.
	Central (East side)	30,000 sq. ft.
	Central (West side)	40,000 sq. ft.
(G)	Accessory Building	15% of Principal Building ground floor



Building Envelope Standards – Building Placement

- Required Front Build-To Zones and Building Setbacks
 - Minimum/maximum range of 0-80 feet or minimum 10-foot setback
- Required Rear Building Setback
 - Range of 0-10 feet minimum
- Required Interior Side Building Setback
 - No maximum
- Building Frontage Buildout
 - Range of 40-70 percent minimum

Centennial Green - Building Placement by Street Type						
		<i>East Spring Valley Road</i>	<i>Centennial Boulevard</i>	<i>Greenville Avenue (North of Centennial Boulevard)</i>	<i>Greenville Avenue (South of Centennial Boulevard)</i>	<i>Buckingham Road (East of DART)</i>
Ⓐ	Required Front Build-To Zone	<ul style="list-style-type: none"> • Min. 0 feet • Max. 10 feet 	<ul style="list-style-type: none"> • Min. 10 feet • Max. 80 feet 	<ul style="list-style-type: none"> • Min. 0 feet • Max. 10 feet 	<ul style="list-style-type: none"> • See II.H.3.d.vi. 	<ul style="list-style-type: none"> • See II.H.3.d.vii.
Ⓑ	Required Rear Building Setback	<ul style="list-style-type: none"> • Min. 10 feet if abutting an alley • Min. 0 feet if abutting a property or a mews street 	<ul style="list-style-type: none"> • Min. 10 feet if abutting an alley • Min. 0 feet if abutting a property or a mews street 	<ul style="list-style-type: none"> • Min. 10 feet if abutting an alley • Min. 0 feet if abutting a property or a mews street 	<ul style="list-style-type: none"> • Min. 0 feet 	<ul style="list-style-type: none"> • Min. 0 feet
Ⓒ	Required Interior Side Building Setback	<ul style="list-style-type: none"> • No maximum setback 	<ul style="list-style-type: none"> • No maximum setback 	<ul style="list-style-type: none"> • No maximum setback 	<ul style="list-style-type: none"> • No maximum setback 	<ul style="list-style-type: none"> • No maximum setback
Ⓓ	Building Frontage Buildout	<ul style="list-style-type: none"> • Min. 40% of lot width 	<ul style="list-style-type: none"> • Min. 70% of lot width 	<ul style="list-style-type: none"> • Min. 50% of lot width 	<ul style="list-style-type: none"> • Not applicable 	<ul style="list-style-type: none"> • Not applicable



Building Envelope Standards – Parking

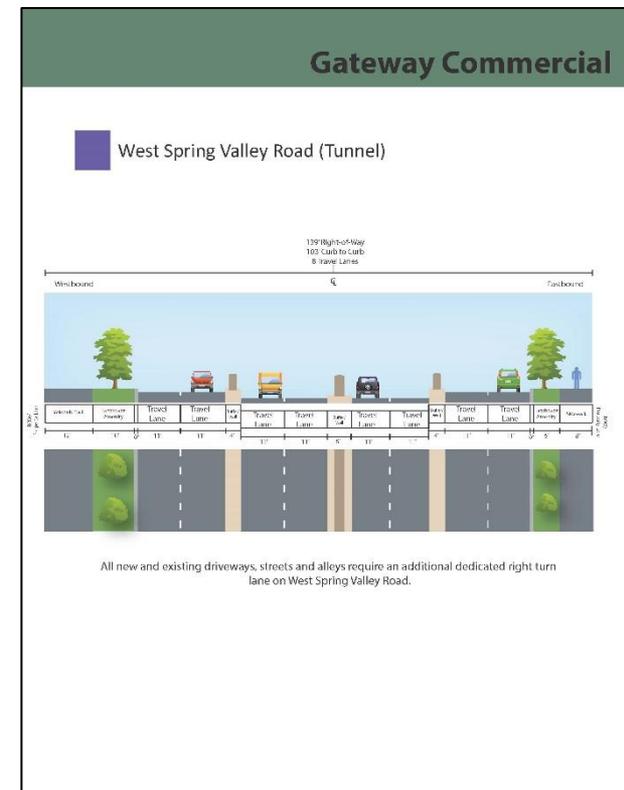
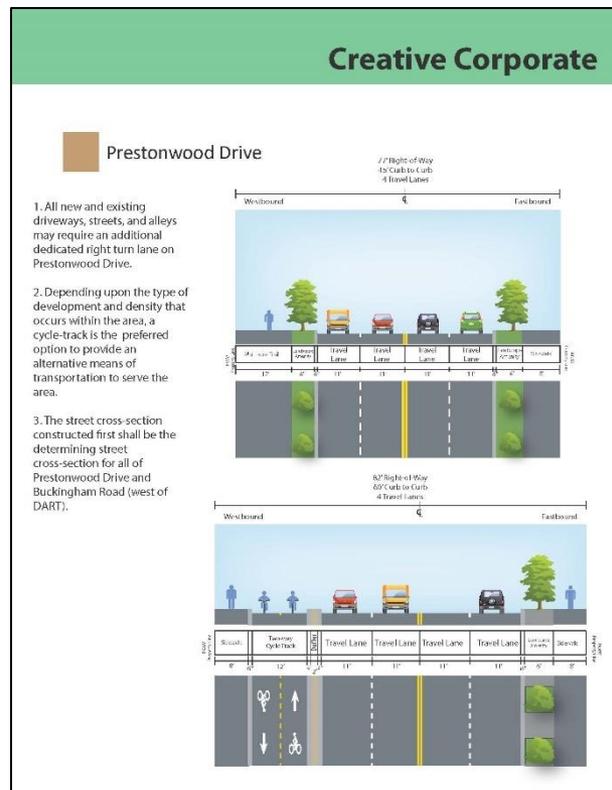
- Regulates parking garages and surface lots
- On-street and off-street
 - On-street based on applicable Street Typology and Streetscape Standards
- Shared parking preferable
- Establishes minimum requirements for off-street private parking
 - Based on use
 - Parking ratio requirements lessened to provide greater flexibility for leasing and reuse of properties
 - Supplemented by CZO Subsection 21-52(b), Off-Street Parking and the City Parking Design Manual

Use	Parking Required
Multi-Family	1.5 spaces per dwelling unit
Single Family Dwelling (attached and detached)	2 spaces per dwelling unit (see II.H.3.f.iii.e), Single Family Dwelling Parking)
Live/Work Unit	2 spaces per dwelling unit
Institutional and Assembly Uses	1 space per 3 persons accommodated
Hotel (Full Service)	1.25 spaces per guest room
Hotel (limited service or suite)	1 space per guest room
All Other Non-Residential Uses	1 space per 300 square feet



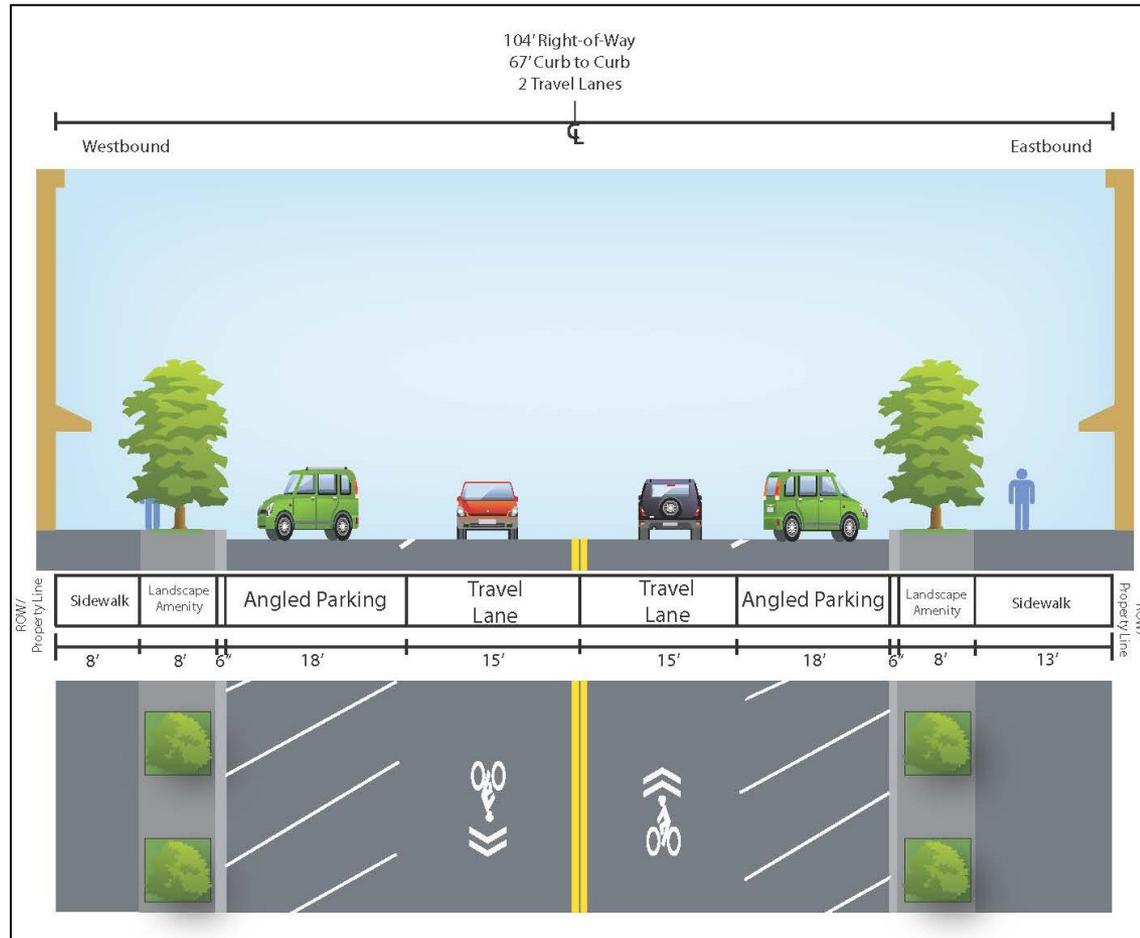
Street Typology and Streetscape Standards

- Establish a range of street types to support surrounding development context
- Provide street types that support development ranging from urban mixed use, to development with a more regional character



Street Typology and Streetscape Standards

- Regulate street cross-sections including number of lanes, on-street parking, street trees, lights, furniture, and sidewalks



Street Furnishings and Architectural Standards

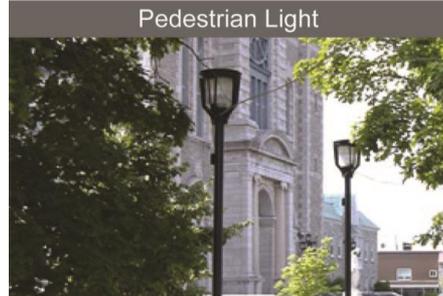
- Urban Design/Street Furnishings

- Consistent with the design character established within the West Spring Valley Corridor

- Architectural Standards

- Contemporary influenced materials and techniques for walls, roofs, windows and doors, and lighting and mechanical equipment
- Includes regulations for outdoor seating areas and fencing

Pedestrian Light



Bench



Trash Receptacle



Planter



Tree Grate

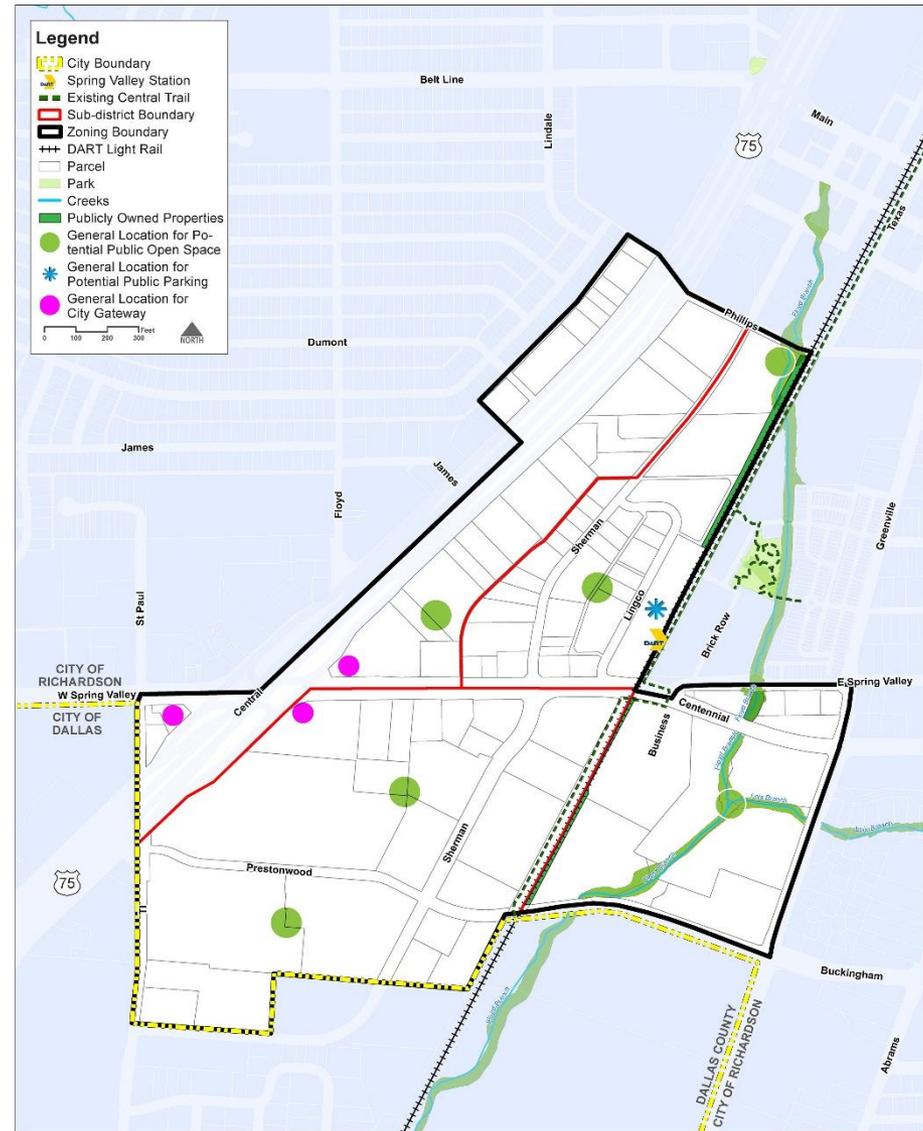


Bike Rack



Public Open Space and Public Parking

- Identifies existing publicly owned properties, general locations for potential public open space, public parking and city gateways
- Recommended percentages of open space are different by sub-district based on vision, intent and purpose of sub-district
- City gateways could potentially be large scale development, public art and/or entrance signage



October 2016



Other Regulations

- Mechanical, Service Areas and Utilities
 - Regulates loading docks, outdoor storage, roof-mounted equipment, dumpsters, ground-mounted equipment, wall-mounted equipment
- Thoroughfare Screening
 - Requires six foot masonry wall or living screen to buffer residential back yards from street
- Residential Zoning District Adjacency
 - Requires minimum six foot masonry screening wall between non-residential/multi-family uses and single family residential zoning districts when not separated by a local street



Signage, Administration and Definitions

■ Signage

- Regulates 24 types of signs permitted in sub-districts allowing signage consistent with pedestrian-oriented, as well as auto-oriented (i.e. US 75) environments

- Has been updated to reflect inclusion of Southern Sub-districts

■ Administration

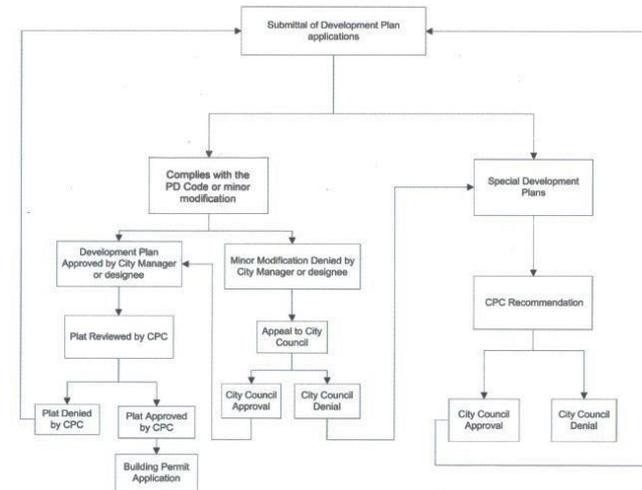
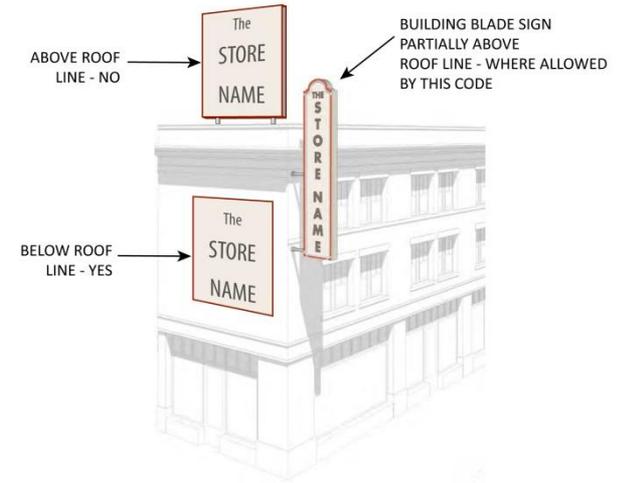
- Review and approval process based on ordinance criteria to permit streamlined review and approval

- Allows for administrative review and approval for most projects

- Non-conforming uses, structures, signs and site elements are regulated

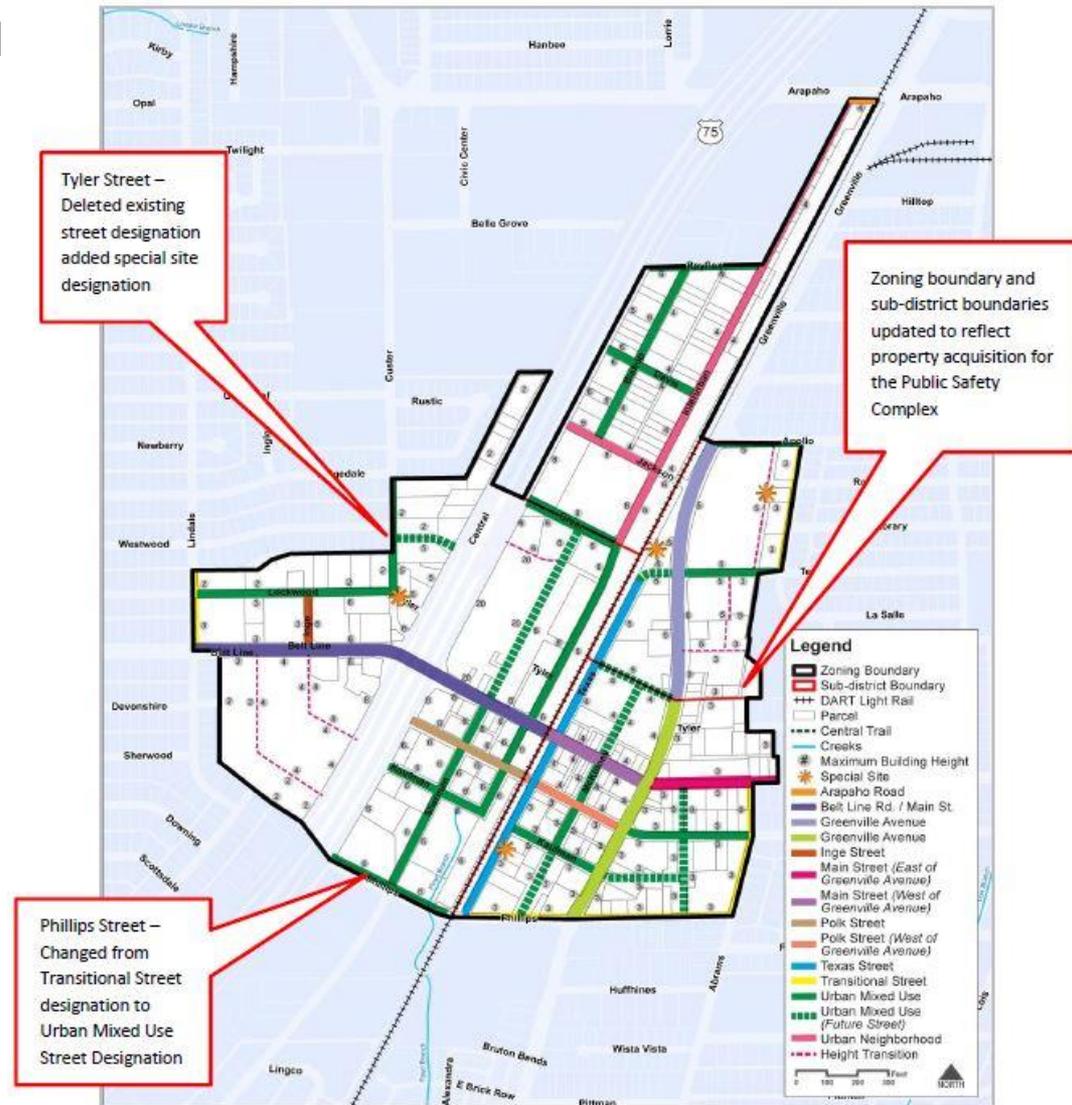
- In general, no expansion of non-conformities but may continue; exception: Code allows for limited expansion of non-conforming buildings provided certain criteria are met (see IV.H.5.)

- Comprehensive list of definitions not currently in Comprehensive Zoning Ordinance and/or where the definition needed to be updated



Updates to Central Sub-districts Regulating Plans

- Modified zoning boundary and sub-district boundaries to reflect property acquisition for Public Safety Complex
 - Affected Main Street and Chinatown sub-districts
 - Updated all maps in Code affected by boundary changes
- Tyler Street – deleted existing street designation and added special site designation
- Phillips Street – changed from Transitional Street to Urban Mixed Use Street Designation



Updates to Central Sub-districts Code

- Amendments/refinements identified including (but not limited to):
 - On-premise alcoholic beverage consumption
 - Belt Line Road/Main Street (West of Greenville Avenue) cross-section
 - Use charts
 - Mews streets and alleys
 - Building frontage build-out
 - Architecture materials
 - Mechanical equipment screening
 - Exemption for municipally-owned facilities
 - Future Streets
 - Signage
 - Non-conforming properties
 - Definitions
 - Scrivener's errors, references



Updates to Central Sub-districts Code

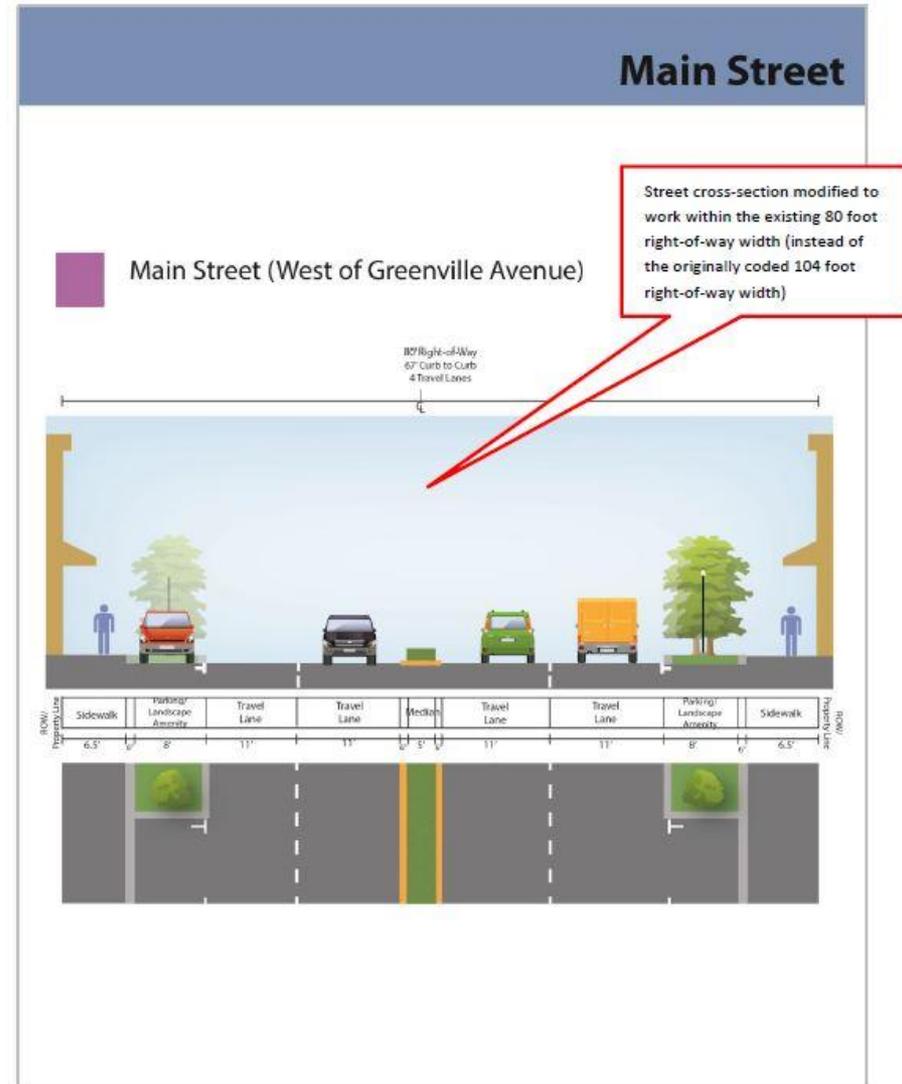
- Dumpsters*
 - Placement requirements modified
 - Clarified recycling allowed – must follow screening and placement standards for all dumpsters
- Outdoor seating areas/patios*
 - Established new regulations which regulate materials that may be used for patio enclosures
 - Clarified placement requirements
- Fences/Screening Walls and Street Walls*
 - Established new regulations which regulate placement and materials that may be used
 - Interurban Sub-district – contracting operations and limited motor vehicles uses only allowed to have fencing between the building and street right-of-way provided certain requirements are met

**Requested by property/business owners*



Updates to Central Sub-districts Street Typologies

- Modified the Main Street (West of Greenville Avenue) cross-section to work within existing 80-foot right-of-way
- Updated two street typologies in Central Place (Urban Mixed Use Street and Transitional Street) due to adding Southern Sub-districts
- Updated one street typology in Chinatown (Urban Mixed Use) due to street reference correction needed
- Added a new Mews Street cross-section



Next Steps

Code Adoption

- City Plan Commission, by a vote of 6-0, recommended approval of the Code as presented on September 20, 2016
- City Council conducts public hearing
 - May close public hearing on October 24, 2016 and take final action
 - May close public hearing on October 24, 2016 and table to November 14, 2016 to continue deliberations
 - May continue the public hearing and deliberations to November 14, 2016
- City Council's action final
 - Approve as submitted
 - Approve with additions or amend conditions/provisions within the Code
 - Deny (preferably without prejudice)



City Council Work Session

October 24, 2016



Image Source – City of Richardson



MAIN STREET / CENTRAL - REZONING INITIATIVE | PHASE III SOUTH END

Kimley»Horn
JACOBS
Richardson, Texas

City Council Public Hearing

October 24, 2016



Image Source – City of Richardson



MAIN STREET / CENTRAL - REZONING INITIATIVE | PHASE III SOUTH END

Agenda

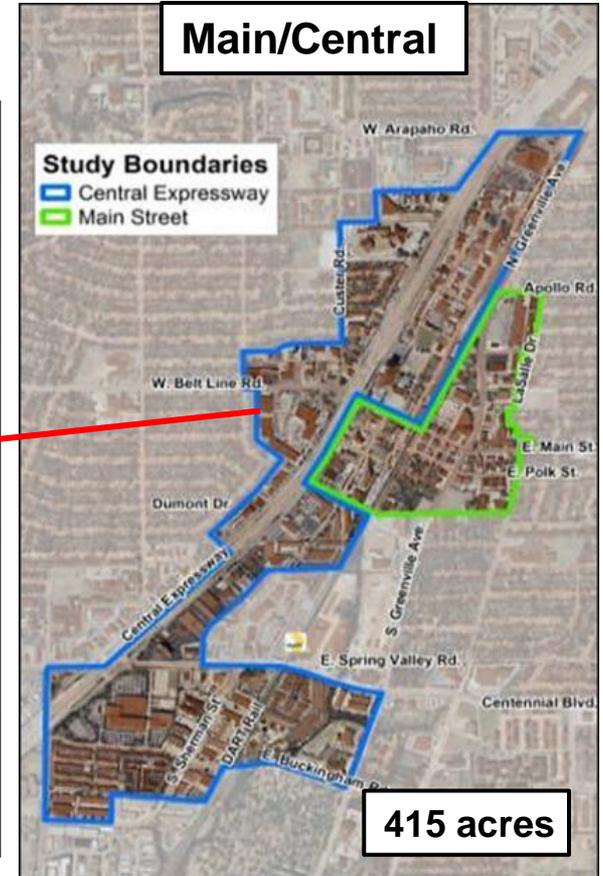
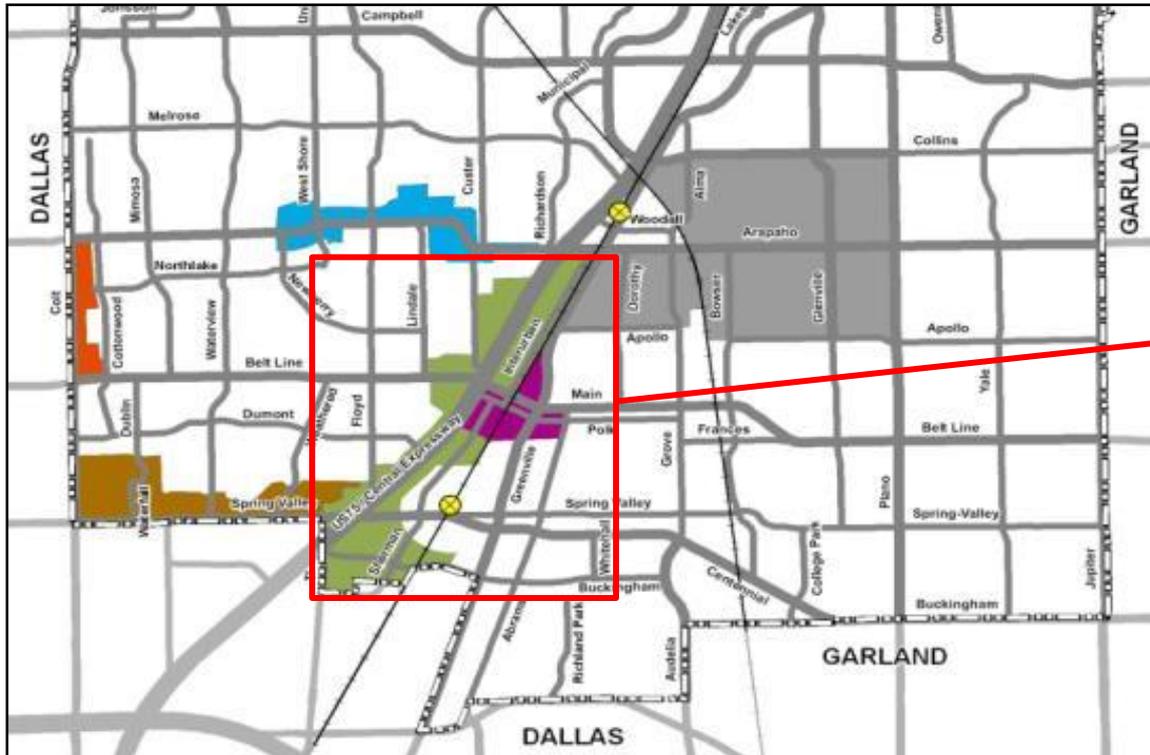
- Project Overview
- Community Input Process
- Key Direction from City Plan Commission and City Council
- Code Layout, Organization and Content
- Next Steps



Project Overview

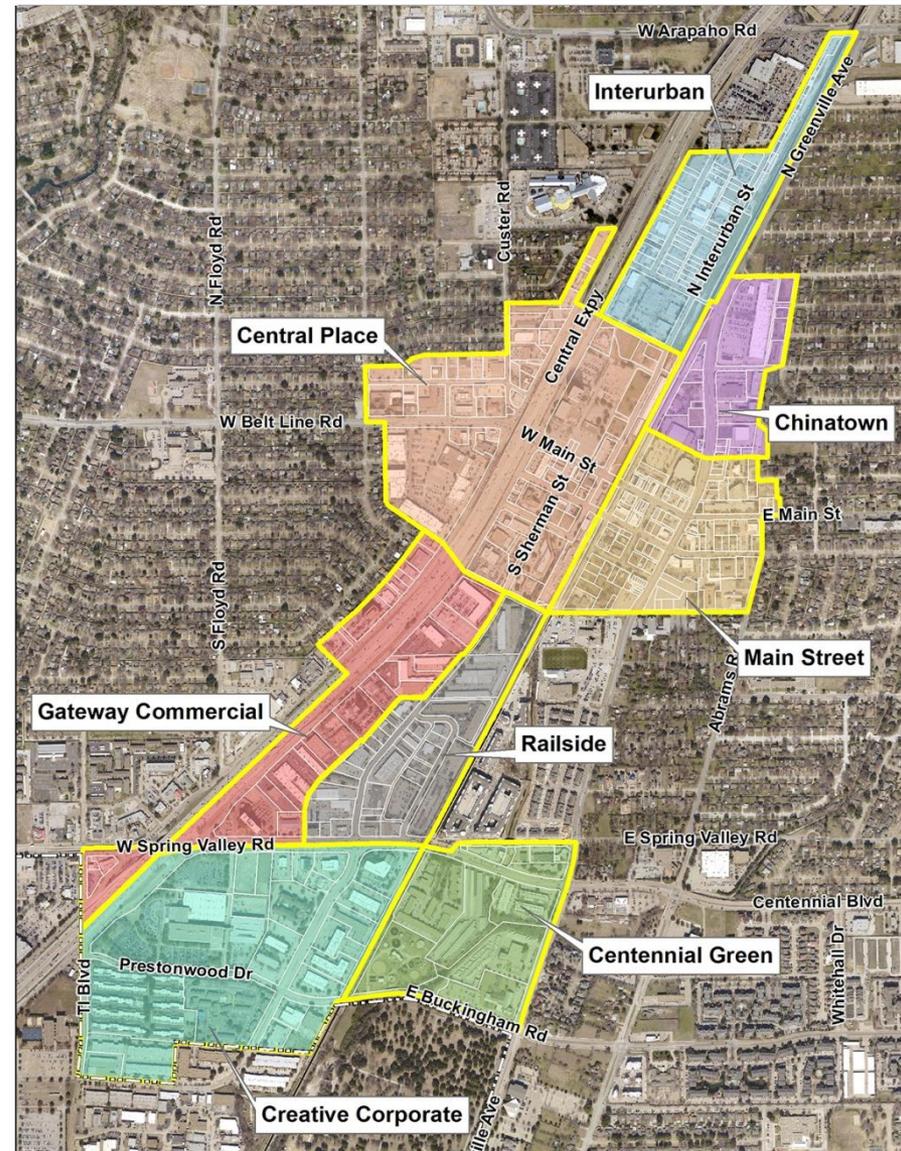
2009 Comprehensive Plan – Six Enhancement Areas

- West Spring Valley (Implementation)
- East Arapaho/Collins (Implementation)
- West Arapaho
- Coit
- Old Town/Main Street (Implementation)
- **Central** (Implementation – portion of corridor)



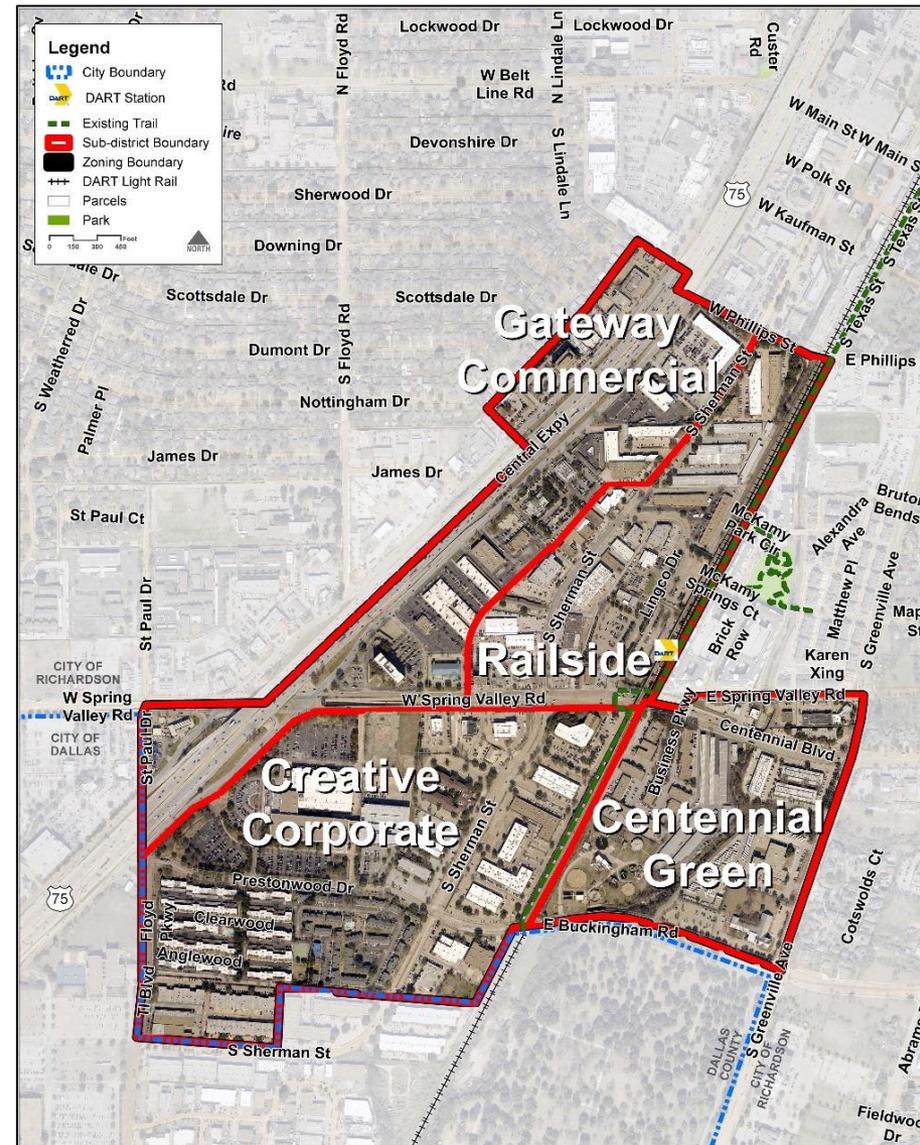
Main Street / Central Expressway Project

- **Phase I** created a vision for the study area and for each of 11 sub-districts based on community goals and market realities (2013)
- **Phase II** created a Form Based Code for the 4 Central Sub-districts (2015)
- **Phase III** focuses on the 4 Southern Sub-districts and will create a Form Based Code for these areas (2016); includes refinements to the existing Code in the Central Sub-districts



Phase III Southern Sub-district Changes

- **Gateway Commercial** – the southern boundary shifted north to Spring Valley Road
- **Creative Corporate** – the northern boundary shifted north to Spring Valley Road
- **McKamy Spring** – became “Centennial Green” and took in the “triangle” between Centennial Boulevard and Spring Valley Road but not the area west of the Spring Valley DART Station (both areas currently part of Spring Valley Station PD)
- **Trailside** – became “Railside” and extended south to Spring Valley Road to take in the area west of the Spring Valley DART Station (currently part of Spring Valley Station PD)



Vision – Gateway Commercial Sub-district

- Focuses on creating a commercial development “gateway” to Richardson
- Builds upon, supports and extends the vision established for the area west of US 75 in the West Spring Valley redevelopment strategy (i.e. mixed-use, mid-rise office and hotel uses in an urban form), including establishing a new business “address”



Vision – Creative Corporate Sub-district

- Focuses on attracting creative, innovative corporations to the corridor
- Supports public desire to attract businesses oriented to creativity, design, and “knowledge” workers



Vision – Centennial Green Sub-district

- Establishes future phases for ultimate build out of transit-oriented development at the nearby Spring Valley Station
- Provides support housing for Creative Corporate and Gateway Commercial sub-districts



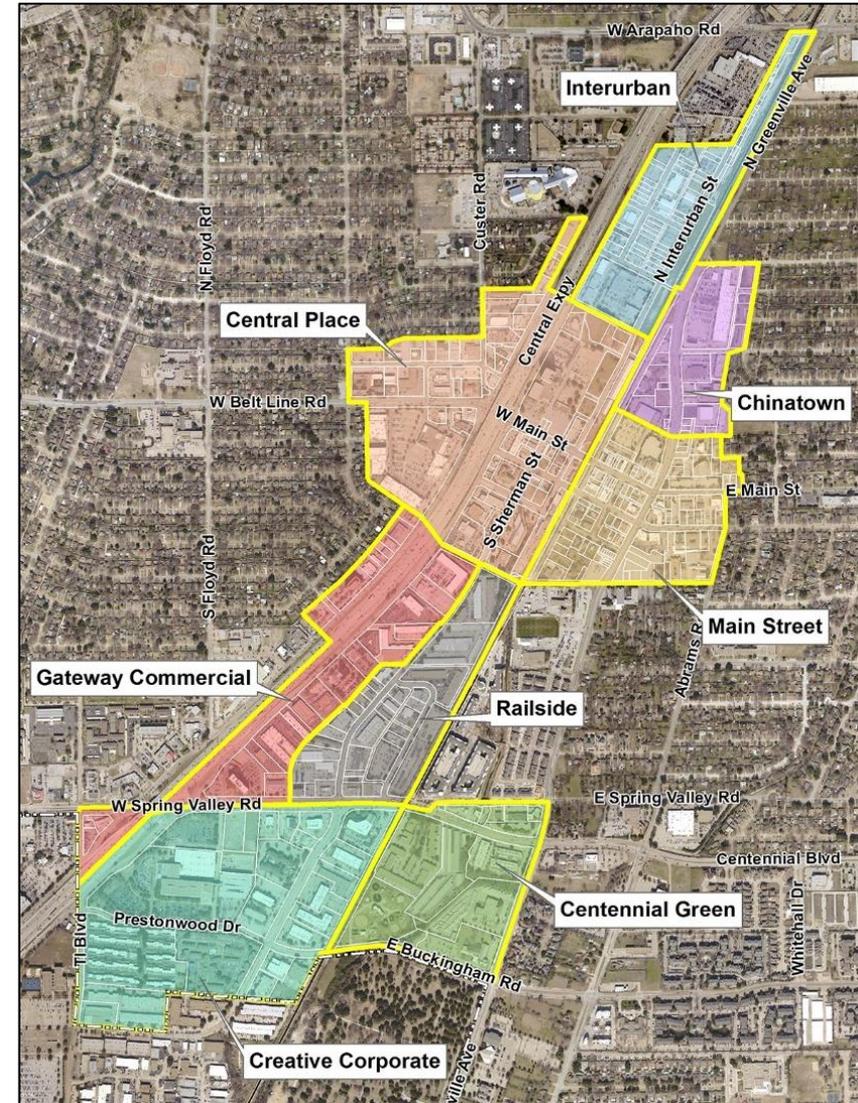
Vision – RAILSIDE Sub-district

- Positions Richardson as a community concerned with sustainability and the arts
- Focuses on adaptive reuse of existing industrial buildings
- New development (including residential and other transit supporting uses) is also envisioned with high quality urban character



Sub-districts for Discussion Tonight

- **Southern Sub-districts (New Code)**
 - Gateway Commercial
 - Creative Corporate
 - Centennial Green
 - Railside
- **Central Sub-districts (Refinements to Existing Code)**
 - Main Street
 - Central Place
 - Chinatown
 - Interurban



Community Input Process

Community Input

The project has used a variety of techniques to gain input from property and business owners, Richardson residents, and other stakeholders:

- Community and Stakeholder Involvement, Round 1
 - Property and business owner meetings on March 2 and 3, 2016 – 62 participants
 - Community Workshop on March 29, 2016 – 65 participants
 - Online Survey during April 2016 – 295 responses
- Community and Stakeholder Involvement, Round 2
 - Property and business owner meetings on July 11 and 12, 2016 – 27 participants
 - Community Workshop on July 12, 2016 – 35 participants
- Input from stakeholders was presented to City Plan Commission/City Council at key points in process for feedback and direction
- City Plan Commission, by a vote of 6-0, recommended approval of the Code as presented on September 20, 2016



Community Awareness

A variety of techniques have been used to promote this project throughout the community:

- Richardson Today
- Week In Review
- Project website
- Project Facebook page
- HOA Presidents Meeting
- Realtors Workshop
- Neighborhood Leadership Workshop
- Zoning signs posted throughout project area
- Letters to property and business owners, residents and other interested persons (workshops and stakeholder meetings)
- Public notification including courtesy letters
- Email distributions

Workshop seeks public input on Main Street/US 75 redevelopment

A community workshop to discuss the next phase for implementing the Main Street/Central Expressway Enhancement/Redevelopment Strategy will be held Tuesday, March 29 at 6:30-9 p.m. in the Grand Hall at the Richardson Civic Center/City Hall.

The workshop will focus on Phase 3 of the project that consists of rezoning four sub-districts located in the southern portion of the Corridor—Gateway Commercial, Trailside, Creative Corporate and McKamy Spring. Complete new land use and development regulations will be created for the sub-districts, incorporating the sub-districts into the Main Street/Central Expressway Form Based Code.

These four sub-districts are in an area that stretches along the east and west sides of US 75 from Phillips Street to south of Spring Valley Road.

Concurrently with rezoning the southern end of the Corridor, the City will be making refinements to the existing Code that apply to the central portion of the Corridor—the Interurban, Central Place, Chinatown and Main Street sub-districts—which was rezoned January 2015.

Throughout the course of this project, there will be more opportunities for input from property owners, business owners and other interested persons during stakeholder meetings, community workshops, and public hearings, as well as online.

Questions regarding the workshop and/or the overall strategy for redevelopment and revitalization of Main Street and the Central Expressway Corridor may be directed to Tina Firsens, Planning Projects Manager, at tina.firsens@cor.gov or mainstreetcentral@cor.gov. More information may also be found at www.cor.net/msc.



Key Direction from City Plan Commission and City Council

City Council and City Plan Commission Involvement

- City Council (CC) and City Plan Commission (CPC) involvement and direction
 - Briefings - January 4 and 19, 2016
 - Tours - April 30, 2016 and May 17, 2016
 - CC Work Sessions - May 2 and August 1, 2016
 - CPC Work Sessions - May 3 and August 2, 2016
 - CPC / CC Joint Work Session - June 20, 2016
- The following identifies key topics and direction provided by the CPC / CC for developing the draft Code



Key Topics

- City Plan Commission and City Council provided direction related to key topics for developing the draft Code:
 - Allowable uses (including auto-related uses)
 - Building heights
 - Development adjacent to US 75
 - Non-conforming properties
 - Future streets
 - Bicycle/pedestrian connections to DART Rail Line
 - Removal of development caps
 - Belt Line Road/Main Street (West of Greenville Avenue) cross-section
 - Loading operations within streets



Code Layout, Organization and Content

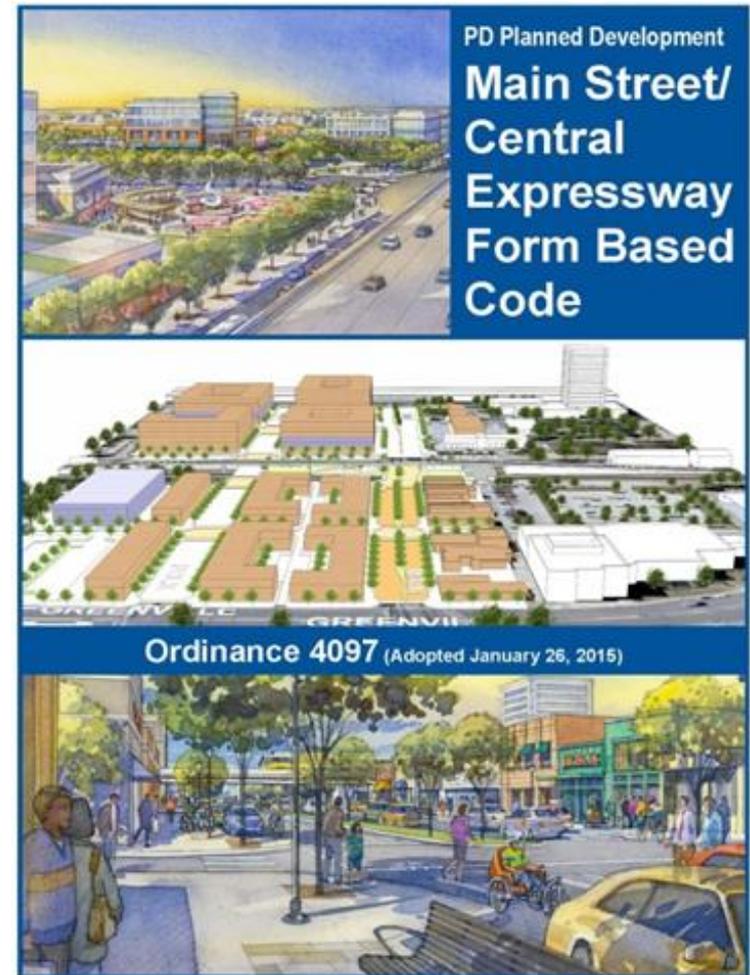
Form Based Code Coverage

- Covers eight sub-districts in Richardson identified in the *Main Street/Central Expressway Study*:
- Southern Sub-districts (**new Code**)
 - Gateway Commercial
 - Creative Corporate
 - Railside (formerly *Trailside*)
 - Centennial Green (formerly *McKamy Spring*)
- Central Sub-districts (**refinements to existing Code**)
 - Interurban
 - Central Place
 - Chinatown
 - Main Street



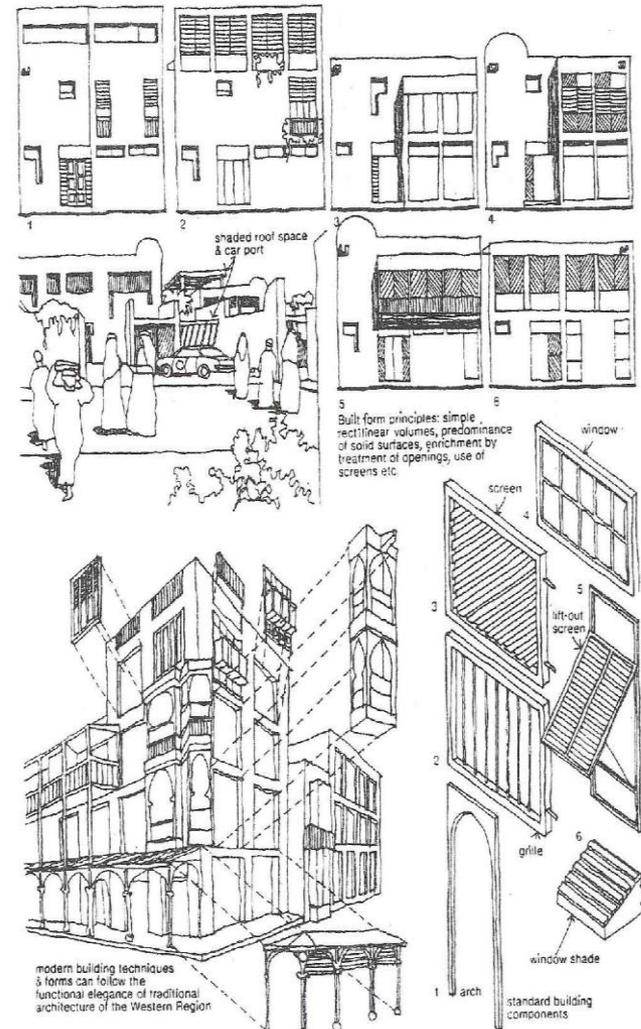
Working Assumptions for Southern Sub-districts

- Continue to use same Code format for Southern Sub-districts that was previously approved for Central Sub-districts
- Overall, a consistent use list will apply to properties in each sub-district; however, may limit uses in certain instances
- Continue to use the same approach for regulating non-conforming uses, structures, signs and site elements
- City Council and City Plan Commission concurred with these working assumptions (June 2016)



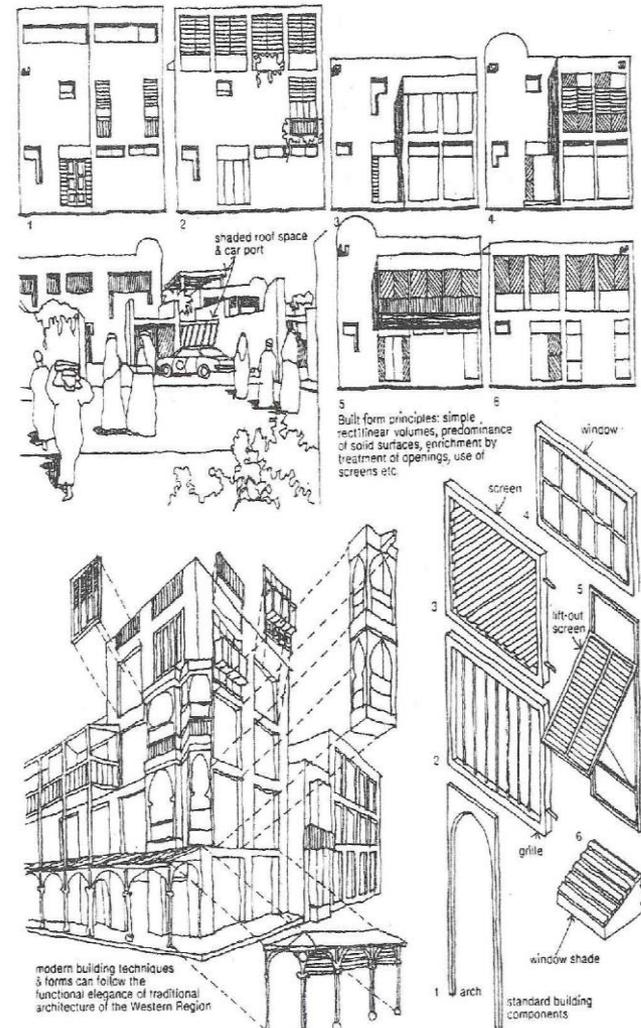
Form Based Code Components

- Overview of Code
- Sub-districts
- Signage
- Administration
- Definitions
- Appendices



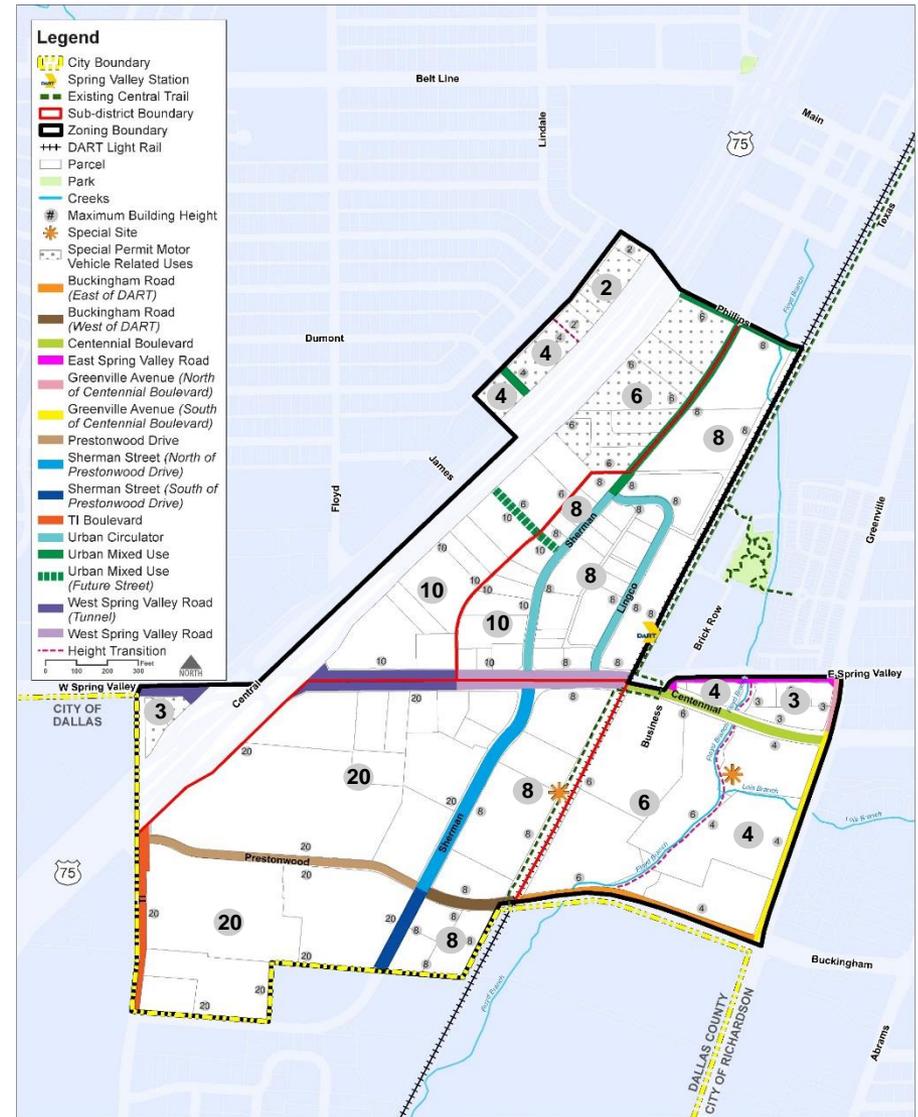
Form Based Code Components

- Each Sub-district includes:
 - Introduction
 - Regulating Plan
 - Public Open Space Plan
 - Public Parking Plan (where applicable)
 - Building and Envelope Standards
 - Street Typology and Streetscape Standards
 - Architectural Standards
 - Mechanical, Service Areas and Utilities
 - Thoroughfare Screening (where applicable)
 - Residential Zoning District Adjacency (where applicable)
 - Signage



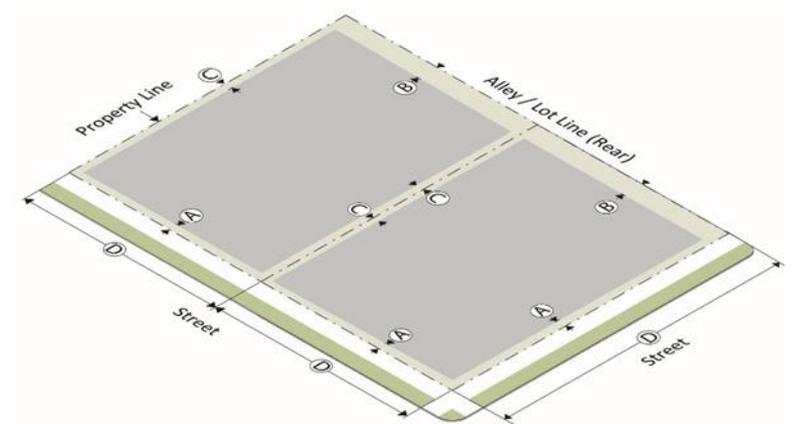
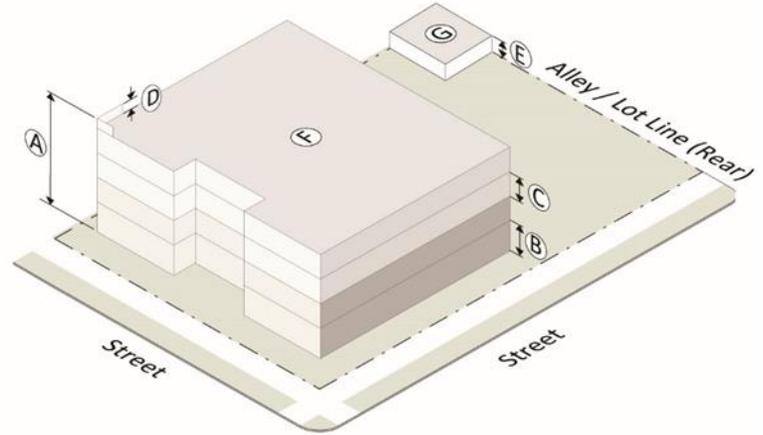
Overall Sub-district Regulating Plan – Southern Sub-districts

- Identifies how all 4 Southern Sub-districts relate to each other
- Shows the street type designations and how the streets relate to each other
- Basis for which all other Code standards are applied
- Shows maximum building heights and land use area restrictions



Building Envelope Standards

- Establishes a list of allowable uses for each sub-district
- Establishes lot development standards based on street frontage for:
 - Building height
 - Maximum building ground floor
 - Required front build-to zones or minimum front yard building setbacks
 - Required rear and interior setbacks
 - Building frontage buildout
 - Parking



Building Envelope Standards – Schedule of Uses

- Use lists specific to each sub-district based on **intent** and **purpose** of sub-district
- Uses are allowed throughout each sub-district (unless otherwise specified) by right or with a special permit
- Some uses include supplemental use requirements to further regulate location and type of use

Railside Use Type	How Permitted P – Permitted S – Special Permit	Supplemental Use Requirements
Residential		
Multi-Family	P	Minimum Density of forty dwelling units per acre.
Townhome	P	Minimum Density of ten dwelling units per acre.
Non-Residential		
Adult Day Care	S	
Antenna, Accessory	P	See Article XXII-E, Supplemental Regulations for Certain Uses, Richardson Comprehensive Zoning Ordinance, as amended or its successors.
Antenna, Commercial	P	
Antenna, Commercial, In Excess of Three Antennas	S	
Antenna, Freestanding	S	
Antenna, Mounted	P	
Art Gallery	P	
Assisted Living Facility	S	
Bakery	P	
Bank or Financial Institution	P	
Barber or Beauty Salon	P	Skin and nail care as accessory uses only.
Beer and Wine Package Sales - 75% or more Revenue from Sales	S	
Beer and Wine Package Sales – Less than 75% Revenue from Sales	P	
Catering Service	P	
Child Care Center	S	See Article XXII-E, Supplemental Regulations for Certain Uses, Richardson Comprehensive Zoning Ordinance, as amended or its successors.
Church	P	
Commercial Amusement Center	S	



Building Envelope Standards – Building Height

■ Building Height

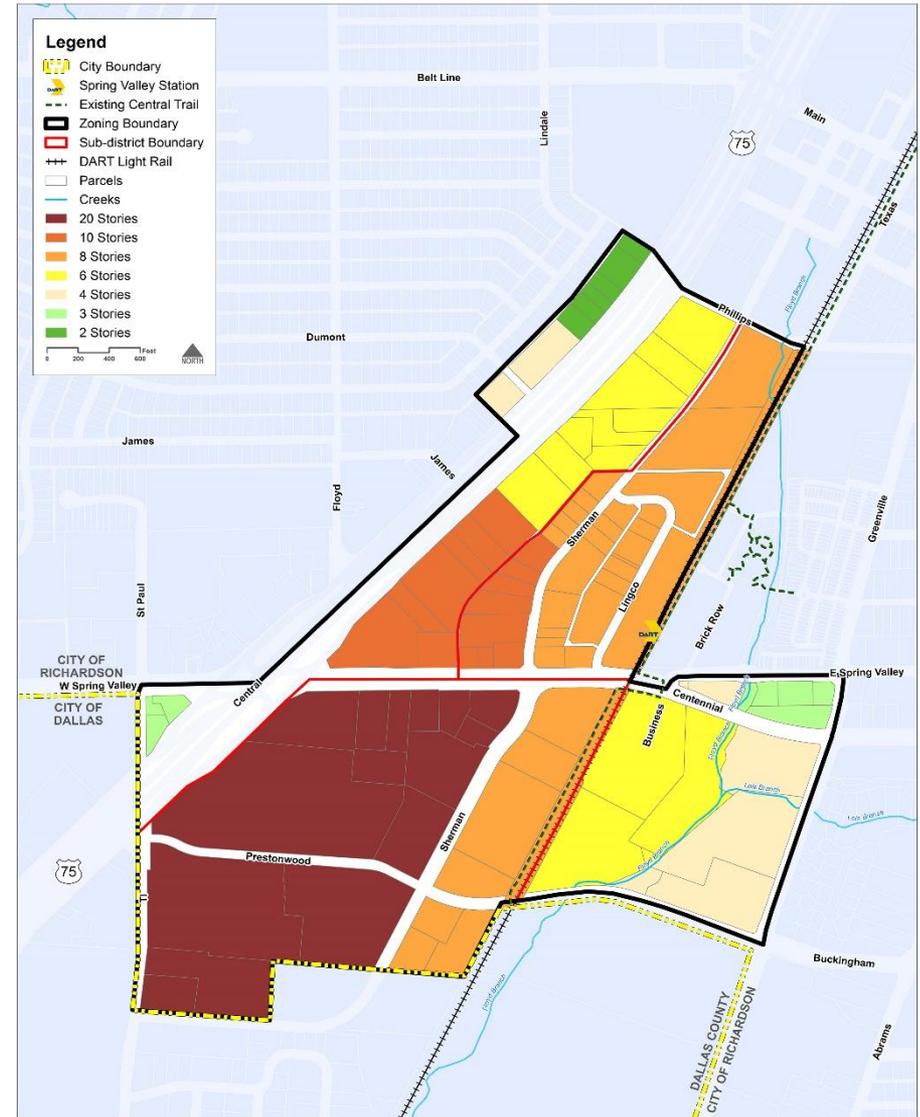
- Regulating plan establishes the maximum building heights for an area
- Code includes maximum height dimensions in feet, which correspond with the building stories – the feet dimensions for the Southern Sub-districts are the same dimensions adopted for the Central Sub-districts
- Single family residential adjacency reflected in building heights shown on regulating plans

Building Stories	Maximum Building Height (in feet)	Maximum Height with Parapet/ Architectural Feature
1 story	15 See also II.E.3.e.i.g)	21 See also II.E.3.e.i.g)
2 stories	27	33
3 stories	39	45
4 stories	51	57
5 stories	63	69
6 stories	75	81
7 stories	87	93
8 stories	99	105
9 stories	111	117
10 stories	123	129



Building Envelope Standards – Maximum Building Heights

- Maximum allowed building heights range from 2 stories (adjacent to existing single family residential neighborhoods) to 20 stories on properties with a more regional context (furthest from existing single family residential neighborhoods)



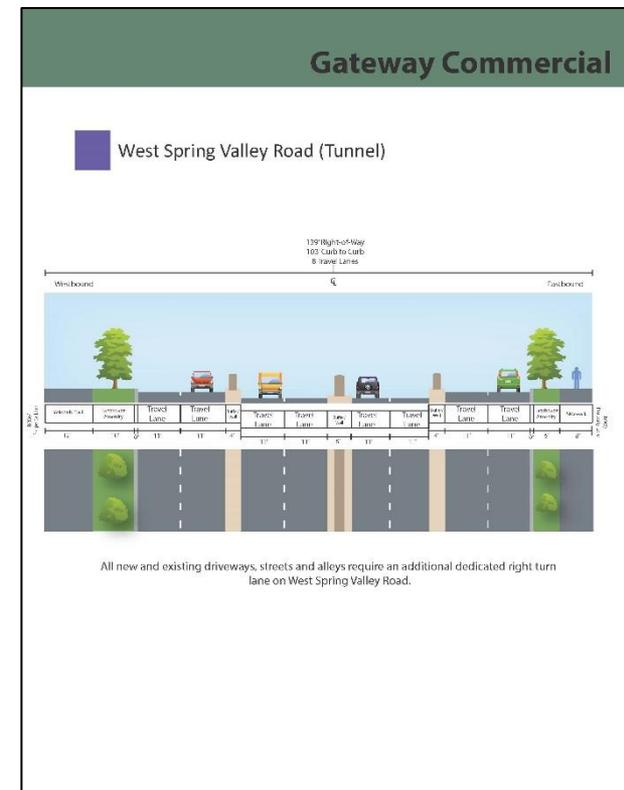
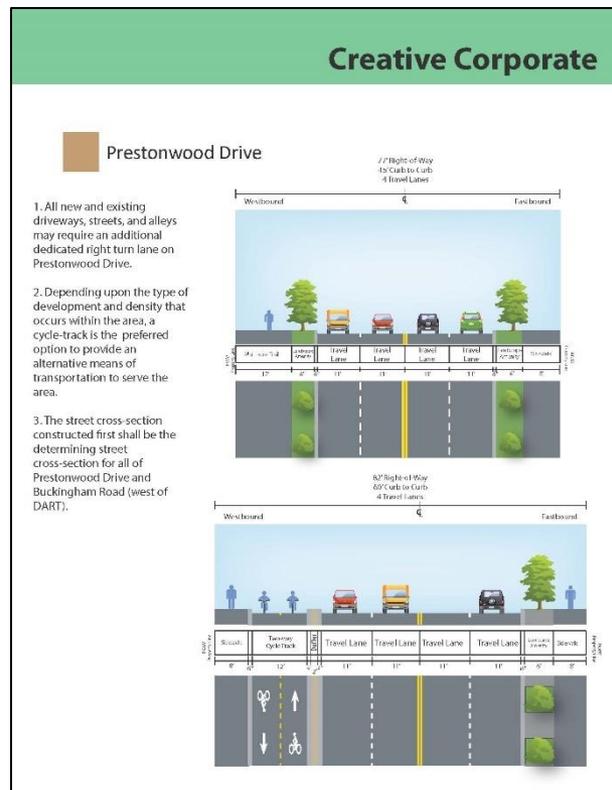
Building Envelope Standards – Parking

- Regulates parking garages and surface lots
- On-street and off-street
 - On-street based on applicable Street Typology and Streetscape Standards
- Shared parking preferable
- Establishes minimum requirements for off-street private parking
 - Based on use
 - Parking ratio requirements lessened to provide greater flexibility for leasing and reuse of properties
 - Supplemented by CZO Subsection 21-52(b), Off-Street Parking and the City Parking Design Manual

Use	Parking Required
Multi-Family	1.5 spaces per dwelling unit
Single Family Dwelling (attached and detached)	2 spaces per dwelling unit (see II.H.3.f.iii.e), Single Family Dwelling Parking)
Live/Work Unit	2 spaces per dwelling unit
Institutional and Assembly Uses	1 space per 3 persons accommodated
Hotel (Full Service)	1.25 spaces per guest room
Hotel (limited service or suite)	1 space per guest room
All Other Non-Residential Uses	1 space per 300 square feet

Street Typology and Streetscape Standards

- Establish a range of street types to support surrounding development context
- Provide street types that support development ranging from urban mixed use, to development with a more regional character



Street Furnishings and Architectural Standards

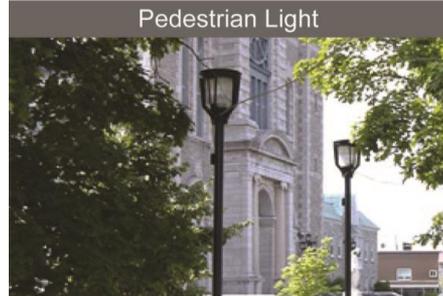
- Urban Design/Street Furnishings

- Consistent with the design character established within the West Spring Valley Corridor

- Architectural Standards

- Contemporary influenced materials and techniques for walls, roofs, windows and doors, and lighting and mechanical equipment
- Includes regulations for outdoor seating areas and fencing

Pedestrian Light



Bench



Trash Receptacle



Planter



Tree Grate

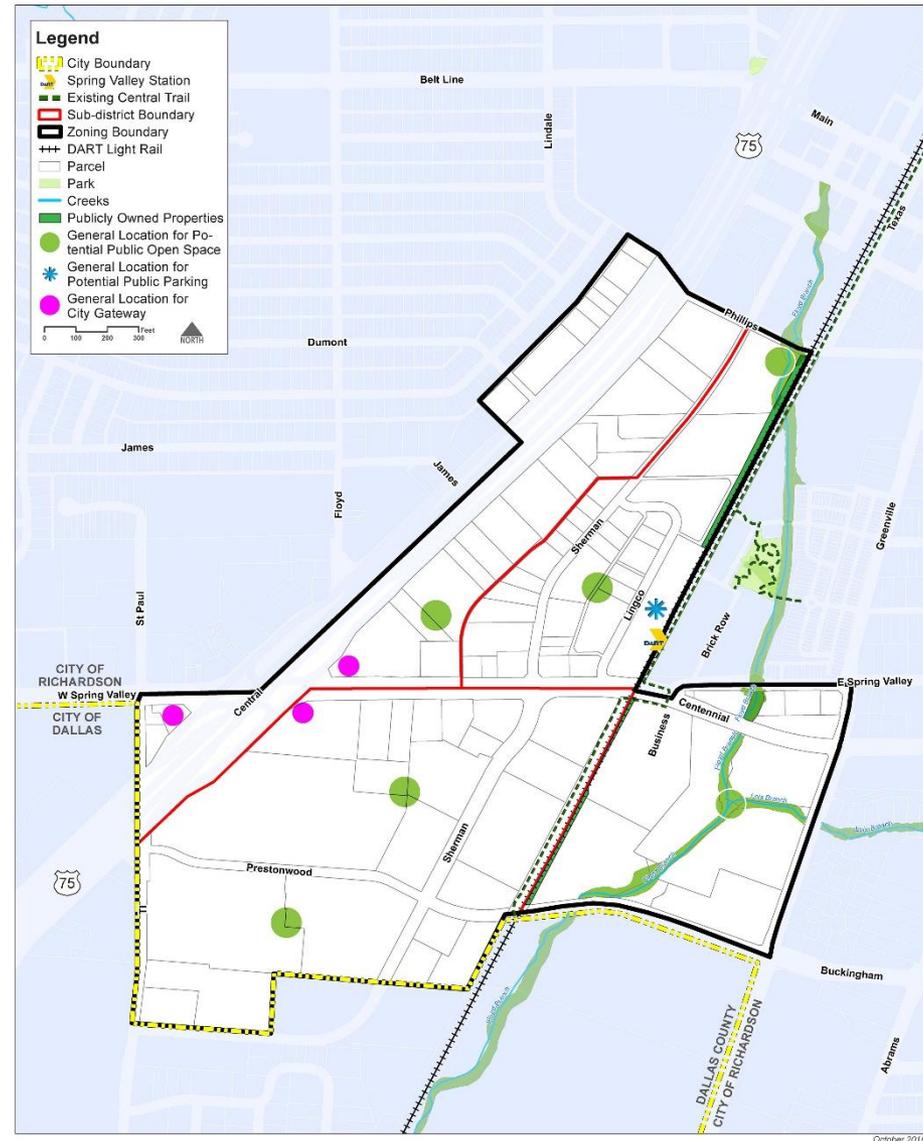


Bike Rack



Public Open Space and Public Parking

- Identifies existing publicly owned properties, general locations for potential public open space, public parking and city gateways
- Recommended percentages of open space are different by sub-district based on vision, intent and purpose of sub-district
- City gateways could potentially be large scale development, public art and/or entrance signage



Other Regulations

- Mechanical, Service Areas and Utilities
 - Regulates loading docks, outdoor storage, roof-mounted equipment, dumpsters, ground-mounted equipment, wall-mounted equipment
- Thoroughfare Screening
 - Requires six foot masonry wall or living screen to buffer residential back yards from street
- Residential Zoning District Adjacency
 - Requires minimum six foot masonry screening wall between non-residential/multi-family uses and single family residential zoning districts when not separated by a local street



Signage, Administration and Definitions

■ Signage

- Regulates 24 types of signs permitted in sub-districts allowing signage consistent with pedestrian-oriented, as well as auto-oriented (i.e. US 75) environments

- Has been updated to reflect inclusion of Southern Sub-districts

■ Administration

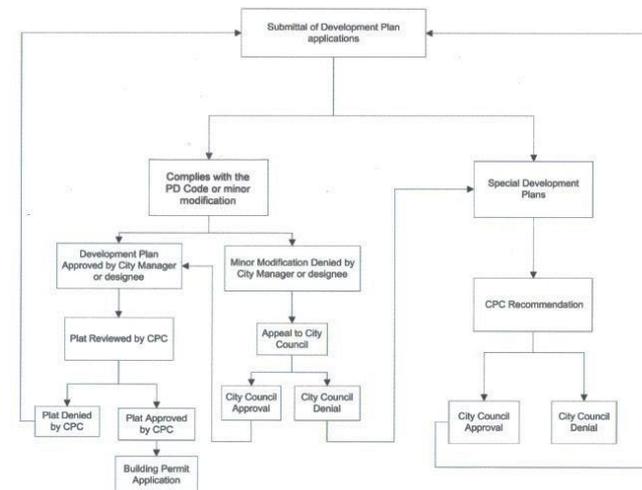
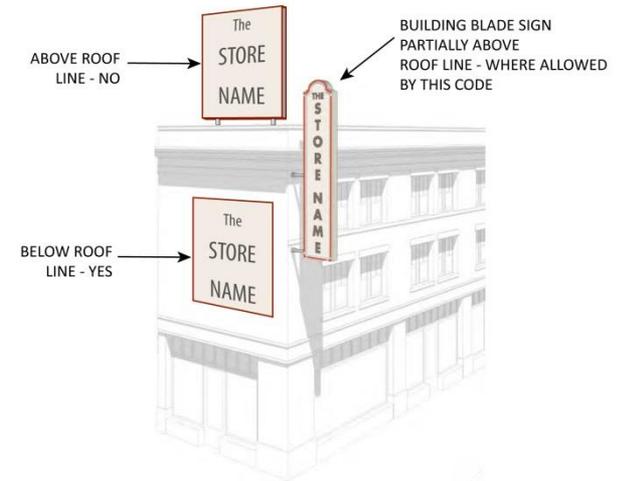
- Review and approval process based on ordinance criteria to permit streamlined review and approval

- Allows for administrative review and approval for most projects

- Non-conforming uses, structures, signs and site elements are regulated

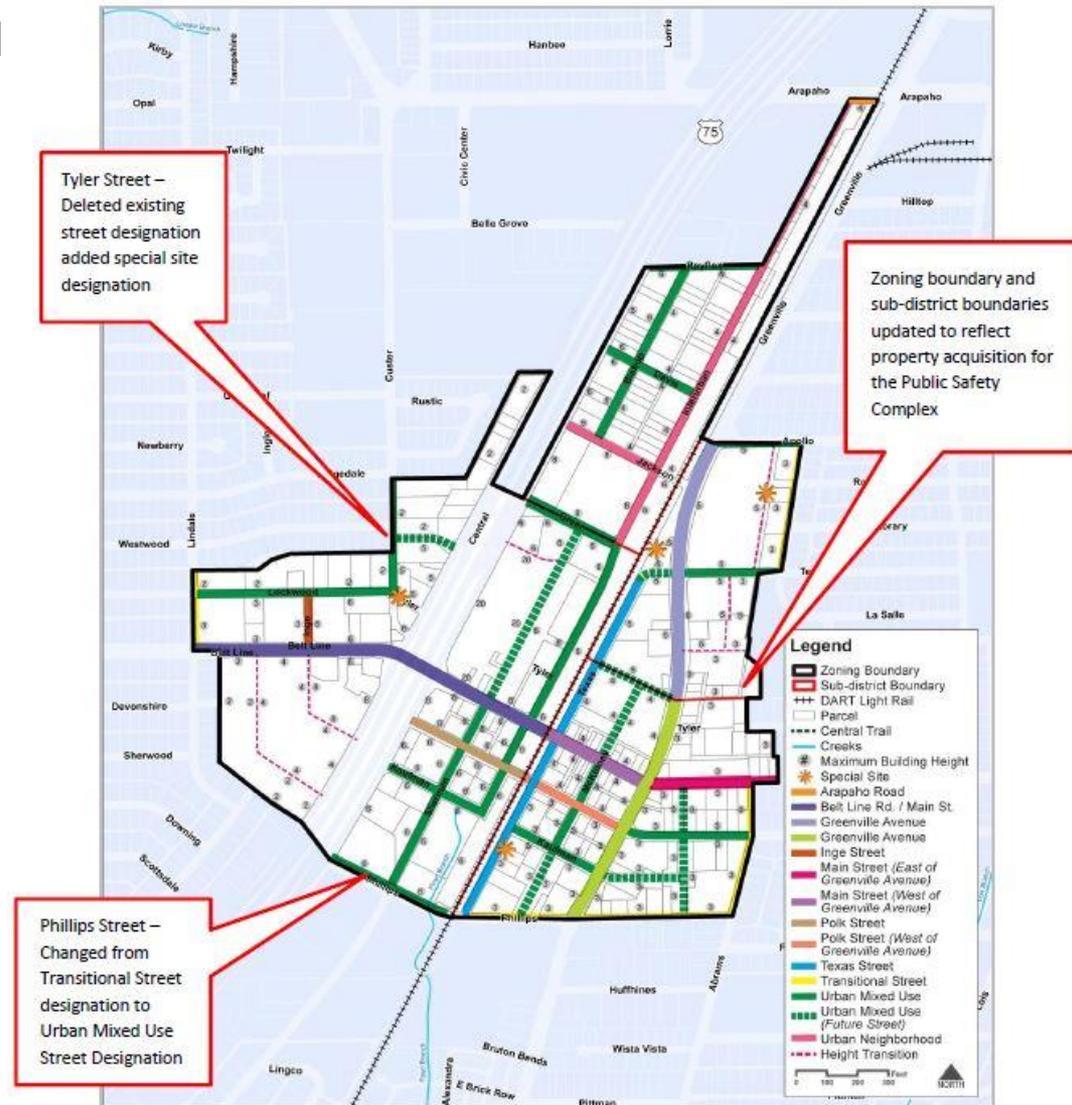
- In general, no expansion of non-conformities but may continue; exception: Code allows for limited expansion of non-conforming buildings provided certain criteria are met (see IV.H.5.)

- Comprehensive list of definitions not currently in Comprehensive Zoning Ordinance and/or where the definition needed to be updated



Updates to Central Sub-districts Regulating Plans

- Modified zoning boundary and sub-district boundaries to reflect property acquisition for Public Safety Complex
 - Affected Main Street and Chinatown sub-districts
 - Updated all maps in Code affected by boundary changes
- Tyler Street – deleted existing street designation and added special site designation
- Phillips Street – changed from Transitional Street to Urban Mixed Use Street Designation



Updates to Central Sub-districts Code

- Amendments/refinements identified including (but not limited to):
 - On-premise alcoholic beverage consumption
 - Belt Line Road/Main Street (West of Greenville Avenue) cross-section
 - Use charts
 - Mews streets and alleys
 - Building frontage build-out
 - Architecture materials
 - Mechanical equipment screening
 - Exemption for municipally-owned facilities
 - Future Streets
 - Signage
 - Non-conforming properties
 - Definitions
 - Scrivener's errors, references



Updates to Central Sub-districts Code

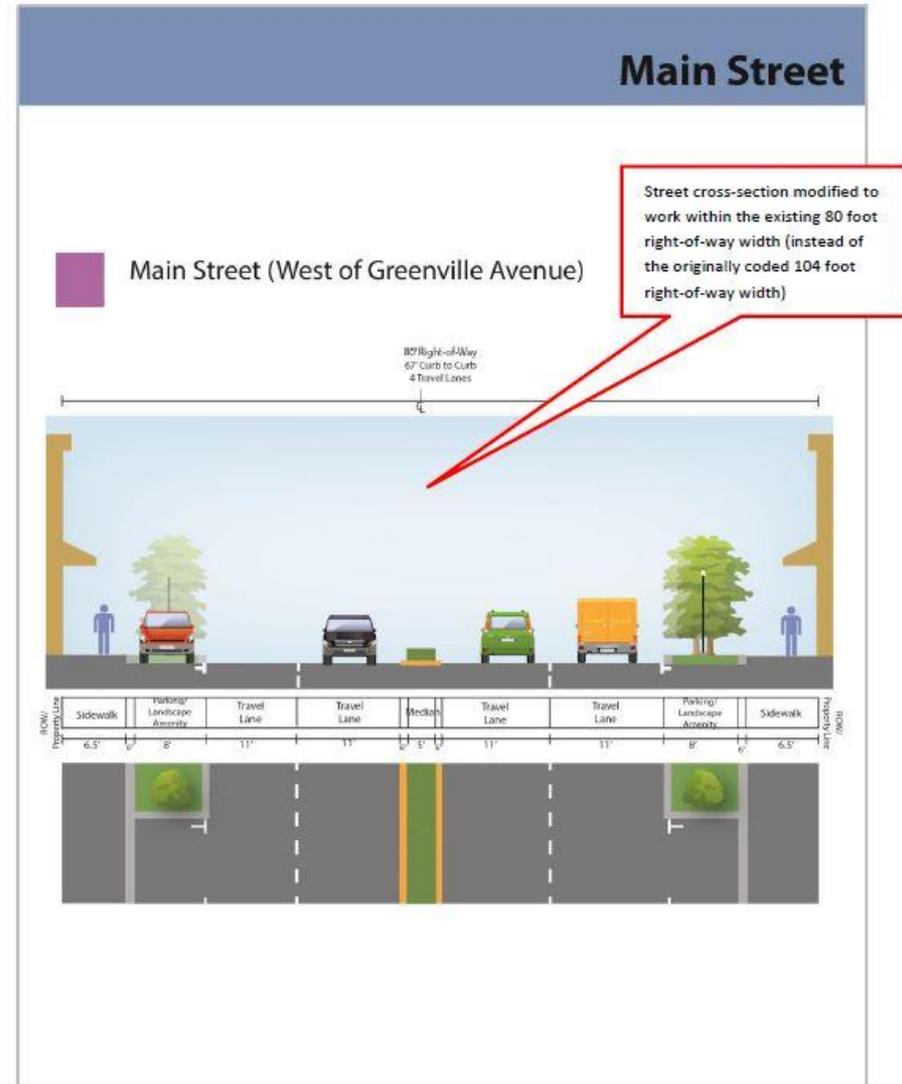
- Dumpsters*
 - Placement requirements modified
 - Clarified recycling allowed – must follow screening and placement standards for all dumpsters
- Outdoor seating areas/patios*
 - Established new regulations which regulate materials that may be used for patio enclosures
 - Clarified placement requirements
- Fences/Screening Walls and Street Walls*
 - Established new regulations which regulate placement and materials that may be used
 - Interurban Sub-district – contracting operations and limited motor vehicles uses only allowed to have fencing between the building and street right-of-way provided certain requirements are met

**Requested by property/business owners*



Updates to Central Sub-districts Street Typologies

- Modified the Main Street (West of Greenville Avenue) cross-section to work within existing 80-foot right-of-way
- Updated two street typologies in Central Place (Urban Mixed Use Street and Transitional Street) due to adding Southern Sub-districts
- Updated one street typology in Chinatown (Urban Mixed Use) due to street reference correction needed
- Added a new Mews Street cross-section



Next Steps

Code Adoption

- City Plan Commission, by a vote of 6-0, recommended approval of the Code as presented on September 20, 2016
- City Council conducts public hearing
 - May close public hearing on October 24, 2016 and take final action
 - May close public hearing on October 24, 2016 and table to November 14, 2016 to continue deliberations
 - May continue the public hearing and deliberations to November 14, 2016
- City Council's action final
 - Approve as submitted
 - Approve with additions or amend conditions/provisions within the Code
 - Deny (preferably without prejudice)



City Council Public Hearing

October 24, 2016



Image Source – City of Richardson



MAIN STREET / CENTRAL - REZONING INITIATIVE | PHASE III SOUTH END