

ORDINANCE NO. 4080

AN ORDINANCE OF THE CITY OF RICHARDSON, TEXAS, AMENDING THE COMPREHENSIVE ZONING ORDINANCE AND ZONING MAP OF THE CITY OF RICHARDSON, AS HERETOFORE AMENDED, SO AS TO GRANT A CHANGE IN ZONING FOR 54.5573 ACRES OF LAND DESCRIBED IN EXHIBIT "A" HERETO FROM PD PLANNED DEVELOPMENT AS ESTABLISHED BY ORDINANCE NO. 3805 TO PD PLANNED DEVELOPMENT FOR TRANSIT-ORIENTED MIXED USE DEVELOPMENT (CARUTH TRANSIT ORIENTED PLANNED DEVELOPMENT CODE -EAST); PROVIDING A SAVINGS CLAUSE; PROVIDING A REPEALING CLAUSE; PROVIDING A SEVERABILITY CLAUSE; PROVIDING FOR A PENALTY OF FINE NOT TO EXCEED THE SUM OF TWO-THOUSAND (\$2,000.00) DOLLARS FOR EACH OFFENSE; AND PROVIDING AN EFFECTIVE DATE. (ZONING FILE 14-17).

WHEREAS, the City Plan Commission of the City of Richardson and the governing body of the City of Richardson, in compliance with the laws of the State of Texas and the ordinances of the City of Richardson, have given requisite notice by publication and otherwise, and after holding due hearings and affording a full and fair hearing to all property owners generally and to all persons interested and situated in the affected area and in the vicinity thereof, the governing body, in the exercise of the legislative discretion, has concluded that the Comprehensive Zoning Ordinance and Zoning Map should be amended; **NOW THEREFORE**,

BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF RICHARDSON, TEXAS:

SECTION 1. That the Comprehensive Zoning Ordinance and Zoning Map of the City of Richardson, Texas, duly passed by the governing body of the City of Richardson on the 5th day of June, 1956, as heretofore amended ("the CZO"), be, and the same is hereby further amended so as to grant a change in zoning for a 54.5573 acre tract of land generally located at the northeast corner of US Highway 75 and Renner Road and more particularly described in Exhibit "A" attached hereto and made a part hereof for all purposes ("the Property") from PD Planned Development as established by Ordinance No. 3805 to PD Planned Development for Transit-Oriented Mixed Use Development (Caruth Transit Oriented Planned Development Code - East)("C-TOPD- East District").

SECTION 2. That the use and development of the Property shall be subject to the following:

1. The Property shall be used and developed in accordance with the applicable provisions of the CZO, and amended, and the use and development regulations set forth in the *Caruth Properties Transit Oriented Development Code - East*, attached hereto as Exhibit "B" and incorporated herein by reference ("the C-TOPD East Code"). To the extent of any irreconcilable conflict between the provisions of the CZO and the C-TOPD East Code, the provisions of the C-TOPD East Code shall control.
2. Notwithstanding anything to the contrary in the C-TOPD East Code or other provisions of this Ordinance, the number of multi-family units to be constructed on the 86.0 acres of land described on Exhibits "A-1" and "A-2" of Ordinance Number 3805, adopted February 14, 2011, which land includes the Property, shall not exceed 1,250 as provided in Section 2.2 of Ordinance No. 3805.
3. A revised Traffic Impact Analysis (TIA) must be submitted and approved by the City prior to:
 - A. approval of an ordinance amending the development regulations set forth in this Ordinance which will result in (i) an increase in land use intensity or (ii) modifications to the mix of land uses on the Property as set forth in the C-TOPD East Code; or
 - B. issuance of a building permit by the City for construction, reconstruction, and/or remodeling of a building which, when completed, will result in the total area of office use space within the Property exceeding 1.5 million square feet; or
 - C. issuance of a certificate of occupancy for all or part of one or more existing buildings constructed on the Property which, if issued, will result in a change of use that will result in the total area of office use space within the Property exceeding 1.5 million square feet.

SECTION 3. That the Property shall be used in the manner and for the purpose provided for by the Comprehensive Zoning Ordinance of the City of Richardson, Texas, as heretofore amended, and subject to the aforementioned special conditions.

SECTION 4. That all other provisions of the ordinances of the City of Richardson in conflict with the provisions of this ordinance be, and the same are hereby, repealed, and all other provisions of the ordinances of the City of Richardson not in conflict with the provisions of this ordinance shall remain in full force and effect.

SECTION 5. That should any sentence, paragraph, subdivision, clause, phrase or section of this Ordinance be adjudged or held to be unconstitutional, illegal or invalid, the same shall not affect the validity of this Ordinance as a whole, or any part or provision thereof other

than the part so decided to be invalid, illegal or unconstitutional, and shall not affect the validity of the Comprehensive Zoning Ordinance as a whole.

SECTION 6. That an offense committed before the effective date of this ordinance is governed by prior law and the provisions of the Comprehensive Zoning Ordinance, as amended, in effect when the offense was committed and the former law is continued in effect for this purpose.

SECTION 7. That any person, firm or corporation violating any of the provisions or terms of this Ordinance shall be subject to the same penalty as provided for in the Comprehensive Zoning Ordinance of the City of Richardson, as heretofore amended, and upon conviction shall be punished by a fine not to exceed the sum of Two Thousand Dollars (\$2,000) for each offense; and each and every day such violation shall continue shall be deemed to constitute a separate offense.

SECTION 8. That this Ordinance shall take effect immediately from and after its passage and the publication of the caption, as the law and charter in such case provide.

DULY PASSED by the City Council of the City of Richardson, Texas, on the 27th day of October, 2014.



APPROVED:

Laura Mann
MAYOR

APPROVED AS TO FORM:

Peter G. Smith
CITY ATTORNEY
(kbl:10-23-14:68813)

CORRECTLY ENROLLED:

Aimee Nemece
CITY SECRETARY

EXHIBIT "A"

LEGAL DESCRIPTION

ZF 14-17 (Northeast Corner US Highway 75 and Renner Road

Being a tract or parcel of land situated in the City of Richardson, Collin County, Texas, being part of the John G. Vance Survey, Abstract 938 and being part of a 106.527 acre tract of land conveyed to W.W. Caruth, Jr. as recorded in Volume 462, Page 121 of the Deed Records of Collin County, Texas, and being more particularly described as follows:

BEGINNING at an iron rod for corner at the intersection of the northerly line of Renner Road (as dedicated by deed to the City of Richardson, Texas in Volume 1853, Page 177, of the Deed Records of Collin County, Texas), and the westerly right-of-way line of the H. & T.C. Railroad (50 feet from centerline)

THENCE North 89 degrees 51'00" West along said northerly line of Renner Road a distance of 216.46 feet along said northerly line of Renner Road a distance of 216.46 feet to an iron rod for angle point;

THENCE North 89 degrees 52'36" West continuing along said northerly line of Renner Road a distance of 7.78 feet to an iron rod for angle point;

THENCE North 78 degrees 19'00" West continuing along the northerly line of Renner Road a distance of 36.15 feet to an iron rod for angle point;

THENCE North 89 degrees 51'00" West continuing along the northerly line of Renner Road (40 feet from centerline) a distance of 57.98 feet to a point for corner in the centerline of Spring Creek;

THENCE North 31 degrees 41'00" East along the centerline of said Spring Creek a distance of 4.95 feet to a point for corner in the northerly line of Renner Road;

THENCE North 89 degrees 47'27" West along the northerly line of said Renner Road a distance of 241.17 feet to a point for corner;

THENCE North 00 degrees 12'33" East along the northerly line of said Renner Road a distance of 17.00 feet to a point for corner;

THENCE North 89 degrees 47'27" West along the northerly line of said Renner Road a distance of 389.83 feet to a point for corner in the northeasterly cut-off line between the northerly line of said Renner Road and the easterly line of U.S. Highway No. 75;

THENCE North 40 degrees 53'57" West along said cut-off line a distance of 9.32 feet to a point in the easterly line of U.S. Highway No. 75;

THENCE northerly along said easterly line of U.S. Highway No. 75 the following described courses:

North 06 degrees 48'30" East a distance of 220.32 feet to an angle point;
North 12 degrees 07'22" East a distance of 316.33 feet to an angle point;
North 25 degrees 12'56" East a distance of 151.88 feet to an angle point;
North 13 degrees 02'16" East a distance of 265.07 feet to an angle point;
North 06 degrees 34'24" West a distance of 155.33 feet to an angle point;
North 09 degrees 19'48" East a distance of 816.60 feet to an angle point;
North 48 degrees 29'00" East a distance of 98.09 feet to an angle point;
North 67 degrees 18'05" East a distance of 98.09 feet to a point on the non-tangent curve to the left;

THENCE easterly along said curve to the left having a radius of 1447.00 feet, a central angle of 14 degrees 01'43" feet, a tangent length of 178.04 feet, a chord bearing of North 86 degrees 37'57" East, and a chord length of 353.41 feet, for an arc distance of 354.29 feet to a point for corner;

THENCE North 83 degrees 04'09" East a distance of 129.06 feet to a point for corner in the southerly line of Texas Power and Light Company right-of-way (150 feet wide) as recorded in Volume 576, Page 395;

THENCE North 89 degrees 40'04" East along said southerly line of Texas Power and Light Company right-of-way a distance of 779.51 feet to an iron rod for corner in the westerly line of H. & T. C. Railroad right-of-way (100 feet wide);

THENCE South 21 degrees 02'00" West along said westerly line of the H. & T. C. Railroad right-of-way a distance of 2235.09 to the **POINT OF BEGINNING** containing 54.5573 acres, more or less.

CARUTH PROPERTIES TRANSIT ORIENTED PLANNED DEVELOPMENT CODE - EAST

October 27, 2014



GFF Planning



Gateway
Planning Group

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Section 1. Purpose and Intent:

The purpose of the Caruth Properties Transit Oriented Planned Development Code - East (hereinafter, “the C-TOPD Code - East”) is to support development of a pedestrian-oriented, mixed-use urban development environment, with convenient access to rail transit, shopping, employment, housing, and regional retail services. The goal of the Caruth Properties Transit Oriented Planned Development – East District (hereinafter, “the C-TOPD - East District”) is to promote an efficient, compact land use pattern; encourage pedestrian activity; reduce the reliance on private automobiles; and provide a more functional and attractive community through the use of recognized principles of urban design.

- 1.1 Economic Development – The C-TOPD - East District and corresponding standards are created to support economic development, sustainable tax base, and job creation by establishing adjacency predictability of private development that supports and leverages investment in and around the C-TOPD - East.
- 1.2 Implement the Design Goals of the Regulating Plan – The objective of the C-TOPD - East District is to foster a major regional employment center with significant regional retail and residential uses within convenient walking distance from the existing transit station and potential transfer station to the Cotton Belt rail line. The existing transit station provides a connection to the twelve (12) member cities of DART, including Dallas. Development within this area would be of high intensity, accommodating large scale office and retail users while providing for transitions to adjoining uses and neighborhoods.
- 1.3 Establish Specific Development Standards – The C-TOPD Code - East implements the vision for Transit Oriented Development along the DART rail corridor as established in the City’s Comprehensive Plan and Caruth Properties Regulating Plan, hereafter known as the Regulating Plan (Appendix A). The Regulating Plan shall provide guidance to property owners, developers, and the City on the form, character, and intensity of future development in the C-TOPD - East. Creation of different Character Zones within the C-TOPD - East District enables specific site and locational standards to be enumerated and applied. Clear graphic standards are provided for location, height, and building elements. Such standards promote sustainability, public welfare, walkable mixed use development, housing variety and transportation choice.

Section 2. Components of the Code:

- 2.1 This C-TOPD Code - East shall apply to the C-TOPD - East District unless otherwise specified in this Code. Development of property within the C-TOPD - East shall comply with the respective development standards set forth in the C-TOPD Code - East. The components of this C-TOPD - East consist of:
 - 2.1.1 C-TOPD - East District Regulating Plan: The C-TOPD - East District Regulating Plan, hereafter known as the “Regulating Plan”, is its official zoning map. It identifies the applicable character zones within the C-TOPD - East District including:
 - i. Character Zones – The C-TOPD - East is divided into different “Character Zones”. A Character Zone creates a distinct urban form within that Zone which is different from urban forms in other Character Zones. Each Character Zone shall establish use and development standards including height, bulk, building and parking location, and functional design. The Regulating Plan classifies all lots within the C-TOPD - East into one of five (5) Character Zones.
 - ii. Special Frontage Standards – The Special Frontage Standards establish exceptions and special conditions for all buildings along designated frontages. These Special Frontage standards shall apply in addition to the underlying Character Zone standards.

- iii. Street Designations by Street Type – The Street Designations illustrate the design, configurations and development context for all streets within the C-TOPD - East. The Street Classification addresses vehicular lane widths, number of lanes, pedestrian accommodation, street tree requirements, on-street parking, and parkway and median standards (streetscape standards). Street designations on the Regulating Plan include Type ‘A’ and Type ‘B’ Streets, Mandatory and Non-Mandatory Street Networks. The Mandatory Street Network specifies the future streets needed to implement the C-TOPD - East Regulating Plan, and shall be required and shall generally meet the locational and connectivity goals of the Regulating Plan. Their design shall be guided by the Street Type Specifications. The Non-Mandatory Street Network indicates locations of suggested, but not required streets within the C-TOPD - East Regulating Plan. These streets are not mandatory, but at the election of the developer and their design shall be guided by the Street Type Specifications.
- iv. Civic/Open Space Designations – Civic/Open Space Designation include Mandatory Civic/Open Spaces. The Mandatory Civic/Open Space areas shown on the Regulating Plan designate the locations of proposed civic/open spaces (including parks, plazas, greens, and squares).

2.1.2 Development Standards: The C-TOPD Code - East (the text portion of this Code) enumerates the development standards with text and graphics for Character Zones, building form, civic/open space, landscape, architectural, signage, lighting, and all related standards for all streets, public and private development.

Section 3. Administration

This section sets forth the provisions for reviewing and approving development applications within the C-TOPD - East District. The intent is to ensure that all development is consistent with the provisions of this Code. All sections of this Code shall be applied during the review process.

3.1 The development standards under Articles XIII-A thru XVI and Article XXI-C of the City of Richardson Comprehensive Zoning Ordinance, as amended, shall not apply to the C-TOPD - East except as specifically referenced herein. Development standards not addressed in the C-TOPD - Code - East shall be governed by the City of Richardson Comprehensive Zoning Ordinance to the extent they are not in conflict with the intent or text of the C-TOPD Code - East.

3.2 Sign Standards under Chapter 18, as amended, of the City of Richardson Code of Ordinances, shall not apply to the C-TOPD - East except as specifically referenced herein.

3.3 Using this Code:

The following basic steps should be followed to determine the uses and development standards applicable to all properties within the C-TOPD - East District:

3.3.1 Locate the subject property on the Regulating Plan.

3.3.2 Identify:

- i. the Character Zone in which the property is located;
- ii. the Street Type designation along all its street frontages; and,
- iii. any Special Frontage Requirements that may be applicable to the subject property.

3.3.3 Review the Schedule of Uses by Character Zone as listed in Table 6.1 to determine allowed uses.

3.3.4 Examine the corresponding zone standards in the Building Form and Development Standards in Section 7 to determine the applicable development standards.

3.3.5 Refer to Section 5.5 for Special Frontage Standards.

3.3.6 Refer to Section 8 for Building Design Standards.

3.3.7 Refer to Section 9 for Street Type and Streetscape Standards.

The information from the above listed steps explains where the building will sit on the lot, the limits on its three dimensional form, the range of uses, and the palette of materials that will cover it.

- 3.4 Development within the C-TOPD - East District that complies with the provisions of this Code shall follow the City's development process as outlined in Chapter 21, Article II of the City of Richardson's Code of Ordinances and shall be approved by the City Manager or designee (see Appendix C for flow chart of the review process). In addition to complying with applicable City regulations that are not in conflict with this Code, the applicant shall provide the information required to adequately show compliance with this Code.
- 3.5 Standard for Approval of development plans: If a development plan conforms to the standards set forth in this Code and applicable City regulations not in conflict with this Code, the development plan shall be approved. Upon request by an applicant, the authority charged with approving the development plan shall certify the reason for an action taken on a development plan.
- 3.6 The City Manager or designee shall be responsible for the following:
- 3.6.1 Reviewing development plan applications for compliance with the requirements of C-TOPD - East.
 - 3.6.2 Approving development plan applications that are in compliance with the requirements of the C-TOPD Code - East.
 - 3.6.3 Making determinations on the applications and interpretations of standards in this Code.
 - 3.6.4 Approving revisions to previously approved development plans that comply with this Code and all applicable city ordinances.
 - 3.6.5 Approving any minor modifications to the approved Regulating Plan per Section 3.8.
 - 3.6.6 Recommendations on any SDP applications to the City Plan Commission (CPC) and City Council.
- 3.7 Special Development Plans: A request for a modification to any of the standards of this Code other than minor modifications permitted under Sections 3.8 shall be reviewed and processed as Special Development Plans.
- 3.7.1 Special Development Plans (SDP) are intended to allow applicants development flexibility to address specific market opportunities and/or contexts. An application for a Special Development Plan shall be processed as an amendment to the zoning ordinance under Article XXIX of the City of Richardson Zoning Ordinance and may only be considered by the City Council (CC) after the CPC has made a recommendation. The City Manager or designee shall review, make recommendations on any SDPs, and shall forward all SDP applications to the CPC. In evaluating an SDP, CC may consider the extent to which the application meets any of the following:
- i. provides an alternative "Master Plan" approach by consolidating multiple properties to create a predictable, market responsive development for the area; or
 - ii. fits the adjoining context by providing appropriate transitions.
- 3.8 Minor Modifications to the C-TOPD Code - East:
- The City Manager or designee shall have the authority to approve a request for minor modifications to C-TOPD Code - East that:
- 3.8.1 Does not materially change the circulation and building location on the site;
 - 3.8.2 Does not increase the building area permitted under this Code;
 - 3.8.3 Does not change the relationship between the buildings and the street;
 - 3.8.4 Does not allow a use not otherwise authorized in this Code;
 - 3.8.5 Does not allow greater height of any building or reduction of any parking requirement established in this Code; or

3.8.6 Changes to established street cross sections per Table 3.1 below and Section 9 of this Code.

The City Manager or designee shall also have the authority to approve minor modifications outlined in Table 3.1. Any appeals to the decisions of the City Manager on minor modifications shall be heard by the City Council. Any City Council denials of minor modifications or any changes beyond those that meet the criteria above, the thresholds established in Table 3.1, shall be processed as an amendment to this Code under Article XXIX of the City of Richardson Comprehensive Zoning Ordinance.

Table 3.1 Minor Modification Thresholds

<i>Standard</i>	<i>Minor Modification Threshold</i>	<i>Comments</i>
Area/boundary of Character Zones (including any Mandatory Civic/Open Spaces)	No more than a 15% change (increase or decrease) in the area of the TOD Freeway Hi-Rise and TOD Arterial Mixed Use (aggregate or per block). No more than 25% change (increase or decrease) in the area of the TOD Core, TOD Mid-Rise and TOD Mixed Use Zone (aggregate or per block).	<ul style="list-style-type: none"> • Shall not eliminate any Character Zone • 15% or 25% measurement shall be based on the total area of that specific Character Zone within the entire C-TOPD - East District
Location of any Mandatory Street	Location shall not move more than 100' in any direction. (This standard does not apply to the TOD Trail Connector Street Type)	<ul style="list-style-type: none"> • Shall maintain the connectivity intended by the Regulating Plan
Building Form and Development Standards		
<ul style="list-style-type: none"> • Build to zones/setbacks 	No more than a 20% change in the maximum or minimum setback.	<ul style="list-style-type: none"> • Changes to the Build to zones or setbacks may only be due to any changes to the street cross sections or change in the width of a sidewalk • In no case shall the sidewalk be less than 6 feet in width.
<ul style="list-style-type: none"> • Building Frontage 	No more than a 15% reduction in the required building frontage along each block of a Type 'A' Street	<ul style="list-style-type: none"> • Any reduction in the required building frontage shall be to accommodate Porte-cocheres for drop-off and pick-up.
<ul style="list-style-type: none"> • Required Off-street Parking 	No more than 20% reduction in the required off-street parking	<ul style="list-style-type: none"> • Shared parking agreements may be required between adjoining properties. • A parking demand analysis may be required based on the mix of uses being proposed.
<ul style="list-style-type: none"> • Street screen 	Waiver of Street Screen requirement along a Type 'B' Street	<ul style="list-style-type: none"> • Requirement for a street screen may only be waived along a Type 'B' Street along the frontage of any interim surface parking lot (off-street) that is intended to be in-filled with a parking structure. • In no case shall any portion of the surface parking have frontage along a Type 'A' Street without a required street screen • In no case shall the (off-street) surface parking lot be located at a street intersection for a minimum depth of 30' along each street (regardless of the Street Type).
<ul style="list-style-type: none"> • Street Cross Sections 	Cross sections of new streets may be adjusted with respect to number of lanes, lane widths, on-street parking configuration, pedestrian accommodation, and street tree planting	<ul style="list-style-type: none"> • Any changes in the street cross sections shall be based on specific development context such as vegetation, natural features,

		<p>drainage, and fire access and is subject to approval by the City.</p> <ul style="list-style-type: none"> Street cross sections may allow encroachment of porte cocheres into the street right-of-way and parking lanes, but not within any travel lanes subject to approval by the City.
Street Cross Sections	On-street parking configuration adjustments	<ul style="list-style-type: none"> On-street parking may be adjusted from angled to parallel or angled to drop-off lanes to accommodate valet drop-offs and pickups and similar functions subject to a Development Plan or parking plan submitted by the applicant and subject to approval by the City.

3.9 Requirement for Periodic Review:

3.9.1 The City Manager or his/her designee shall convene an annual City Council work session with the property owners and/or the Property Owners Association (POA) of all the property within the planned development, for the purpose of receiving project status updates on the development of the property and to assess progress of the planned development districts to achieve its intended outcome. The project updates should include, but are not limited to, total square footage of developed property, square footage of non-residential development, number of residential units, mix of land uses and traffic/transportation related items.

Section 4. Definitions

In addition to Definitions in Article I of the City of Richardson Comprehensive Zoning Ordinance, the following terms shall have the corresponding interpretations.

Arcade means a portion of the main façade of the building that is at or near the sidewalk and a colonnade supports the upper floors of the building. Arcades are intended for buildings with ground floor commercial or retail uses and the arcade may be one or two stories.

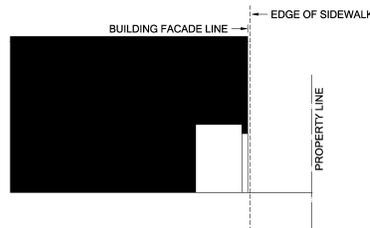


Image of an arcade

Attics/Mezzanines means the interior part of a building contained within a pitched roof structure or a partial story between two main stories of a building.

Auto-Related Sales and Service Uses means establishments that provide retail sales and services related to automobiles including, but not limited to, tires, batteries, gasoline, etc.

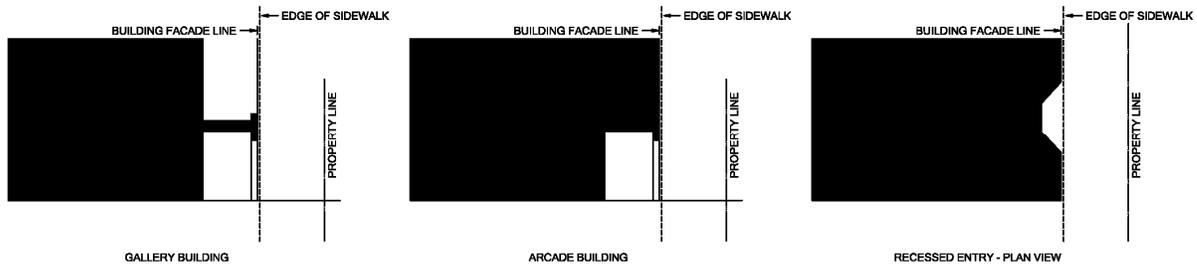
Block means the aggregate of lots, pedestrian passages and rear alleys, circumscribed by streets.

Block Face means the linear dimension of a block along one of its street frontages.

Build-to Line means the line at which the principal building’s front façade shall be built.

Build-to Zone (BTZ) means the area within which the principal building’s front façade is to be built.

Building Façade Line means the horizontal plane along a lot where the building’s front façade is actually located.



Building Façade Line Illustrations

Building Form Standards means the standards established for each Character Zone that specify the height, bulk, orientation, and elements for all new construction and redevelopment.

Building Frontage means the percentage of the building’s front façade that is required to be located at the front Build-to Line or Zone as a proportion of the block frontage along that public street. Parks, plazas, squares, improved forecourts, and pedestrian breezeway frontages shall be considered as buildings for the calculation of building frontage.

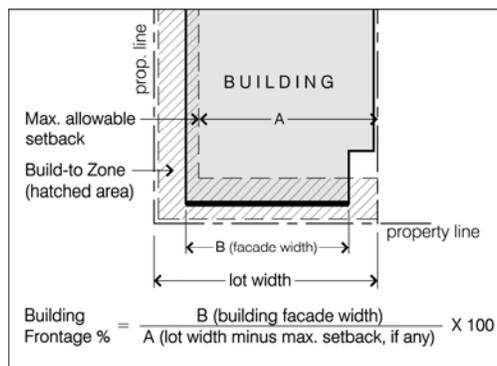


Image showing how a lot’s building frontage is calculated.

Character Zone means an area within the C-TOPD - East that creates a distinct urban form different from other areas within the C-TOPD - East. Character Zones are identified in the Regulating Plan.

City Manager means the City Manager of the City of Richardson or his/her designee.

Civic/Open Space means publicly accessible open space in the form of parks, courtyards, forecourts, plazas, greens, pocket parks, playgrounds, etc. They may be privately or publicly owned. For all residential uses, privately accessible open spaces such as courtyards, porches, and balconies may also be considered as Civic/Open Space for the purposes of the C-TOPD Code - East.

Comprehensive Plan means the City of Richardson Comprehensive Plan that establishes the blueprint for the long-term growth and development of the City as adopted on the effective date of this Code.

Encroachment means any structural or non-structural element such as a sign, awning, canopy, terrace, or balcony that breaks the plane of a vertical or horizontal regulatory limit, extending into a Setback, into the Public R.O.W, or above a height limit.

Gallery means an extension of the main façade of the building that is at or near the front property line and the gallery may overlap the public sidewalk.

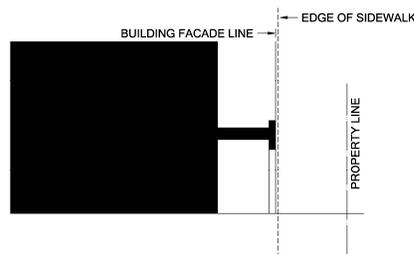


Image of a Gallery

Kiosk means a small, roofed structure, often open on one or more sides, used as a newsstand or booth. This structure could be temporary or permanent.

Live-Work Unit means a dwelling unit that is also used for work purposes, provided that the ‘work’ component is restricted to the uses of professional office, artist’s workshop, studio, or other similar uses and is located on the street level and constructed as separate units under a condominium regime or as a single unit. The ‘live’ component may be located on the street level (behind the work component) or any other level of the building. Live-work unit is distinguished from a home occupation otherwise defined by the C-TOPD Code - East in that the work use is not required to be incidental to the dwelling unit, non-resident employees may be present on the premises and customers may be served on site.

Living Screen means a Street Screen composed of landscaping in the form of vegetation.

Master Sign Plan means a unique sign plan to implement a specific vision for a portion or all of the development that meets Section 10.2 of this Code.

Minor Modification means any changes to the C-TOPD Code - East that meets the criteria established in Section 3.8 and Table 3.1.

Motor Court means an uncovered space that is wholly or partly surrounded by buildings or walls intended as a courtyard for vehicles as they approach a building. Surface parking is permitted within this space and shall not include more than 75 spaces. Surface treatments of the vehicular zones are richer in character than traditional concrete surface parked lots, using a variety of textures and colors such as brick, concrete pavers and colored concrete. Individual motor courts shall be separated by intervening buildings.

Non-Mandatory Street means a street that is designated on the Regulating Plan which is shown as a suggested street within the C-TOPD - East. The Non-Mandatory Street is not required and is at the election of the developer.

Park means a civic/open space that is a preserve available for unstructured recreation.

Plaza means a primarily hardscaped civic/open space with formal landscaping, available for civic purposes and commercial activities. A plaza shall be spatially defined by buildings.

Playground means a civic/open space designed and equipped for children's recreation. A playground may be fenced and may include an open shelter. Playgrounds may be located within residential areas and may be placed within a block. They may be included in other civic/open spaces.

Pedestrian Easement means a grant of use of private property for pedestrian access and use. This area contains the sidewalk, street trees, lighting and pedestrian furniture.

Regulating Plan means the Zoning Map attached hereto as Appendix A that shows the Character Zones, Civic Spaces, location of Streets, maximum height permitted and other Special Requirements applicable to the C-TOPD - East subject to the standards in the C-TOPD Code - East. For the purposes of this Code, the Regulating Plan shall also be the Concept Plan for the C-TOPD - East District.

Retail Ready means space constructed at a minimum interior height of 15 feet which may be used for non-commercial uses and can be converted into retail/commercial use. Prior to the issuance of a certificate of occupancy for a retail/commercial use in a Retail-Ready space, the space must comply with all building and construction codes for that use. In addition, the space must comply with all building and construction codes for that use. The intent of Retail-Ready space is to provide the flexibility of occupying a space in accordance with market demand and allowing the use in such space to change to retail/commercial uses accordingly.

Retail Sales Retail establishments are the final step in the distribution of merchandise. They are organized to sell in small quantities to many customers. Establishments in stores operate as fixed point-of-sale locations, which are designed to attract walk-in customers. Retail establishments often have displays of merchandise and sell to the general public for personal or household consumption, though they may also serve businesses and institutions. Some establishments may further provide after-sales services, such as repair and installation. Included in, but not limited to this category, are durable consumer goods sales and service, consumer goods, other grocery, food, specialty food, beverage, dairy, etc, and health and personal services.

Service Uses means a category for limited personal service establishments which offer a range of personal services that include (but not limited to) clothing alterations, shoe repair, dry cleaners, laundry, health and beauty spas, tanning and nail salons, hair care, etc.

Sign, Building Blade means a pedestrian-oriented sign that is affixed perpendicular to the corner of a building or to the front façade of a building above the ground floor to provide identification for the whole building.



Image of a Building Blade Sign

Sign, Marquee means a sign structure placed over the entrance to a theatre or other public gathering venue. It has signage stating either the name of the establishment or the name of the event, artist, and other details of the event appearing at that venue. The marquee is often identifiable by a surrounding cache of light bulbs, usually yellow or white, that flash intermittently or as chasing lights. Marquee signs may often be combined with Building Blade signs.



Image of a Marquee sign with a Building Blade Sign

Sign, Monument means any sign which is connected to the ground and which has no clear space for the full width of the sign between the bottom of the sign and the surface of the ground. A monument sign may include a sign face and sign structure, and may also include a sign base and sign cap.



Image of a Monument Sign

Sign, Sandwich Board means a portable sign consisting of two panels of equal size, which are hinged at the top or one panel with a support and placed on the ground or pavement so as to be self-supporting.



Images of sandwich board signs.

Sign, Tenant Blade means a smaller pedestrian-oriented sign that is affixed perpendicular to the building façade under a canopy or awning or immediately over a tenant space and provides identification for individual tenants within a building.



Image of a Tenant Blade Signs

Special Development Plan means a development application that meets Section 3.7 of this Code.

Station Platform Frontage: means the special frontage requirement on blocks with frontage along the rail station platform as indicated in the Regulating Plan.

Street Screen means a freestanding wall or living screen built along the BTZ or in line with the building façade line along the street. It may mask a parking lot or a loading/service area from view or provide privacy to a side yard and/or strengthen the spatial definition of the public realm.



Image of a combination masonry and living street screen

Street Network means the Mandatory and Non-Mandatory network for new and existing streets within the C-TOPD - East as established in the Regulating Plan.

Street Type means a specific designation for streets that establish a certain character and cross-sections to improve walkability within the C-TOPD - East.

Tree Well means an unpaved area around the trunk of a tree within the sidewalk area that is either landscaped with ground cover or covered with a tree grate.



Example of a tree wall with a tree grate



Example of a tree well with landscaping

Type 'A' Street means the streets identified as such on the Regulating Plan. Type 'A' Streets are the primary pedestrian streets.

Type 'B' Street means the streets identified as such on the Regulating Plan. Type 'B' Streets are intended to primarily accommodate access to parking, service, and other auto-related functions.

Section 5. The Regulating Plan

5.1 The Regulating Plan (Appendix A) is hereby adopted as the official zoning map for the C-TOPD - East District.

5.2 Character Zones Established – the following Character Zones are established. The boundaries of the specific Character Zones shall be established in the Regulating Plan.

5.2.1 TOD Core: The TOD Core provides the most opportunity for the highest intensity development. It is the area that has significant development impact and the highest pedestrian activity due to its adjacency to the transit station. The TOD Core consists of the highest density, with the greatest variety of uses. Development within the TOD Core Zone shall meet the Building Form and Development Standards in Section 7.1 of this Code.

5.2.2 TOD Mixed Use: The TOD Mixed Use is the area adjacent to the TOD Core that is intended for high intensity commercial and residential uses in addition to supporting retail and restaurant uses. Development within the TOD Mixed Use Zone shall meet the Building Form and Development Standards in Section 7.2 of this Code.

5.2.3 Arterial Mixed Use: Arterial Mixed Use is intended to provide appropriate transitions to major regional roadways while taking advantage of the arterial frontage for limited auto-oriented sites. The Arterial Mixed Use frontage also acts as a gateway into the C-TOPD - East by providing an attractive street frontage for the overall development. Development within the Arterial Mixed Use Zone shall meet the Building Form and Development Standards in Section 7.3 of this Code.

5.2.4 TOD Freeway Hi-Rise: TOD Freeway Hi-Rise is intended to provide an appropriate transition into the C-TOPD - East from the President George Bush Turnpike (SH 190) and US 75 access roads. This area is also intended for high intensity development. Development within the TOD Freeway Hi-Rise Zone shall meet the Building Form and Development Standards in Section 7.4 of this Code.

5.2.5 TOD Mid-Rise: TOD Mid-Rise consists of many attributes of the TOD Freeway Hi-Rise, but with a focus on more residential uses that are structure-parked. Ground floor retail uses are encouraged, but not required. Development within the TOD Mid-Rise Zone shall meet the Building Form and Development Standards in Section 7.5 of this Code.

5.3 Street Designations Street Type Established – The Regulating Plan shall establish the following Street Designations:

5.3.1 Type “A” Streets Established – Type “A” Streets are intended to be the primary pedestrian streets. The Type “A” Streets are TOD Main Street and TOD Station.

5.3.2 Type “B” Streets Established – Type “B” Streets are intended to balance pedestrian orientation with automobile orientation (service access, driveways, drive-through lanes, etc.). The Type “B” Streets are Parkway and TOD Street.

5.4 Mandatory Streets by Street Type – The Street Network indicates Mandatory and Non-Mandatory streets needed to implement the C-TOPD - East Regulating Plan. The Regulating Plan designates the type, classification and location of streets. All new streets in the C-TOPD - East shall meet the street design standards established in Section 9 herein.

5.5 Special Frontage Requirements – In order to address specific requirements and transitions based on street frontage and adjacency, the following Special Frontage Requirement as established in the Regulating Plan shall apply:

5.5.1 Station Platform Frontage: Ground floors of all buildings designated as Station Frontage on the Regulating Plan shall provide shaded areas to a minimum depth of 6 feet. Shaded devices may include arcades, galleries, awnings, canopies, etc.

5.5.2 Ground Floor Retail Ready: ground floors of all buildings designated as Ground Floor Retail Ready on the Regulating Plan shall be provided. The specific standards can be found in Section 7.

5.6 Street Network by Street Type – The Street Network indicates Mandatory and Non-Mandatory streets needed to implement the C-TOPD - East Regulating Plan. The Regulating Plan designates the type and location of streets. All new streets in the C-TOPD - East shall meet the street design standards established in Section 9 herein

5.7 Civic/Open Space – The Regulating Plan indicates Mandatory Civic/Open Spaces. The specific standards for Civic/Open Space are established in Section 11.

5.8 Building Height – The Regulating Plan also indicates the minimum and maximum building height permitted within each character zone of the C-TOPD - East District.

Section 6. Schedule of Permitted Uses

6.1 Generally: Due to the emphasis on urban form over land uses in the C-TOPD - East, general use categories have been identified by Character Zone. Uses not listed in the following schedule, but are substantially similar, may be permitted upon the approval of the City Manager or designee, subject to appeal directly to the City Council.

6.2 Schedule of Uses:

Table 6.1

Character Zone	TOD Core	TOD Mixed Use	Arterial Mixed Use	TOD Freeway Hi-Rise	TOD Mid-Rise
Land Use					
Commercial Uses (Office, Retail, Sales & Service Uses)					
Retail Sales or Service with <u>no drive through facility</u> (includes alcohol sales, which shall meet Chapter 4, Alcohol Beverages of the City of Richardson Code of Ordinances). Excluded from this category are Auto-Retail Sales and Service Uses (see Section 3 of the Code for Definition of Retail, Service uses, and Auto-related Sales and Service)	P	P	P	P	P
Finance, Insurance, and Real Estate establishments including banks, credit unions, real estate, and property management services, <u>with no drive through facility</u>	P	P	P	P	P
Offices for business, professional, and technical uses such as accountants, architects, lawyers, doctors, etc.	P	P	P	P	P
Research laboratory headquarters, laboratories and associated facilities	P	P	P	P	P
Food Service Uses such as full-service restaurants, cafeterias, bakeries and snack bars with <u>no drive through facilities</u> Included in this category is café seating within a public or private sidewalk area with no obstruction of pedestrian circulation. Also included in this category is the sale of alcoholic beverages which shall meet Chapter 4, Alcoholic Beverages of the City of Richardson Code of Ordinances.	P	P	P	P	P
Any use with a drive through facility	NP	P/C	P/C	NP	NP
Auto-related Sales and Service	NP	NP	P/C	NP	NP
Community and Cultural Uses					
Art galleries	P	P	P	P	P
Art, antique, furniture or electronics studio (retail, repair or fabrication; excludes auto electronics sales or service)	P	P	P	P	P
Games arcade establishments	P	P	P	P	P
Theater, cinema, dance, music or other entertainment establishment	P	P	P	P	P
Museums and other special purpose recreational institutions	P	P	P	P	P
Fitness, recreational sports, gym, or athletic club	P	P	P	P	P
Parks, greens, plazas, squares, and playgrounds	P	P	P	P	P
Public and Institutional Uses					
Business associations and professional membership organizations	P	P	P	P	P
Child day care and preschools	P	P	P	P	P
Schools, libraries, and community halls	P	P	P	P	P
Universities and Colleges	P/C	P/C	P	P	P/C
Hospital	P	P	P	P	P
Civic uses	P	P	P	P	P
Social and fraternal organizations	P	P	P	P	P
Social services and philanthropic organizations	P	P	P	P	P
Religious Institutions	P	P	P	P	P
Funeral homes	P	P	P	P	P
Residential Uses					
Home Occupations	P/A	P/A	P/A	P/A	P/A
Multi-family residential					

Character Zone	TOD Core	TOD Mixed Use	Arterial Mixed Use	TOD Freeway Hi-Rise	TOD Mid-Rise
Ground Floor	P/C	P	NP	P/C ***	P/C
Upper Floors	P	P	NP	P ***	P
Residential Lofts	P	P	P	P	P
Live-Work Unit	P	P	P	P	P
Single-family residential attached dwelling unit (Townhomes)	NP	P/C	NP	NP	NP
Single-family residential detached dwelling unit	NP	NP	NP	NP	NP
Accessory residential unit	NA	NA	NA	NA	NA
Model homes for sales and promotion**	P	P	P	P	P
Hotels	P	P	P	P	P
Parking, surface (primary use of property)	NP	NP	NP	NP	NP
Parking, surface (accessory use of property)	P	P	P	P	P
Parking, structured	P	P	P	P	P
Private attached garage	NP	P	NP	NP	NP
Private detached garage	NP	NP	NP	NP	NP
Sales from kiosks	P	P	P	P	P
Veterinary clinic	NP	P	P	NP	NP
Community garden	P	P	P	P	P
Incidental Outdoor Display (subject to standards in Section 7 of the Code)	P/A	P/A	P/A	P/A	P/A
Antennas including cell, accessory, and mounted on top of buildings.	P/A/C	P/A/C	P/A/C	P/A/C	P/A/C
Utility infrastructure	P/A/C	P/A/C	P/A/C	P/A/C	P/A/C
Rain water harvesting equipment	P/A/C	P/A/C	P/A/C	P/A/C	P/A/C
Wind energy equipment	P/A	P/A	P/A	P/A	P/A
Solar energy equipment	P/A	P/A	P/A	P/A	P/A
Special Event	P	P	P	P	P

P= Permitted by right NP= Not Permitted P/C= Permitted with design criteria per Table 6.2 P/A = Permitted Accessory Use NA= Not applicable

A* = Accessory use to not exceed 25% of the primary use building square footage

** Model homes are limited to a time period until all the homes are sold in the neighborhood.

*** = Multi-Family Residential – Ground Floor and Upper Floor not permitted in TOD Freeway Hi-Rise Character Zone at the southwest corner of PGBT and U.S. 75.

6.3 Use Criteria: All uses listed as P/C in Table 6.1 shall also meet the following standards in Table 6.2

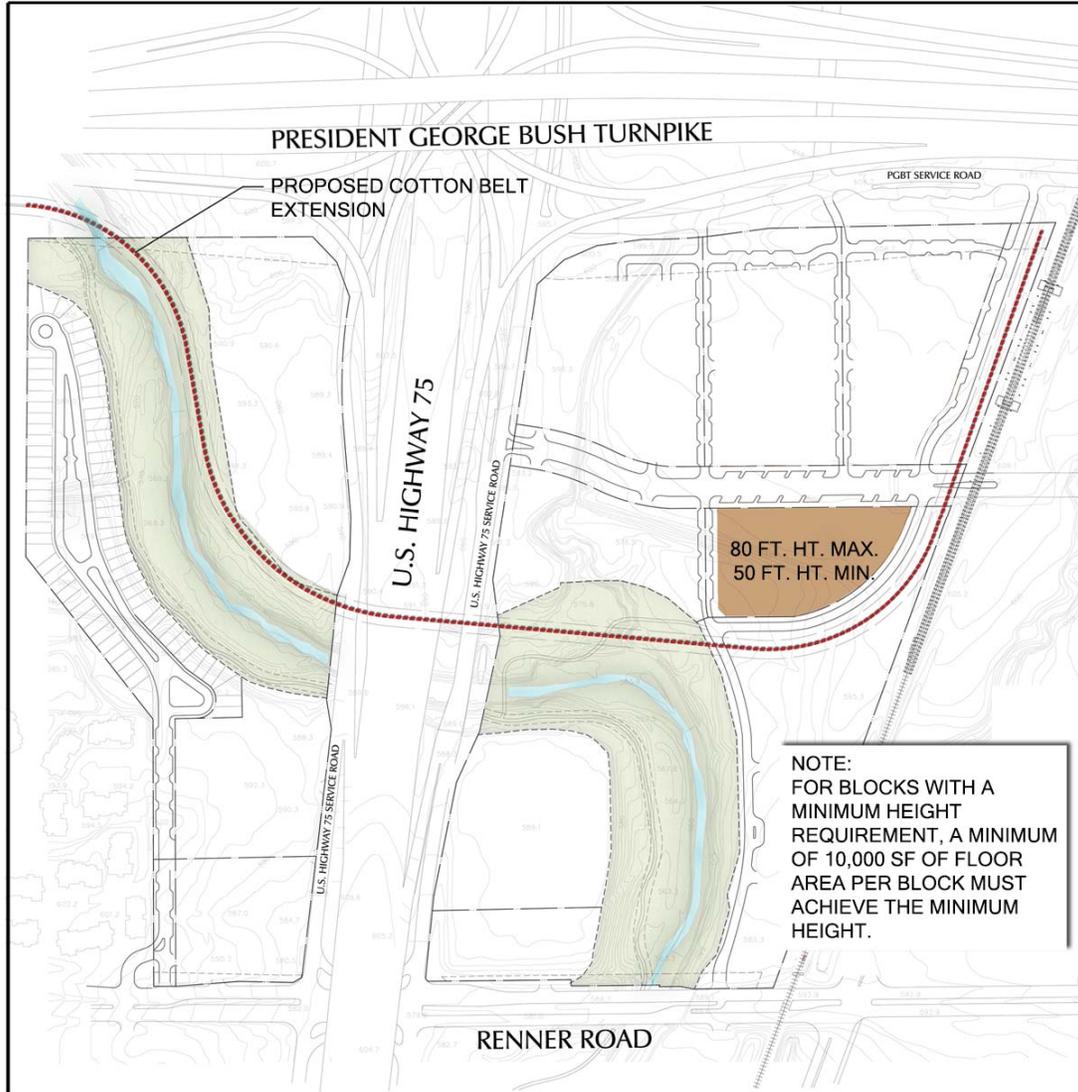
Table 6.2 – Use Criteria		
<i>Use</i>	<i>Zone</i>	<i>Location & Design Criteria</i>
Non-Residential Uses		
Any permitted use with a drive through facility	TOD Mixed Use & Arterial Mixed Use	<ul style="list-style-type: none"> All drive through access (driveways) shall be from Type 'B' Streets. Drive through lanes and/or canopies shall not have frontage along on or be located along any Type 'A' Streets. Drive through areas screened by a 4' high Street Screen.
Universities and Colleges	TOD Core, TOD Mixed Use & TOD Mid-Rise	<ul style="list-style-type: none"> Shall be required to provide structured parking as part of the build-out for the university/college campus
Auto-related Sales and Service	Arterial Mixed Use	<ul style="list-style-type: none"> No more than 50% of a block's frontage along a Type 'B' Street shall be occupied by gas pumps, canopies, and/or service bays. No outdoor storage of vehicles or other products sold shall be permitted. All auto related sales display shall be inside storefronts.
Residential Uses		
Multi-family residential Ground Floor	TOD Core, TOD Freeway Hi-Rise & TOD Mid-Rise	<ul style="list-style-type: none"> All ground floors as depicted on the Regulating Plan shall be built to Retail Ready standards.
Single-family residential attached dwelling unit (Townhomes)	TOD Mixed Use	<ul style="list-style-type: none"> Shall not be permitted along Renner Road
Other Uses		
Antennas including cell, accessory and mounted (Excluded from this category are freestanding and commercial antennas and equipment buildings)	All Zones	<ul style="list-style-type: none"> Antennas shall be permitted on rooftops. Antennas shall be screened entirely with a screen of same color as the principal building. Antennas shall not be visible from adjacent Type 'A' Street.
Utility equipment (includes electrical transformers, gas meters, etc)	All Zones	<ul style="list-style-type: none"> On all other frontages, they shall be screened with a Street Screen at least as high as the equipment being screened. On Type "A" street frontages, utility equipment shall also be recessed into the building. Utility equipment shall be permitted on rooftops. Utility equipment shall be screened entirely with a screen of same color as the principal building.
Rain water harvesting equipment	All Zones	<ul style="list-style-type: none"> Rain water harvesting equipment shall be permitted on rooftops. Rain water harvesting equipment shall be screened entirely with a screen of same color as the principal building.

Section 7. Building Form and Development Standards

The following section establishes the Building Form and Development Standards for all Character Zones within the C-TOPD - East District. Diagrams and reference letters are used for illustrations purposes only. Reference letters may not be in continuous sequence.

7.1 TOD Core Zone

Character Zone: TOD Core

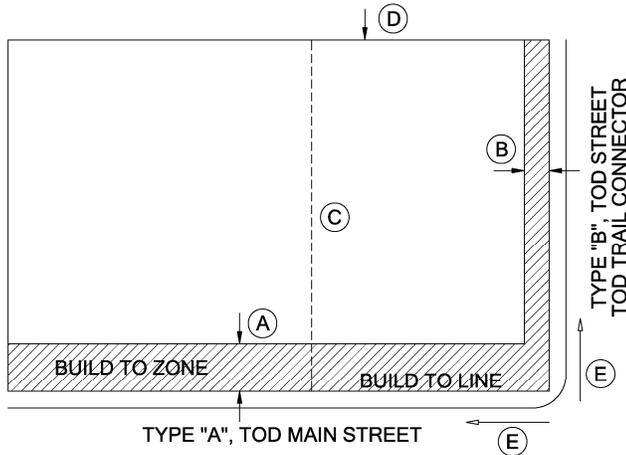


TOD Core Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements.

TOD Core Zone

7.1.1 Building Placement



REFER TO SECTION 9 FOR BTL LOCATION

Build-To Zone (BTZ)
(Distance from pedestrian easement to edge of the BTZ)

Type "A," TOD Main Street	0 - 25 feet	(A)
Type "B," TOD Street, TOD Trail Connector	0 - 10 feet	(B)

Setback

Side (from property line)	0 feet (see #1)	(C)
Rear (from property line)	0 feet (see #1)	(D)

Building Frontage

Building Frontage required along all street BTZ	90% (min.) (see #4)	(E)
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7.1.2 Block Standards

Block face dimensions	250 - 500 feet
Block perimeter	1500 feet (maximum)

7.1.3 Building Height

Principal Building Standards

Building maximum	Shall be established on the Regulating Plan
First floor to floor height	15 feet min. (see #2)
Ground floor finish level	12 inches max. above sidewalk (except for ground floor Retail Ready buildings)
Upper floor(s) height (floor-to-ceiling)	10 feet min. (excluding bathrooms, hallways, closets and areas common to furr downs)

7.1.4 Ground Floor Retail Ready Requirements

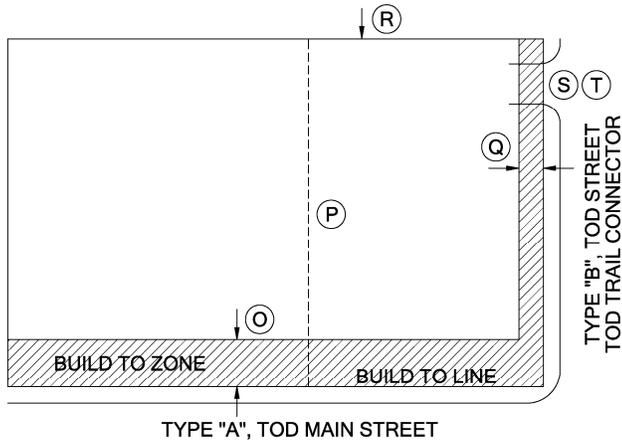
Ground floors of all buildings fronting on Cityline Drive (TOD Main Street) shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

Notes

- #1 - Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #2 - First floor heights shall not apply to parking structures.
- #3 - All buildings in the TOD Core Zone shall meet the Building Design Standards in Section 8.
- #4 - Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen that is at least as high as the service equipment being screened. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen that provides an opaque screen at maturity or a combination of masonry and living screen. Species shall be selected from the C-TOPD - East Planting List in Appendix B of the C-TOPD Code - East. The required Street Screen shall be located within the BTZ along the corresponding frontage.
- #5 - Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.
- #6 - Setbacks and build-to lines for recessed entry and arcade buildings shall be measured from the building façade line which contains the recessed entry or arcade.
- #7 - Required parking may be provided anywhere within the C-TOPD - East
- #8 - Article III, Chapter 21, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas.

TOD Core Zone

7.1.5 Parking & Service Access



7.1.6 Encroachments

Canopies, signs, awnings and balconies may encroach over the BTZ, setback, and, sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

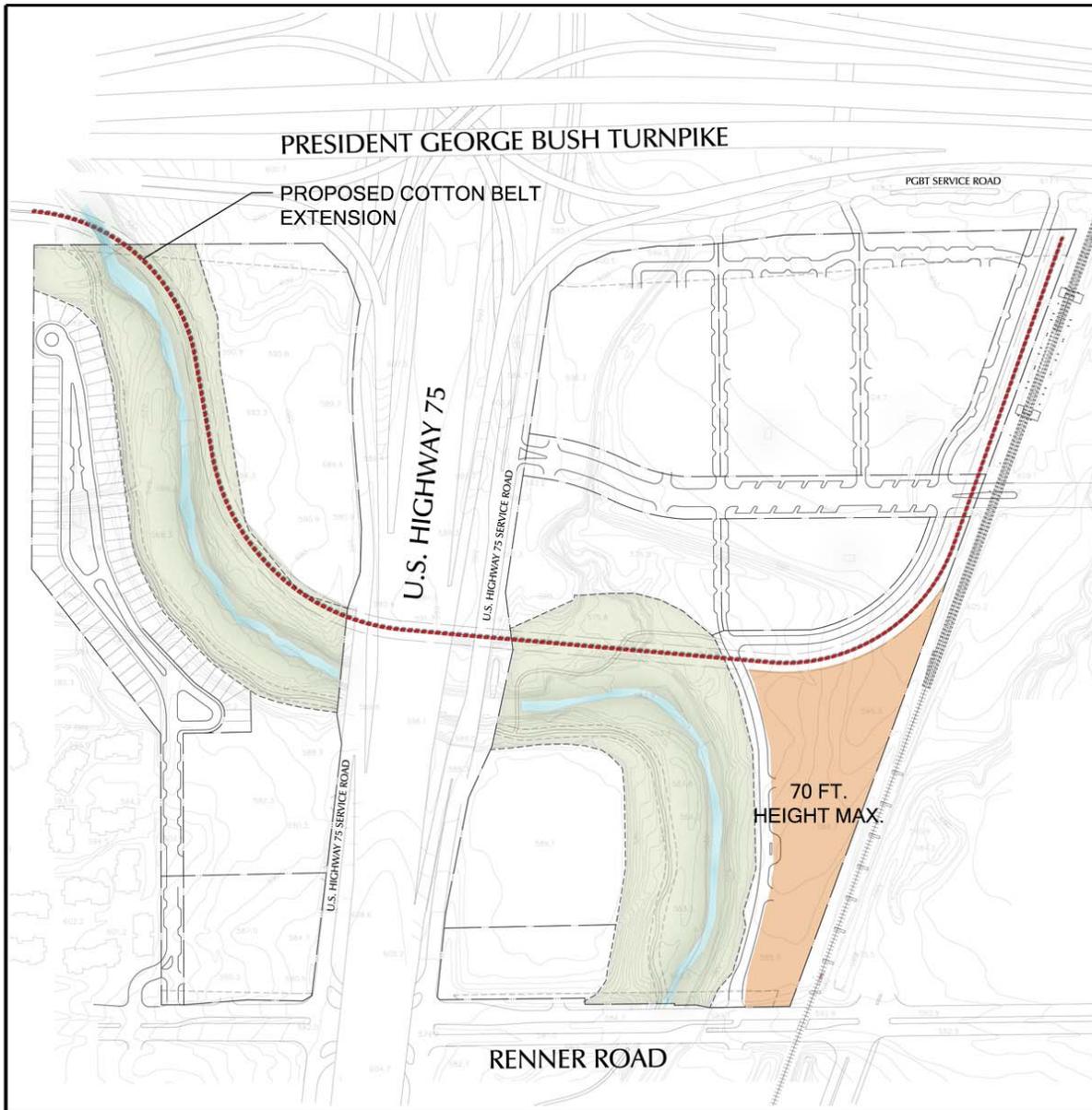
7.1.7 Applicability

Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

(i) Parking Location		
Surface/At Grade Parking		
Type "A," TOD Main Street	Shall be located behind the principal building	⓪
Type "B," TOD Street, TOD Trail Connector	Min. of 3 feet behind the building facade line along that street	Ⓠ
Side setback (distance from property line)	0 feet min.	Ⓟ
Rear setback (distance from property line)	0 feet min.	Ⓡ
Above Grade Parking		
Setback along Type "A," TOD Main Street, Type "B," TOD Street, TOD Trail Connector and Civic Space (distance from edge of sidewalk)	0 feet min.	⓪
Side and rear setbacks (distance from property line)	0 feet min.	
Upper Floors	May be built up to the building line	
(ii) Required Off-Street Parking Spaces		
Non-residential uses	1 space/300 sq. feet (gross)	
Residential uses	1.5 space/unit	
(iii) Driveways and Service Access		
Parking driveway width	20 feet max. (except when drives may need to be wider to address service access or fire lane standards)	Ⓢ
Driveways and off-street loading and unloading shall not be located on a Type "A," TOD Main Street. Porte cocheres may be permitted on Type "A" Streets to provide drop-off and valet service. Shared driveways and cross access easements are encouraged between lots to minimize curb cuts. If driveway and/or off-street service loading and unloading access is provided from a Type "A," TOD Main Street, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are developed.		Ⓣ

7.2 TOD Mixed Use Zone

Character Zone: TOD Mixed Use

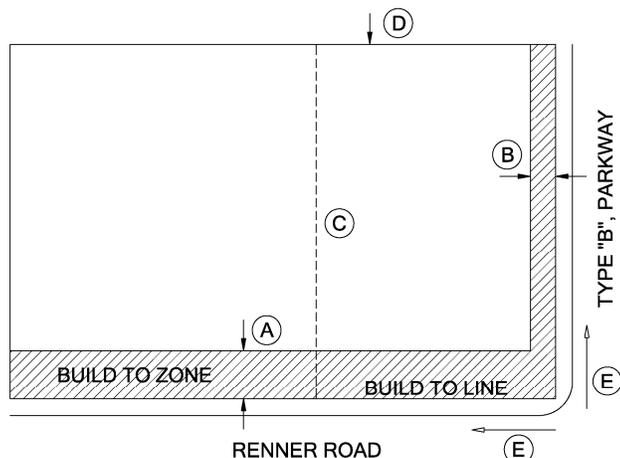


TOD Mixed Use Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements.

TOD Mixed Use Zone

7.2.1 Building Placement



REFER TO SECTION 9 FOR BTL LOCATION

Build-To Zone (BTZ) (Distance from edge of pedestrian easement to edge of the BTZ)		
Renner Road	None	(A)
Type "B," Parkway	0 - 10 feet (see #1)	(B)
Setback		
Side (distance from property line)	0 feet (see #2)	(C)
Rear (distance from property line)	0 feet (see #2)	(D)
Building Frontage		
Building Frontage required along all street BTZ	80% (min.) (see #3)	(E)

7.2.2 Block Standards

Block face dimensions	250 – 500 feet (see #4)
Block perimeter	1500 feet (maximum) (see #4)

7.2.3 Building Height

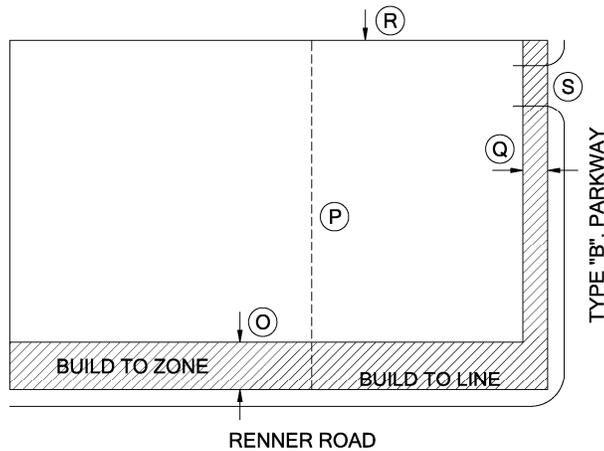
Principal Building Standards	
Building maximum	Shall be established as on the Regulating Plan
First floor to ceiling height	10 feet min. (excluding bathrooms, hallways, closets, and areas common to furr downs)
Ground floor finish level	12 inches max. above sidewalk (except for ground floor Retail Ready buildings)
Upper floor(s) height (floor-to-floor)	10 feet min. (excluding bathrooms, hallways, closets, and areas common to furr downs)

Notes

- #1 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #2 – First floor heights shall not apply to parking structures.
- #3 – All buildings in the TOD Mixed Use Zone shall meet the Building Design Standards in Section 8.
- #4 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen that is at least as high as the service equipment being screened. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen that provides an opaque screen at maturity or a combination of masonry and living screen. Species shall be selected from the C-TOPD - East Planting List in Appendix B of the C-TOPD Code - East. The required Street Screen shall be located within the BTZ along the corresponding frontage.
- #5 – Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.
- #6 – Setbacks and build-to lines for recessed entry and arcade buildings shall be measured from the building façade line which contains the recessed entry or arcade.
- #7 – Required parking may be provided anywhere within the C-TOPD - East.
- #8 – Article III, Chapter 21, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas

TOD Mixed Use Zone

7.2.4 Parking & Service Access



7.2.6 Applicability

Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

(i) Parking Location

Surface/At Grade Parking		
Renner Road	Shall be located behind the principal building	⓪
Type "B," Parkway	Min. of 3 feet behind the building facade line along that street	Ⓠ
Side setback (distance from property line)	0 feet min.	Ⓟ
Rear setback (distance from property line)	0 feet min.	Ⓡ
Above Grade Parking		
Setback along Type "B," Parkway (distance from edge of sidewalk)	0 feet min.	⓪
Side and rear setbacks (distance from property line)	0 feet min.	
Upper Floors	May be built up to the building line	

(ii) Required Off-Street Parking Spaces

Non-residential uses	1 space/250 sq. feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

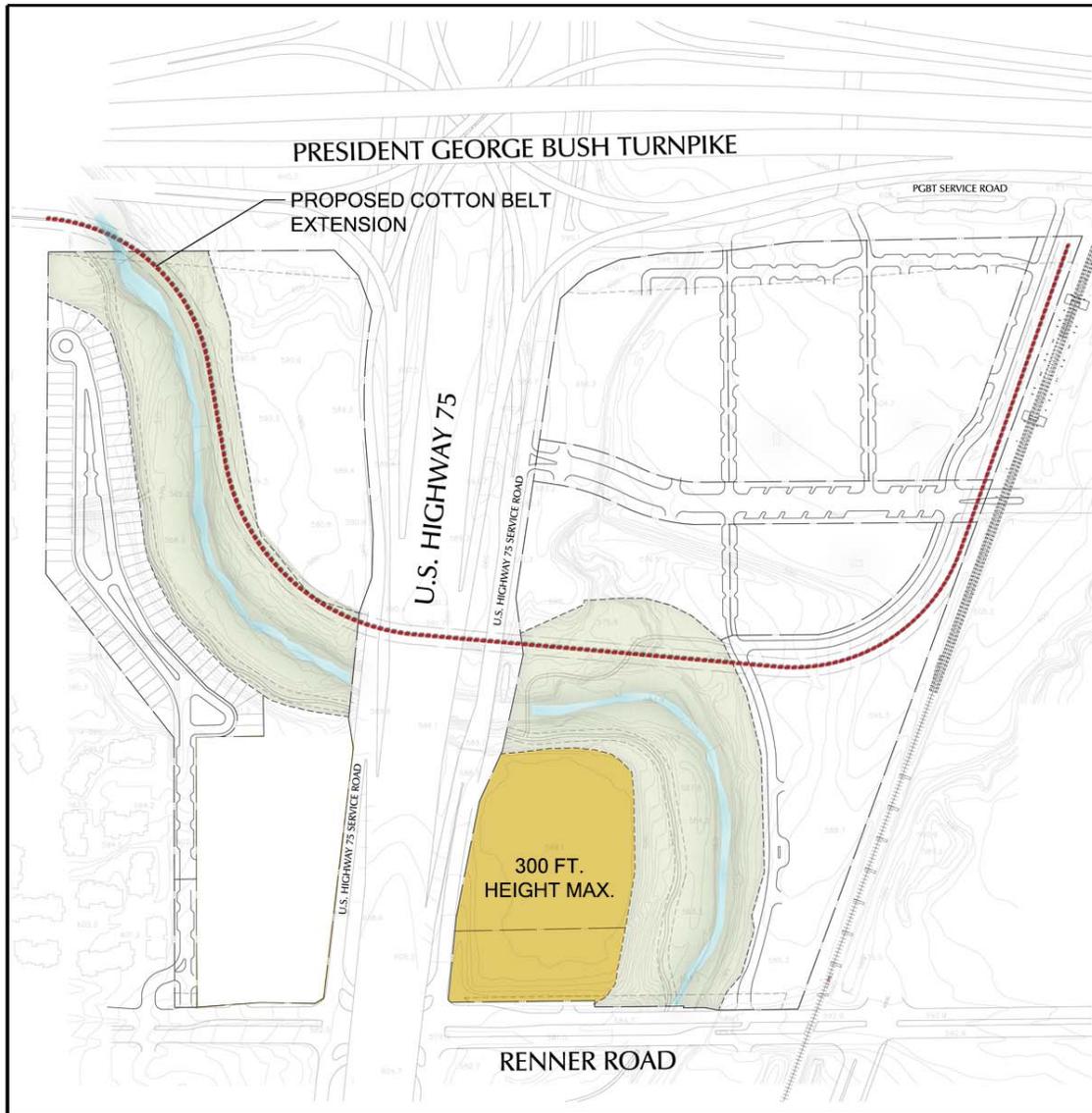
Parking driveway width	20 feet max. (except service drives which may be a max. of 30 feet wide)	Ⓢ
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7.2.5 Encroachments

Canopies, signs, awnings and balconies may encroach over the BTZ, setback, and, sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.3 Arterial Mixed Use Zone

Character Zone: Arterial Mixed Use

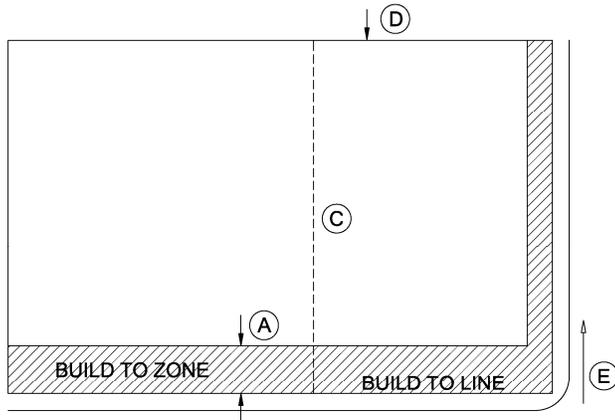


TOD Arterial Mixed Use Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all the requirements.

Arterial Mixed Use Zone

7.3.1 Building Placement



U.S. 75 FRONTAGE ROAD, RENNER
REFER TO SECTION 9 FOR BTL LOCATION

Build-To Zone (BTZ)		
(Distance from Property Line to edge of the BTZ)		
Front (U.S. 75 Service Road, Renner Road))	None	(A)
(Distance from sidewalk easement to edge of the BTZ)		
Civic Space	0 – 40 feet	(B)
Setback		
U.S. 75 Service Road, Renner Road – distance from property line or edge of utility easement.	10 feet (min.)	
Side (distance from property line)	0 feet (see #1)	(C)
Rear (distance from property line)	0 feet (see #1)	(D)
Building Frontage		
Building Frontage required along BTZ	40% (min.)	(E)
Building Frontage required along U.S. 75 Service Road and Renner Road	0% (min.)	

7.3.2 Block Standards

Block face dimensions	No Maximum
Block perimeter	No Maximum

7.3.3 Building Height

Principal Building Standards

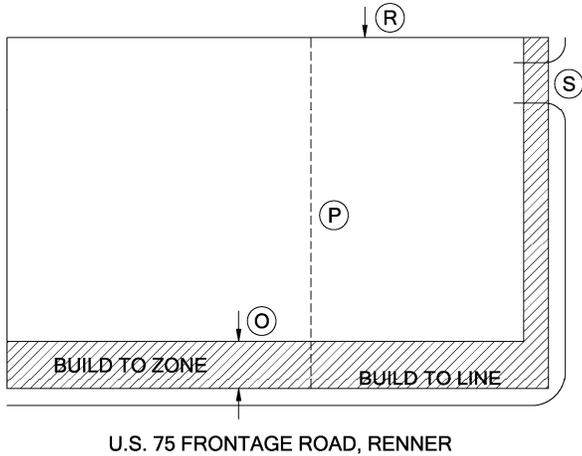
Building maximum	Shall be as established on the Regulating Plan
First floor to floor height	15 feet min. (see #2)
Ground floor finish level	12 inches max. above sidewalk (except for ground floor Retail Ready buildings)
Upper floor(s) height (floor-to-ceiling)	10 feet min.

Notes

- #1 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #2 – First floor heights shall not apply to parking structures.
- #3 – All buildings in the Arterial Mixed Use Zone shall meet the Building Design Standards in Section 8.
- #4 – Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.
- #5 – Setbacks and build-to lines for recessed entry and arcade buildings shall be measured from the building façade line which contains the recessed entry or arcade.
- #6 – Required parking may be provided anywhere within the C-TOPD - East.
- #7 – Article III, Chapter 21, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas
- #8 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high street screen; furthermore, service areas shall be defined by a street section that is at least as high as the service equipment being screened. The street screen shall be of either the same building material as the principal structure of a the lot of masonry or a living screen that provides an opaque screen at maturity or a combination of masonry and living screen. Species shall be selected from the C-TOPD – East Planting List in Appendix B of the C-TOPD – East Code. The required street screen shall be located within the BTZ along the corresponding frontage.
- #9 – Surface parking as an ancillary use with an office building motor court shall be permitted with a 0 – 50 foot setback from the building line.

Arterial Mixed Use Zone

7.3.4 Parking & Service Access



7.3.5 Encroachments

Canopies, signs, awnings and balconies may encroach over the BTZ, setback, and, sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.3.6 Applicability

Building Form and Development Standards in this Section shall apply to all development in this Character Zone.

(i) Parking Location

Surface/At Grade Parking

U.S. 75 Service Road, Renner Road setback	Min. 3 feet behind the building façade line (see #9)	
Civic Space setback (distance from approximate line delineated)	Min. of 3 feet behind the building façade line (see #9)	(O)
Side setback (distance from property line)	0 feet	(P)
Rear setback (distance from property line)	0 feet	(R)

Above and Below Grade Parking

Setback along Civic Space (distance from approximate line indicated)	0 feet min.	(O)
Setback along U.S. 75 Service Road and Renner Road (distance from property line)	0 feet min.	
Side and rear setbacks (distance from property line)	0 feet min.	
Upper Floors	May be built up to the building line	

(ii) Required Off-Street Parking Spaces

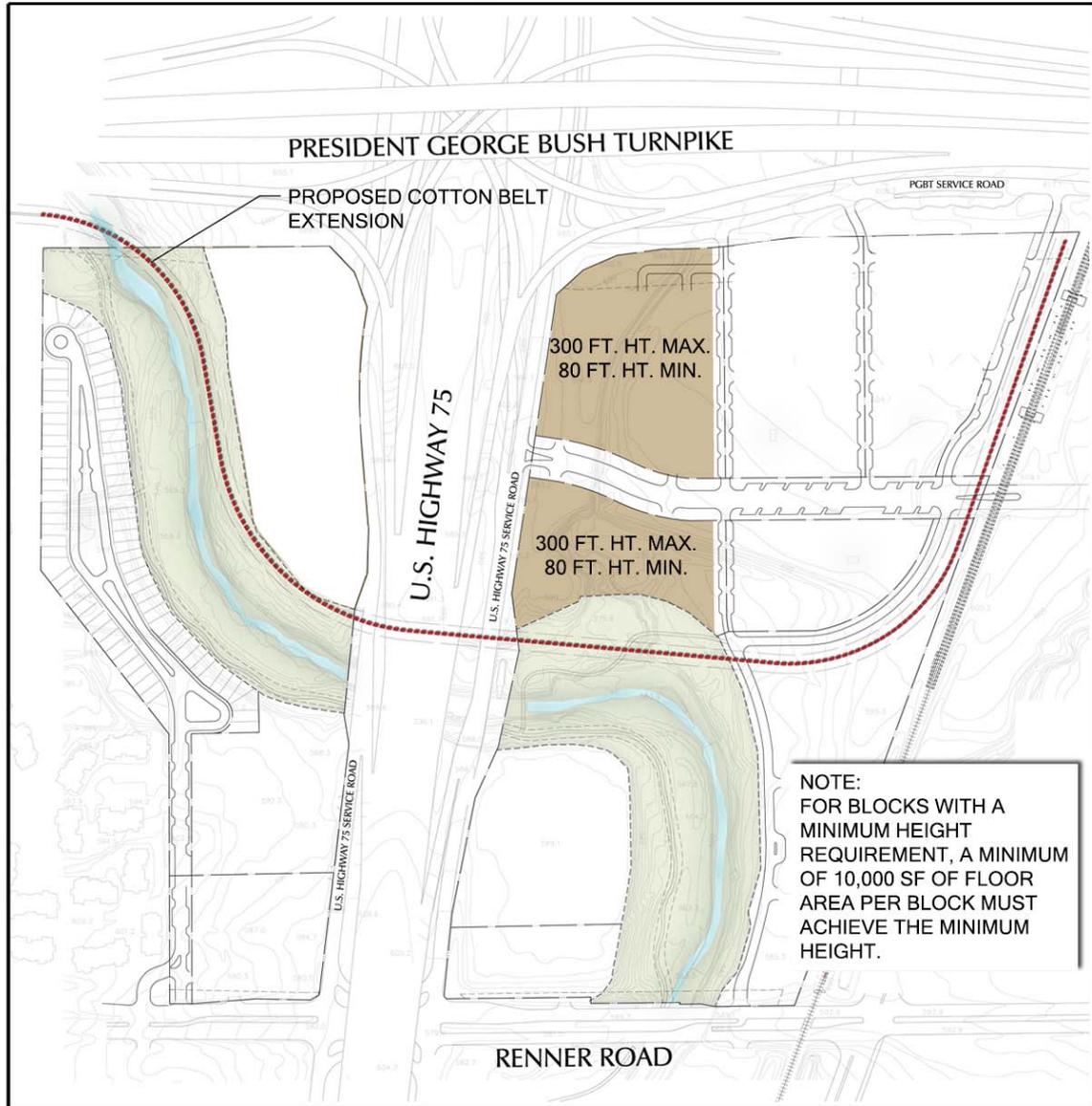
Retail/Restaurant uses	1 space/200 sq. feet (gross)
Residential uses	1.5 space/unit
Office Uses	1 space/300 sq. feet
Hotel/Lodging	1 space/hotel room

(iii) Driveways and Service Access

Parking driveway width	City standards on Arterial Roadways and 24 feet max on all C-TOPD - East Streets, except when drives may need to be wider to address service access or fire lane standards.	(S)
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7.4 TOD Freeway Hi-Rise

Character Zone: TOD Freeway Hi-Rise

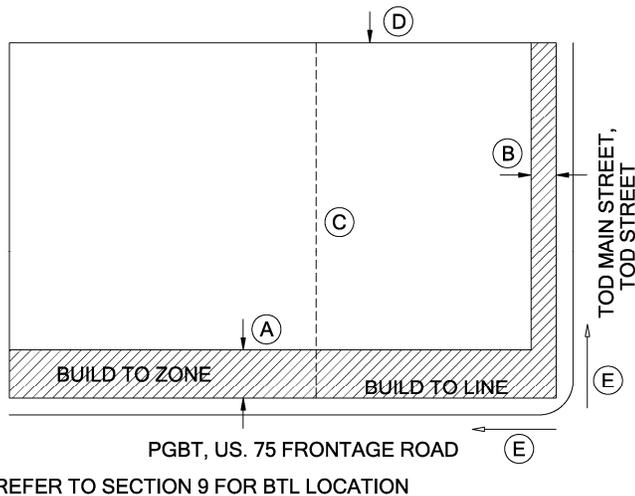


TOD Freeway Hi-Rise Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements.

TOD Freeway Hi-Rise Zone

7.4.1 Building Placement



Build-To Zone (BTZ)		
(Distance from Property Line to edge of the BTZ)		
PGBT and U.S. 75 Service Road	None	(A)
(Distance from pedestrian easement to edge of the BTZ)		
Type "A," TOD Main Street and Civic Space	0 – 25 feet	(B)
Type "B," TOD Street	0 - 10 feet	
Setback		
Side (distance from property line)	0 feet (see #1)	(C)
Rear (distance from property line)	0 feet	(D)
Building Frontage		
Building Frontage required along all street BTZ	90% (min.) (see #4)	
Building Frontage required along PGBT and U.S. 75 Service Road	0% (min.) (see #4)	(E)

7.4.2 Block Standards

Block face dimensions	No Maximum
Block perimeter	No Maximum

7.4.3 Building Height

Principal Building Standards

Building maximum	Shall be as established on the Regulating Plan
First floor to floor height	15 feet min. (see #2)
Ground floor finish level	12 inches max. above sidewalk level (except for ground floor Retail Ready buildings)
Upper floor(s) height	10 feet min. (excluding bathrooms, hallways, closets, and areas common to furr downs)

7.4.4 Ground Floor Retail Ready Requirements

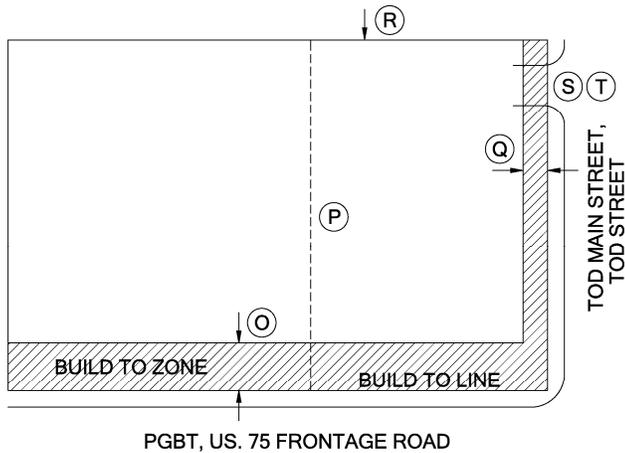
Ground floors of all buildings fronting on TOD Main Street shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

Notes

- #1 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #2 – First floor heights shall not apply to parking structures.
- #3 – All buildings in the TOD Freeway Hi-Rise shall meet the Building Design Standards in Section 8.
- #4 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen that is at least as high as the service equipment being screened. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen that provides an opaque screen at maturity or a combination of masonry and living screen. Species shall be selected from the C-TOPD - East Planting List in Appendix B of the C-TOPD Code - East. The required Street Screen shall be located within the BTZ along the corresponding frontage.
- #5 – Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.
- #6 – Setbacks and build-to lines for recessed entry and arcade buildings shall be measured from the building façade line which contains the recessed entry or arcade.
- #7 – Required parking may be provided anywhere within the C-TOPD - East.
- #8 – Article III, Chapter 21, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas.
- #9 - Surface parking as an ancillary use with an office or condo tower motor court shall be permitted with a 0 – 20 foot setback.
- #10 - Setbacks and build-to lines for recessed entry and arcade buildings shall be measured from the building façade line which contains the recessed entry or arcade.
- #11 - Article 3, Chapter 21, Section 21-52 of the City of Richardson Subdivision and Development Ordinances shall apply for design of off-street parking areas.

TOD Freeway Hi-Rise Zone

7.4.5 Parking & Service Access



7.4.6 Encroachments

Canopies, signs, awnings and balconies may encroach over the BTZ, setback, and, sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.4.7 Applicability

Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

(i) Parking Location

Surface/At Grade Parking		
PGBT Service Road and U.S. 75 setback (see #1)	Shall be located behind the principal building	⓪
Type "A," TOD Main Street and Civic Space (see #1)	Shall be located behind the principal building	⓪
Type "B," TOD Street	Min. of 3 feet behind the building façade line along that street	Ⓠ
Side setback (distance from property line)	0 feet min.	Ⓟ
Rear setback (distance from property line)	0 feet min.	Ⓡ
Above Grade Parking		
Setback along Type "A," TOD Main Street, Type "B," TOD Street, and Civic Space (distance from edge of sidewalk)	0 feet min.	
PGBT Service Road, U.S. 75 Service Road, side and rear setbacks (distance from property line) or edge of sidewalk.	0 feet min.	
Upper Floors	May be built up to the building line	

(ii) Required Off-Street Parking Spaces

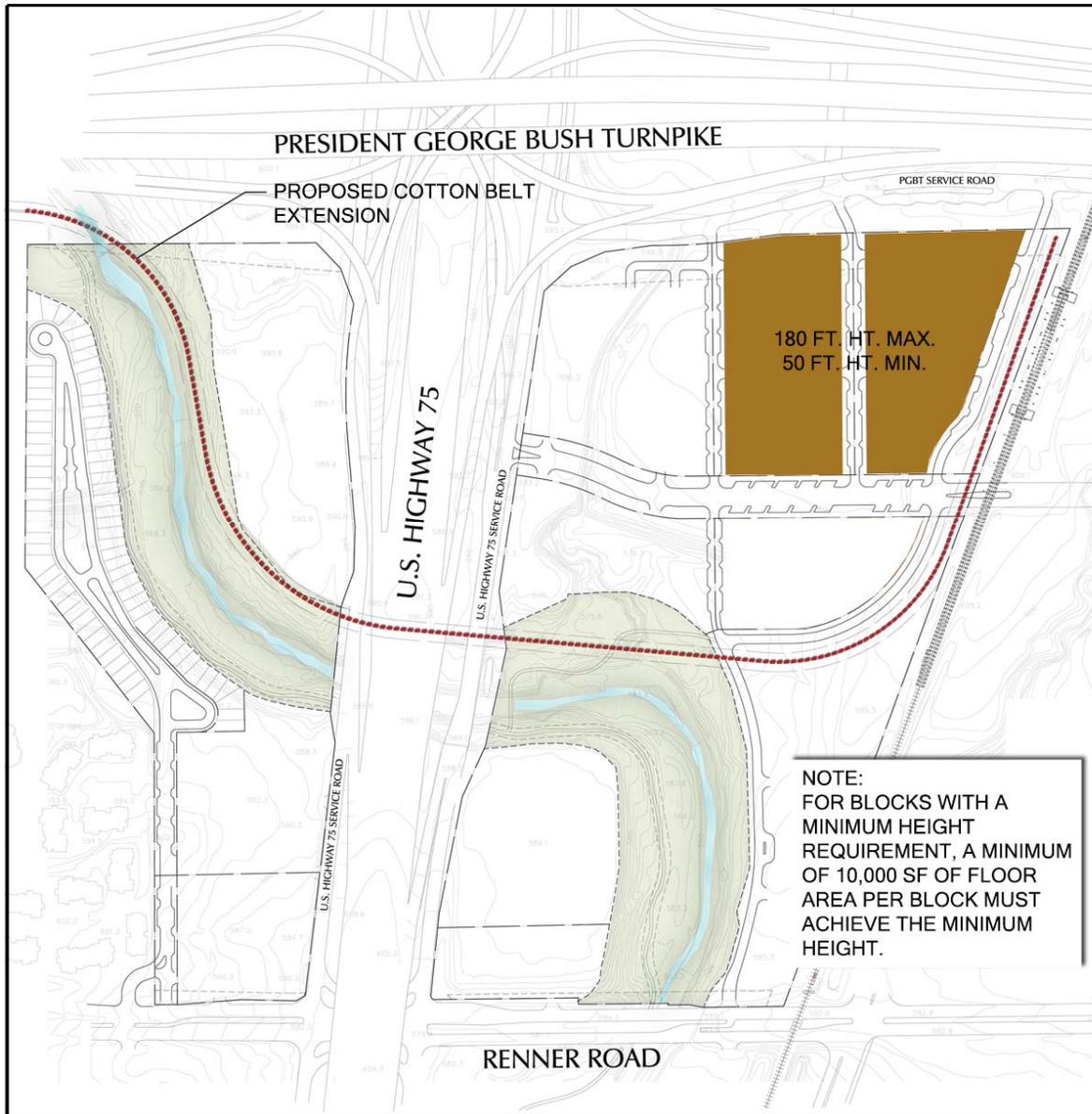
Non-residential uses	1 space/300 sq. feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

Parking driveway width	TXDOT standards on service road and 24 feet max on all C-TOPD - East Streets, except when drives may need to be wider to address service access or fire lane standards.	Ⓢ
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7.5 TOD Mid-Rise

Character Zone: TOD Mid-Rise

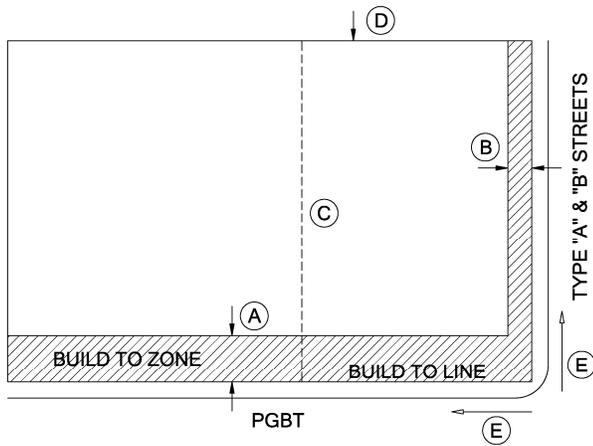


TOD Mid-Rise Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements.

TOD Mid-Rise Zone

7.5.1 Building Placement



REFER TO SECTION 9 FOR BTL LOCATION

Build-To Zone (BTZ)
(Distance from edge of pedestrian easement to edge of the BTZ)

PGBT	None	(A)
TOD Station	0 – 10 feet	(B)
Type "A," TOD Main Street	0 – 25 feet	(B)
Type "B," TOD Street	0 – 10 feet	(B)

Setback

Side (distance from property line)	0 feet (see #1)	(C)
Rear (distance from property line)	0 feet	(D)

Building Frontage

Building Frontage required along all street BTZ	90% (min.) (see #2 and #5)	(E)
Building Frontage required along PGBT and Service Road	0% (min.) (see #2 and #5)	(E)

7.5.2 Block Standards

Block face dimensions	No Maximum
Block perimeter	No Maximum

7.5.3 Building Height
Principal Building Standards

Building maximum	Shall be as established on the Regulating Plan
First floor to floor height	14 feet average per block with no point being less than 11 feet (see #2)
Ground floor finish level	12 inches min. above sidewalk (except for ground floor Retail Ready buildings)
Upper floor(s) height (floor-to-ceiling)	9.5 feet min. (excluding bathrooms, hallways, closets, and areas common to furr downs)

7.5.4 Ground Floor Retail Ready Requirements

Ground floors of all buildings as depicted on the Regulating Plan shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

7.5.5 Station Frontage Requirements

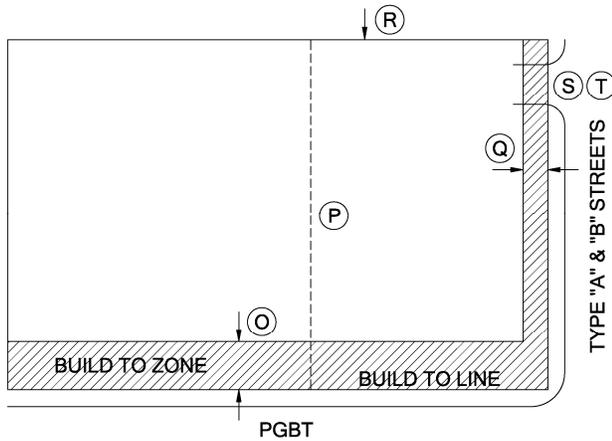
Ground floors of all buildings designated as Station Frontage on the Regulating Plan shall provide shaded areas to a minimum depth of 6 feet. Shaded devices may include arcades, galleries, awnings, canopies, etc.

Notes

- #1 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #2 – First floor heights shall not apply to parking structures; 14 feet average applies to Retail-Ready as depicted on the Regulating Plan.
- #3 – All buildings in the TOD Mid-Rise shall meet the Building Design Standards in Section 8.
- #4 – Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.
- #5 – Setbacks and build-to lines for recessed entry and arcade buildings shall be measured from the building façade line which contains the recessed entry or arcade.
- #6 – Required parking for all residential uses shall be provided for each blocked development.
- #7 – Article III, Chapter 21, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas.
- #8 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high street screen; furthermore, service areas shall be defined by a street section that is at least as high as the service equipment being screened. The street screen shall be of either the same building material as the principal structure of a the lot of masonry or a living screen that provides an opaque screen at maturity or a combination of masonry and living screen. Species shall be selected from the C-TOPD – East Planting List in Appendix B of the C-TOPD – East Code. The required street screen shall be located within the BTZ along the corresponding frontage.
- #9 – Surface parking as an ancillary use with an office motor court shall be permitted with a 0 – 50 foot setback from the building line.
- #10 – On-street parking may count toward meeting the parking requirement for non-residential uses.

TOD Mid-Rise Zone

7.5.6 Parking & Service Access



Driveways and off-street loading and unloading shall not be located on PGBT Service Road.

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Porte cocheres may be permitted on Type "A" Streets to provide drop-off and valet service.

Shared driveways and cross access easements are encouraged between lots to minimize curb cuts.

If driveway and/or off-street service loading and unloading access is provided from PGBT Service Road, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.

7.5.7 Encroachments

Canopies, signs, awnings and balconies may encroach over the BTZ, setback, and, sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.5.8 Applicability

Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

Parking Location

Surface/At Grade Parking

PGBT Service Road setback (see #1)	Shall be located behind the principal building	O
Type "A" Streets (see #1)	Shall be located behind the principal building	O
Type "B" Street setback	Shall be located behind the principal building	Q
Side setback (distance from property line)	0 feet min.	P
Rear setback (distance from property line)	0 feet min.	R

Above Grade Parking

Setback along Type "A" Street, Type "B" Street	0 feet min.
PGBT Service Road, side and rear setbacks (distance from property line)	0 feet min.

Upper Floors May be built up to the building line

Required Off-Street Parking Spaces

Non-residential uses	1 space/300 sq. feet (gross) (see #2)
Residential uses	1.5 space/unit

Driveways and Service Access

Parking driveway width	TXDOT standards on service road and 24 feet max on all C-TOPD – East Streets, except when drives may need to be wider to address service access or fire lane standards.	S
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Section 8. Building Design Standards

The Building Design Standards and Guidelines for the C-TOPD - East shall establish a coherent urban character and encourage enduring and attractive development. Development plans shall be reviewed by the City Manager or designee for compliance with the standards below.

The key design principles establish essential goals for development in the C-TOPD - East to ensure the preservation, sustainability, and visual quality of this unique environment. Buildings shall be located and designed so that they provide visual interest and create enjoyable, human-scaled spaces. The key design principles are:

- a. New buildings shall utilize building elements and details to achieve a pedestrian-oriented public realm.
- b. Compatibility is not meant to be achieved through uniformity, but through the use of variations in building elements to achieve individual building identity.
- c. Building facades shall include appropriate architectural details and ornament to create variety and interest.
- d. Open space(s) shall be incorporated to provide usable public areas integral to the urban environment.

8.1 General to all Character Zones

8.1.1 Building Orientation

- i. Buildings shall be oriented toward public streets, where the lot has frontage along public streets.
- ii. Primary entrance to buildings shall be located on the street along which the building is oriented. At intersections, corner buildings may have their primary entrances oriented at an angle to the intersection.
- iii. All primary entrances shall be oriented to the public sidewalk for ease of pedestrian access. Secondary and service entrances may be located from internal parking areas or alleys.

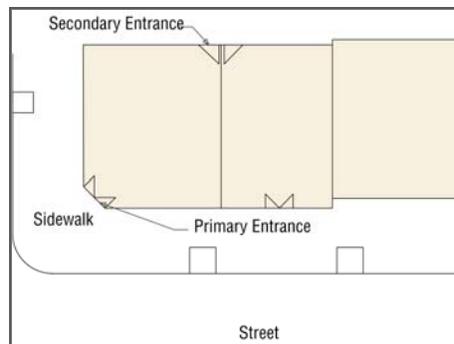


Figure showing required building orientation and location of primary entrances

8.1.2 Design of Parking Structures

- i. All frontages of parking structures located on Type “A” Streets shall not have parking uses on the ground floor to a minimum depth of 30 feet.
- ii. Parking structure facades on all public streets shall be designed with both vertical (façade rhythm of 20 feet to 30 feet) and horizontal (aligning with horizontal elements along the block) articulation.

- iii. Where above ground structured parking is located at the perimeter of a building with frontage along any public street; it shall be screened in such a way that cars on all levels are completely hidden from view, with frontages being precast spandrels (no cables with view). Parking garage ramps shall not be visible from any public street. Ideally, ramps should not be located along the perimeter of the parking structure. Architectural screens shall be used to articulate the façade, hide parked cars and shield lighting.
- iv. When parking structures are located at corners, corner architectural elements shall be incorporated such as corner entrance, signage and glazing.
- v. Parking structures and adjacent sidewalks shall be designed so pedestrians are clearly visible to entering and exiting automobiles.



Images showing appropriate design of Parking Structures

8.1.3 Design of Automobile Related Building Site Elements

- i. Drive-through lanes for commercial uses shall not be located along or visible from any public street within all Character Zones except Arterial Mixed Use and TOD Mixed Use. Drive-through lanes shall be hidden behind a Street Screen within Arterial Mixed Use and TOD Mixed Use.
- ii. All off-street loading, unloading, and trash pick-up areas shall be located along alleys or Type ‘B’ Streets unless permitted in the specific building form and development standards in Section 7. Any off-street loading, unloading, or trash pick-up areas shall be screened using a Street Screen that is at least as tall as the trash containers and/or service equipment it is screening at the BTZ. The Street Screen shall be made up of (i) the same material as the principal building or (ii) a living screen or (iii) a combination of the two.

8.2 Standards Specific to the TOD Core, TOD Mixed Use, Arterial Mixed Use, TOD Freeway Hi-Rise, & TOD Mid-Rise Character Zones:

8.2.1 Roof Form

- i. Buildings shall have simple, flat fronts with minimal articulations with flat or low pitched roofs with parapets (pitches 2.5:12 or lower). Corner hip roof elements and gable accents at the parapet may be permitted. Projecting mansard roofs shall not be permitted.



8.2.2 Façade Composition

- i. Storefronts on facades that span multiple tenants shall use architecturally compatible materials, colors, details, awnings, signage, and lighting fixtures.
- ii. Building entrances may be defined and articulated by architectural elements such as lintels, pediments, pilasters, columns, porticos, porches, overhangs, railings, balustrades, and others as appropriate. All building elements should be compatible with the architectural style, materials, colors, and details of the building as a whole. Entrances to upper level uses may be defined and integrated into the design of the overall building facade.
- iii. Buildings shall generally maintain the alignment of horizontal elements along the block.
- iv. Corner emphasizing architectural features, pedimented parapets, cornices, awnings, blade signs, arcades, colonnades and balconies may be used along commercial storefronts to add pedestrian interest.



Buildings with architectural features and storefront elements that add interest along the street.

- v. Buildings which are located on axis with a terminating street or at the intersection of streets shall be considered as feature buildings. Such buildings shall be designed with features which take advantage of that location, such as an accentuated entry and a unique building articulation which is off-set from the front wall planes and goes above the main building eave or parapet line.

8.2.3 Windows and Doors

- i. Windows and doors on public street (except alleys) fronting facades shall be designed to be proportional and appropriate to the architectural style of the building. First floor windows shall NOT be opaque, tinted or mirrored glass.
- ii. All ground floor front facades of buildings along public streets or Civic/Open Space shall have transparent storefront windows covering no less than 50% of the façade area.



Images showing appropriate window designs and proportions.

8.2.4 Building Materials

- i. At least 85% of each building's façade (excluding doors and windows) along any public street shall be finished in one of the following materials:
 - Masonry (brick, stone, stucco utilizing a three-step process, cast stone, glass or glass block, fiber cement panel, minimum ½ inch thickness, minimum 18 inches width).
- ii. No more than 15% of each façade along any public street shall use accent materials such as wood, fiber cement siding, architectural metal panel, split-face concrete block, tile, pre-cast concrete panels, or Exterior Insulating Finishing System (EIFS). EIFS may only be used 8 feet above the ground floor and is prohibited on all building elevations with the exception of its use for exterior trim and moulding features. The balance of all building elevations shall be a minimum 80% masonry content.



Images showing appropriate building materials within TOD Core, TOD Mixed Use, TOD Freeway Hi-Rise & TOD Mid-Rise.

- iii. Roofing materials visible from any public right-of-way shall be copper, factory finished standing seam metal, slate, synthetic slate, or similar materials.

Section 9. Street & Streetscape Design Standards

9.1 Generally: Streets in the C-TOPD - East District need to support the overall goal of a mixed use, compact, pedestrian oriented district. They should balance all forms of mobility while maximizing convenience for residents and visitors.

The Regulating Plan designates the required and recommended street network within the C-TOPD - East District. This section specifies the typical configuration of streets within the C-TOPD - East District. The specifications address vehicular lane width, parkway widths, R.O.W widths, number of travel lanes, on-street parking, and pedestrian accommodation. The character of streets in the C-TOPD - East District will vary based on the location. The service/access roads of President George Bush Turnpike (State Highway 190) and US 75 are under the purview of Texas Department of Transportation (TxDOT) while the remaining streets are city streets.

9.2 New Streets: This section specifies standards for all new streets in the C-TOPD - East District. New streets shall be based on the Mandatory or Non-Mandatory designation on the Regulating Plan.

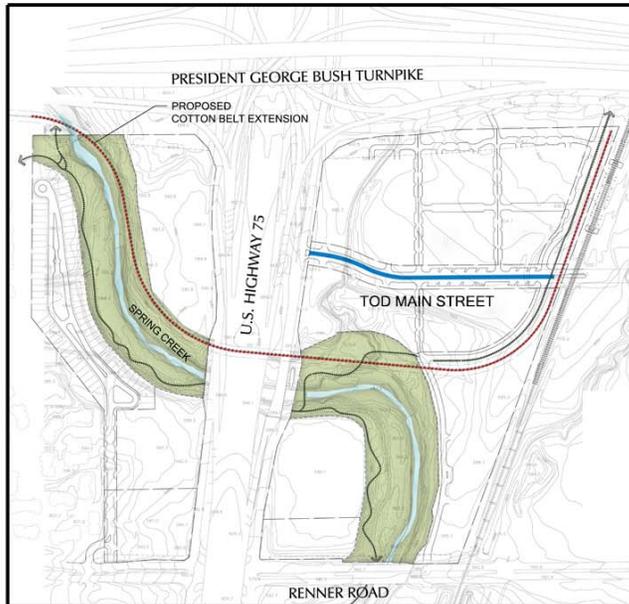
9.3 Street Classifications Established: Table 9.1 and associated cross sections shall establish the cross sections for each street type. The cross sections may be adjusted to fit existing contexts with the approval of the City Engineer. In addition, the proposed cross sections may be adjusted to meet the needs of the Uniform Fire Code as adopted by the City.

Table 9.1

Elements Street Classifications	Street Width (Recommended Minimum)		Number of Vehicular Lanes	Lane Widths for Vehicular Lanes	Number of Bike Lanes	On-Street Parking	Pedestrian Sidewalk Width (min.)	Parkway/ Tree Well
	R.O.W	Pedestrian Easement (both sides)						
TOD Main Street Type "A"	86 feet	16 feet	4	11 feet 14 feet		Yes, both sides, angled	16 feet	Tree well (5 feet X 5 feet)
TOD Street Type "B"	38 feet	13 feet	2	11 feet		Yes, both sides, parallel	8 feet	In bulb out
Parkway Type "B"	30 feet	13 feet	2	15 feet		No	8 feet	Parkway (5 feet)
TOD Station Type "A"	Variable	13 feet (west side)	2	10.5 feet		Yes, west side, parallel	8 feet (west)	Tree well (5 feet X 5 feet)
TOD Trail Connector/Type "B"	22 feet (fire lane only)	13 feet	2 (fire lane only)	11 feet		No	8 feet	Parkway (5 feet)

9.4 TOD Main Street

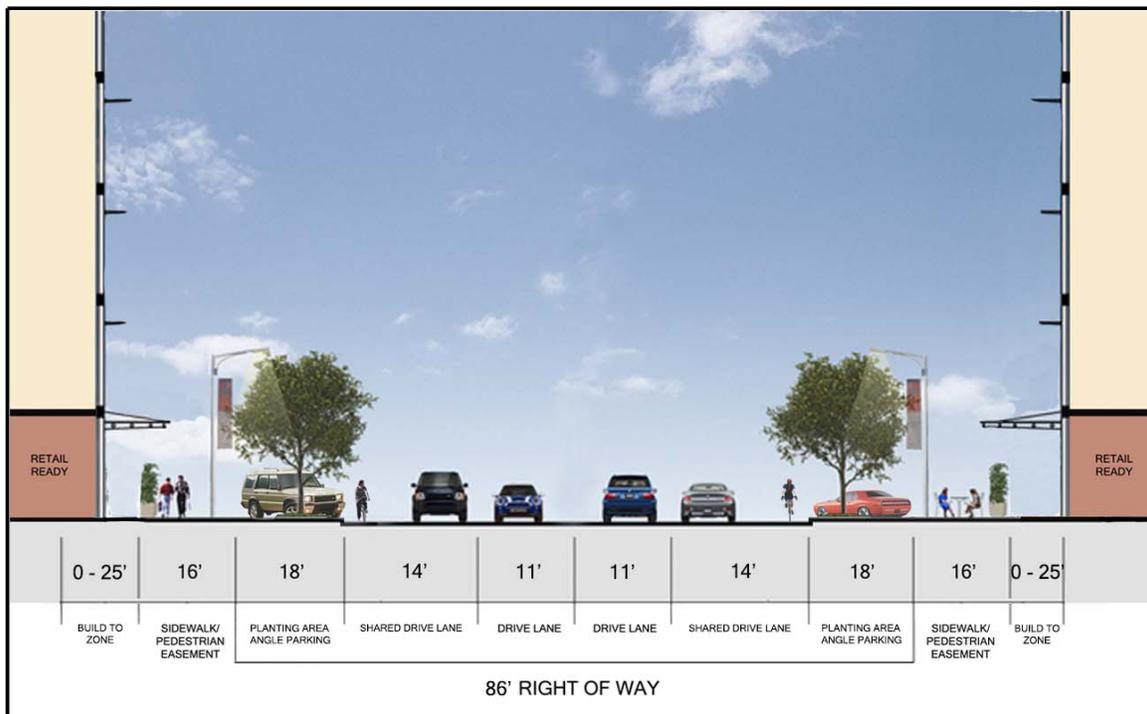
Location Map



TOD Main Street

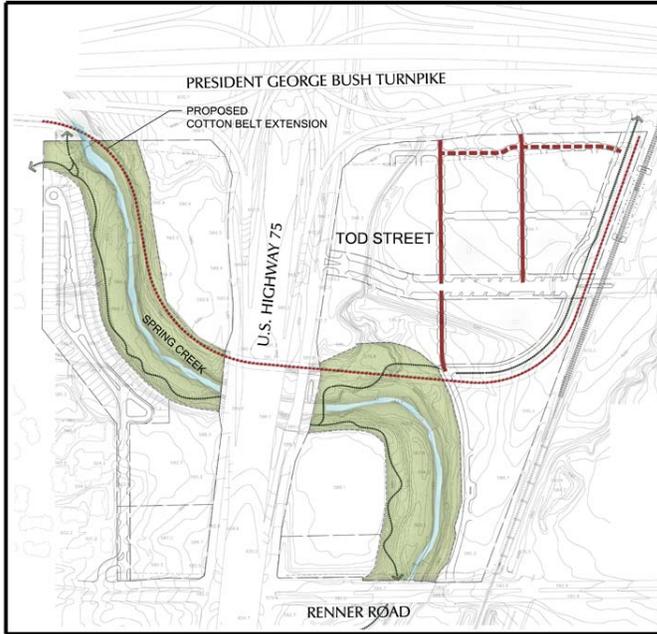
Street Type: A
ROW: 86 ft.
Design Speed: 25 MPH
Parking: Angle on both sides
Number of Vehicular Lanes: 4
Number of Bike Lanes: None
Curb Radius: 20 ft.
Walkway Type: Pedestrian Esmt.
Landscape Type: Street Trees between 40 ft. & 50 ft. avg.

Street Section



9.5 TOD Street

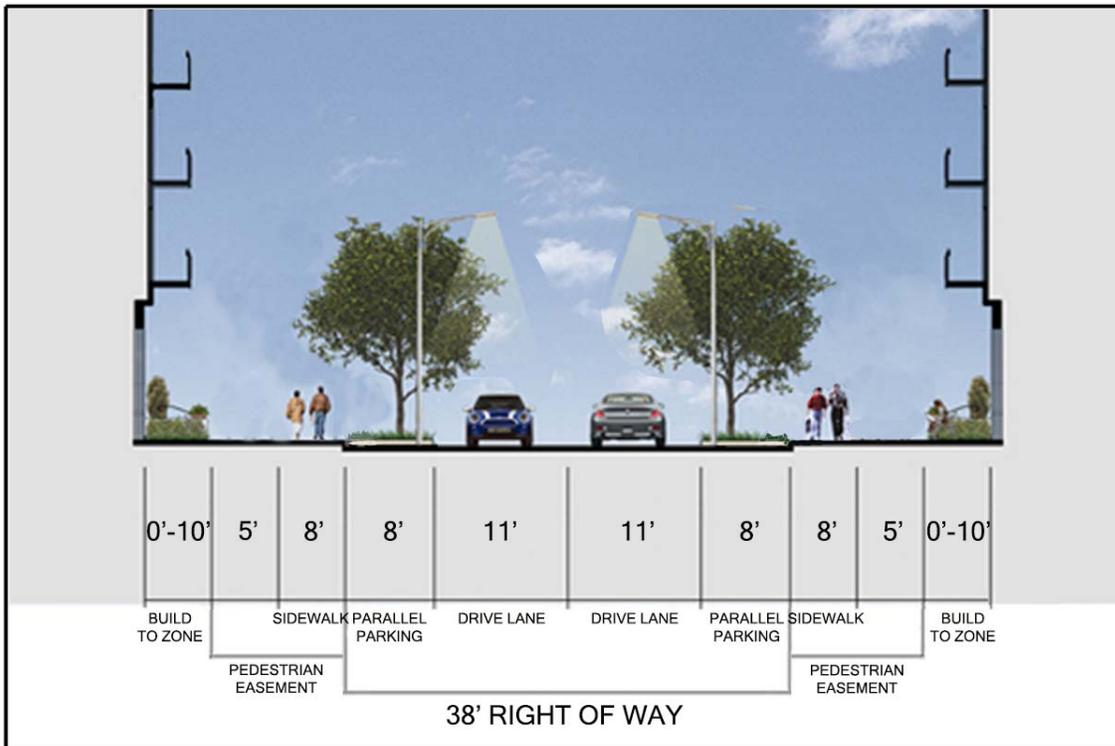
Location Map



TOD Street

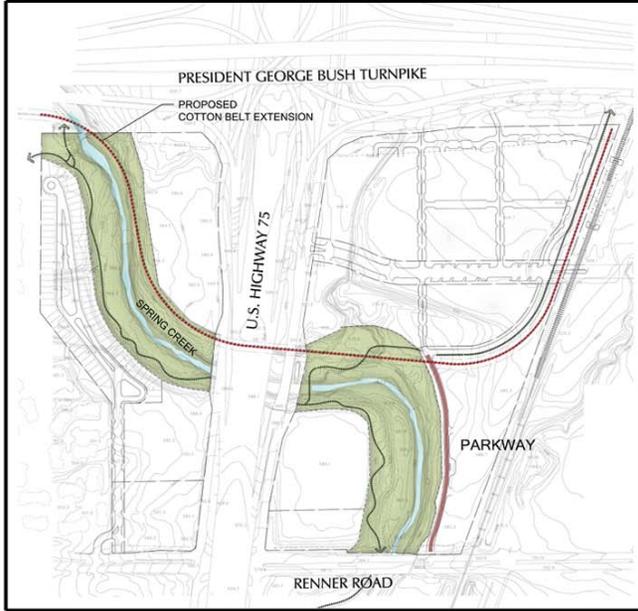
Street Type: B
ROW: 38 ft.
Design Speed: 25 MPH
Parking: Parallel on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: None
Curb Radius: 20 ft.
Walkway Type: Pedestrian Esmt.
Landscape Type: Street Trees 60 ft. avg.

Street Section



9.6 Parkway

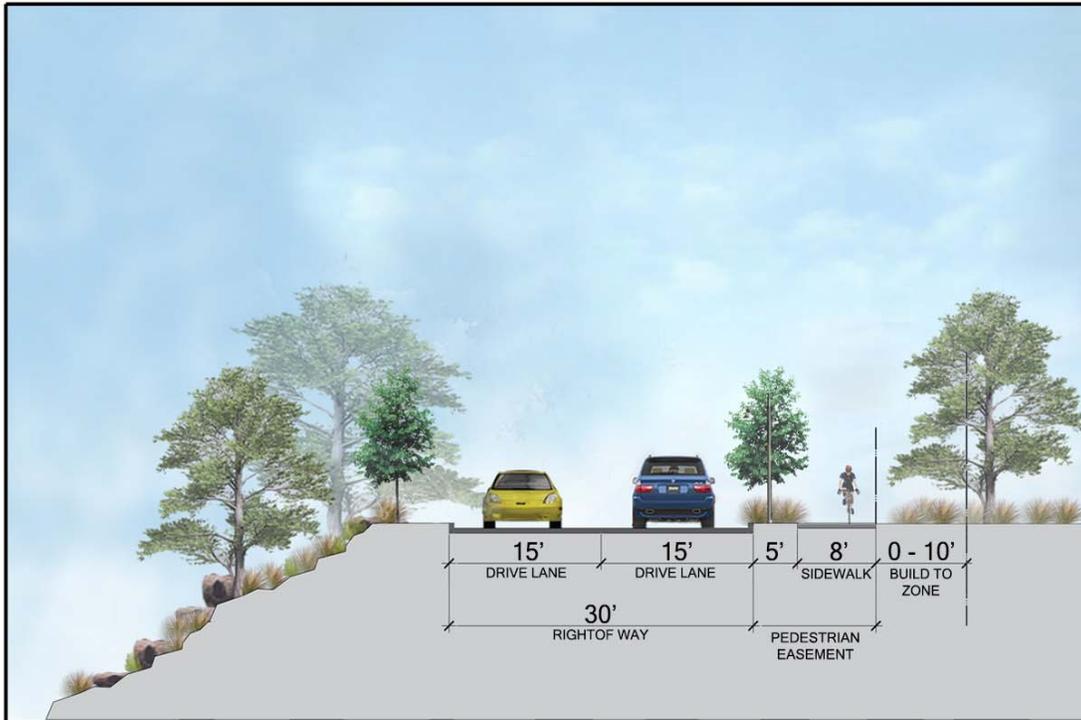
Location Map



Parkway

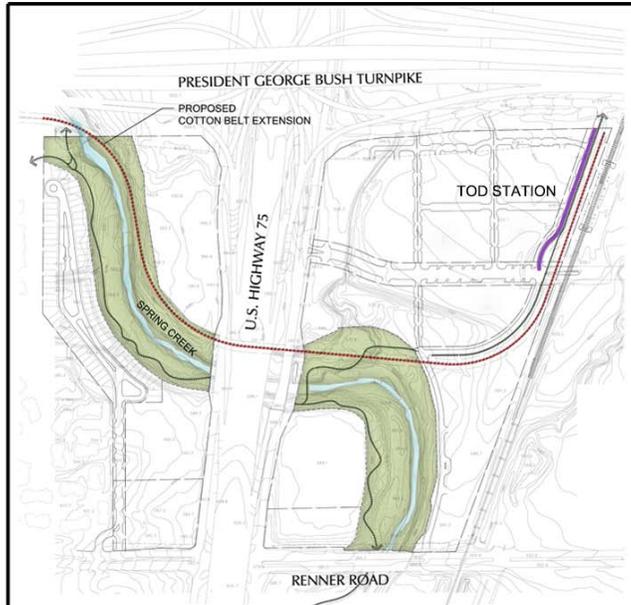
Street Type: B
ROW: 30 ft.
Design Speed: 25 MPH
Parking: None
Number of Vehicular Lanes: 2
Number of Bike Lanes: None
Curb Radius: 20 ft.
Walkway Type: Pedestrian Esmt.
Landscape Type: Street Trees between 40 ft. & 50 ft. avg.

Street Section



9.7 TOD Station

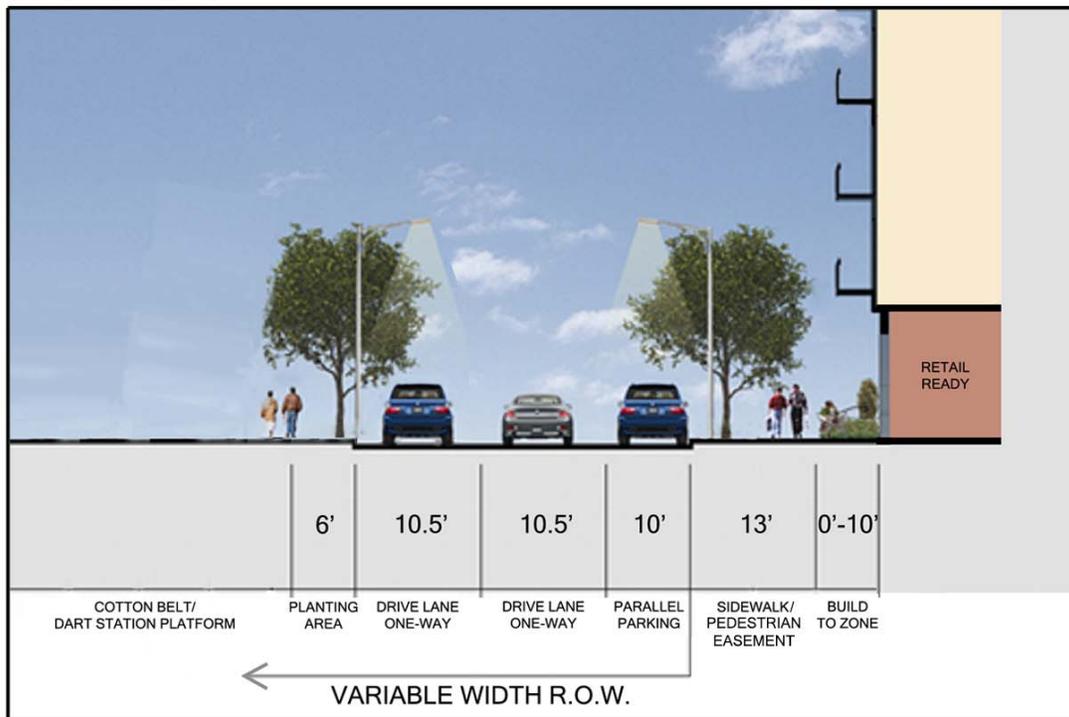
Location Map



TOD Station

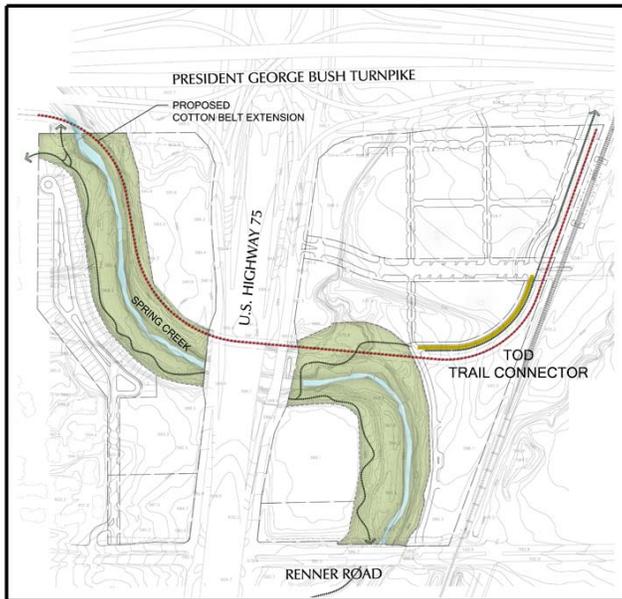
Street Type: A
ROW: Variable
Design Speed: 25 MPH
Parking: Parallel parking on west side
Number of Vehicular Lanes: 2
Number of Bike Lanes: None
Curb Radius: 20 ft.
Walkway Type: Pedestrian Esmt.
Landscape Type: Street Trees between 40 ft. & 50 ft. avg.

Street Section



9.8 TOD Trail Connector

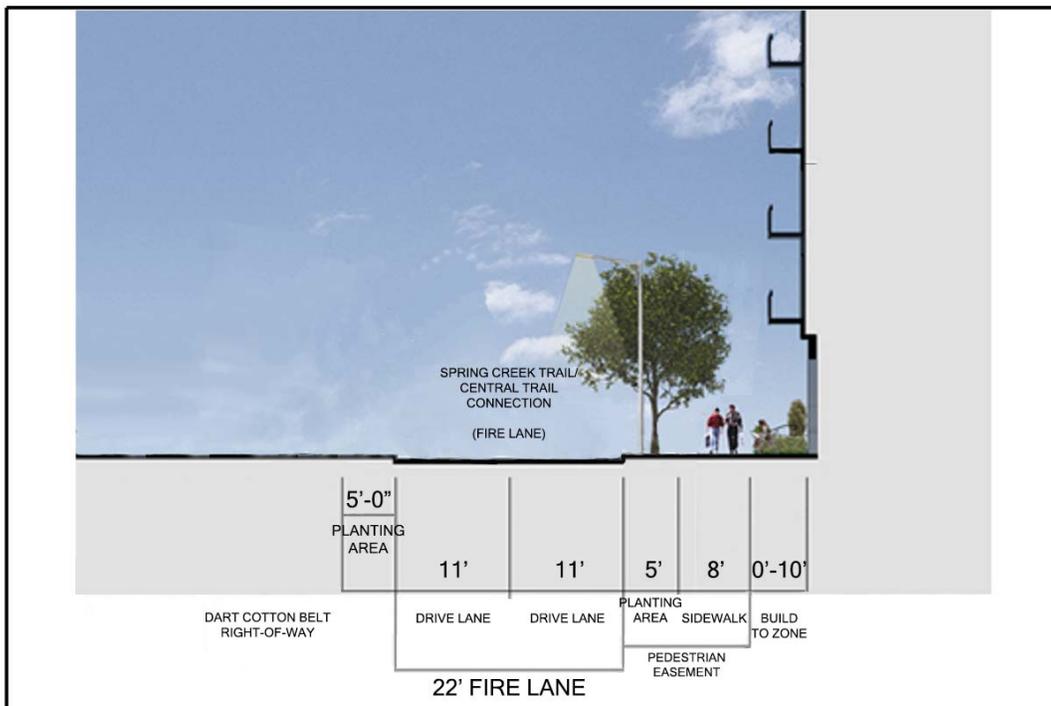
Location Map



TOD Trail Connector

Street Type: B
Fire Lane: 22 ft.
Design Speed: 25 MPH
Parking: None
Number of Vehicular Lanes: 2
Number of Bike Lanes: None
Curb Radius: 20 ft.
Walkway Type: Pedestrian Esmt.
Landscape Type: Street Trees between 40 ft. & 50 ft. avg.

Street Section



- 9.9 Streetscape & Landscape Standards: Streetscape standards shall apply to all streets within the C-TOPD - East. Streetscape standards shall address all elements between the building face and edge of the curb. Typical streetscape elements addressed are street trees, lighting, street furniture and pedestrian amenities, and materials. Maintenance of all landscape within the rights-of-way shall be by the property owners association established for the C-TOPD - East.
- 9.10 Street Trees and Landscaping (within the pedestrian easement):
- 9.10.1 Street trees shall be required on all C-TOPD - East Streets (except on alleys and PGBT access road.)
 - 9.10.2 Street trees shall be planted approximately 3 feet behind the curb line, except when they reside in a bulb out.
 - 9.10.3 Spacing shall be an average of 50 feet on center (measured per block face) along all streets.
 - 9.10.4 The minimum caliper size for each tree shall be 3 in. and shall be a minimum of 12 feet in height at planting. Each tree shall be planted in a planting area no less than 36 sq. feet; however, the tree well area may be no smaller than 25 sq. feet.
 - 9.10.5 Turf and groundcover: When clearly visible from the street and alleys, all unpaved ground areas shall be planted with low growing shrubs or ground cover, ornamental grasses, or a combination thereof. Turf grass must be installed as solid sod and not seeded on.
 - 9.10.6 Species shall be selected from the C-TOPD - East Planting List in Appendix B of this ordinance.
 - 9.10.7 Maintenance of all landscape materials shall meet the requirements of the City of Richardson Landscape Ordinance Requirements.
 - 9.10.8 Along arterials and highway access roads, street trees shall be planted within the required landscape parkway as per the City of Richardson Landscape Ordinance/Policies.
- 9.11 Street Furniture, Lighting, and Materials:
- 9.11.1 Pedestrian scale lighting shall be required along all C-TOPD - East streets (except on PGBT access road). They shall be no taller than 20 feet.
 - 9.11.2 Pedestrian scale and regular street lights shall be placed at uniform locations based on the placement of street trees and other street furniture to provide safety for both pedestrians and automobiles while limiting spill over and light pollution effects of such street lights. The placement and illumination intensity shall be subject to city approval at the time of development plan.
 - 9.11.3 The light standard selected shall be compatible with the design of the street and buildings.
 - 9.11.4 Trash receptacles and bike racks shall be required along all Type 'A' Streets. A minimum of one each per block face shall be required.
 - 9.11.5 Street furniture and pedestrian amenities such as benches are recommended along all Type 'A' Streets.
 - 9.11.6 All street furniture shall be located in such a manner as to allow a clear sidewalk passageway of a minimum of 6 feet.
 - 9.11.7 Materials selected for paving and street furniture shall be of durable quality and require minimal maintenance.

Section 10. Signage

Except as specifically listed below, all other signage and sign standards must comply with Chapter 18 of the City of Richardson Code of Ordinances, as amended.

10.1 For new signs, the standards in Table 10.1 shall apply and sign permits shall be approved administratively by the City of Richardson Building Official unless specifically noted in this section.

Table 10.1

Character Zone	TOD Core	TOD MU	ArterialMU	TOD Freeway	TOD Mid-Rise	Standard
Sign Type						
Wall (Building) Signs	P	P	P	P	P	<ul style="list-style-type: none"> For all ground floor commercial uses (retail, office, and restaurant): One sign per tenant space; area to be calculated at 1.5 sq. feet per linear foot of public street frontage for the tenant space with a maximum of 100 sq. ft per tenant. Second and upper floor commercial uses may also be permitted one second floor wall sign per tenant space per public street frontage; area to be calculated at 1.5 sq. feet per linear foot of second or upper floor frontage along that public street with a maximum of 125 sq. feet Institutional uses (non-profits and churches): One sign per tenant space; area to be calculated at 1.5 sq. feet per linear foot of public street frontage with a maximum of 100 sq. feet Live-Work and Home occupations: One sign limited to an area of 20 sq. feet max. Building sign may encroach a maximum of 12" on to a sidewalk while maintaining a vertical clearance of 8 feet from the finished sidewalk. Building signs may be internally or externally lit. Marquee signs as only permitted as specified below.
Monument Signs	NP	NP	P	P	NP	<ul style="list-style-type: none"> One monument sign per lot per lot street frontage (no more than 2 per lot separated by at least 100 feet) limited to a maximum of 50 sq. feet per sign face and 6 feet in height. Permitted only along PGBT access road, U.S 75 access road, and streets along TOD Freeway Hi-Rise.
Window Signs	P	P	P	P	P	<ul style="list-style-type: none"> Limited to 10% of the window area. The following shall be exempt from this limitation: Addresses, closed/open signs, hours of operation, credit card logos, real estate signs, and now hiring signs. Mannequins and storefront displays of merchandise sold. Interior directory signage identifying shopping aisles and merchandise display areas.
Building Blade Signs	P	P	P	P	P	<ul style="list-style-type: none"> One per building (commercial and mixed use buildings only), always on separate building face

Character Zone	TOD Core	TOD MU	ArterialMU	TOD Freeway	TOD Mid-Rise	Standard
						<ul style="list-style-type: none"> Area = 30 sq. feet maximum per sign face. May encroach a maximum of 6 feet over a sidewalk, but shall not encroach over any parking or travel lane. Building blade signs may be attached to the building at the corners of building or along any street facing façade above the first floor facade.
Tenant Blade Signs	P	P	P	P	P	<ul style="list-style-type: none"> One per commercial tenant space (retail, office, or restaurant use) Area = 16 sq.feet maximum per sign face May encroach a maximum of 4 feet over a public sidewalk, but shall not encroach over any parking or travel lane. Tenant blade signs shall be oriented perpendicular to the building façade and hung under the soffit of an arcade or under a canopy/awning or attached to the building façade immediately over the ground floor tenant space while maintaining a vertical clearance of 8 feet from the finished sidewalk.
Marquee Signs	P	P	P	P	P	<ul style="list-style-type: none"> Permitted for theatres, auditoriums, and other public gathering venues of 100 persons or more Marquee signs shall be attached to the building or located above or below a canopy only Area = 100 sq.feet maximum Message board may be changeable copy (electronic and non-electronic). Electronic message boards shall be non-flashing. Marquee signs shall not be permitted along Renner Road, PGBT access road and U.S. 75 access road.
For sale/for lease signs	P	P	P	P	P	<ul style="list-style-type: none"> Size is limited to 32 sq. feet per sign face All other standards are the same as City or Richardson Sign Regulations.
Address signs	P	P	P	P	P	Same as City of Richardson Sign Regulations
Temporary construction signs	P	P	P	P	P	One (1) free standing sign per lot during construction only; limited to 32 sq. feet
Banners	P	P	P	P	P	Same as City of Richardson Sign Regulations
Sandwich board signs	P	P	P	P	P	<ul style="list-style-type: none"> Permitted only for retail, service, or restaurant uses Limited to 12 sq. feet per sign face per storefront; Sign may not exceed 4 feet in height. A minimum of 6 feet of sidewalk shall remain clear. Chalkboards may be used for daily changing of messages. Readerboards (electronic and non-electronic) shall be prohibited. Sign shall be removed every day after the business is closed.

Character Zone	TOD Core	TOD MU	ArterialMU	TOD Freeway	TOD Mid-Rise	Standard
Light Pole Banners	P	P	P	P	P	<ul style="list-style-type: none"> Permitted only with approval of the Building Official. Max. 10 sq. feet per sign face. Limited to one per light pole All light pole banners shall be approved by the appropriate utility company prior to consideration by the Sign Control Board. Light pole banners shall be limited to publicize community-wide events, holiday celebrations, public art, and other city sponsored events.
Directory signs	P	P	P	P	P	<ul style="list-style-type: none"> Shall be allowed for all multi-tenant commercial and mixed use buildings only One directory sign per multi-tenant building limited to 12 sq. feet in area Design of the sign shall be integral to the façade on which the sign is to be affixed.
LED signs	P	P	P	P	P	<ul style="list-style-type: none"> Shall be covered by a lens or diffuser. Shall only be permitted as part of a Master Sign Plan.
Pole signs	NP	NP	NP	NP	NP	
Off-premises signs	NP	NP	NP	NP	NP	

10.2 An applicant has the option to establish unique sign standards including size, color, type, design, and location. Such applications shall be reviewed as “Master Sign Plans” by the City Manager or designee and are subject to approval of the City Plan Commission. In evaluating a Master Sign Plan, City Plan Commission shall consider the extent to which the application:

- 10.2.1 Promotes consistency among signs within a development thus creating visual harmony between signs, buildings, and other components of the property;
- 10.2.2 Enhances the compatibility of signs with the architectural and site design features within a development;
- 10.2.3 Encourages signage that is in character with planned and existing uses thus creating a unique sense of place; and
- 10.2.4 Encourages multi-tenant commercial uses to develop a unique set of sign regulations in conjunction with development standards.

Section 11. Civic/Open Space Standards

11.1 The design of Civic/Open Space shall be regulated by the Civic/Open Space standards herein which shall create a network of open spaces that recognizes the natural qualities of the area while providing a range of both passive and active recreational opportunities. These opportunities may be accommodated in a variety of spaces ranging from large regional parks to neighborhood-scaled plazas. The open space network will be serviced by an interconnected network of trails and paths for pedestrians and bicyclists alike.

11.2 Open Space Standards – Spring Creek Corridor



The required open space, Spring Creek Corridor, as designated on the Regulating Plan, will create an important public space that connects the community within the C-TOPD - East and allows for active and passive recreation. Spring Creek Corridor shall primarily be naturally landscaped with many places to sit on benches or low walls. Appropriate civic elements, fountains or open shelters may be included. TOD Freeway Hi-Rise, TOD Mixed Use and Arterial Mixed Use buildings shall front onto and activate this space.

Typical Characteristics

General Character

- Large, open space
- Spatially defined by landscaping and building frontages
- Paths, trails, open shelters, lawns, trees and shrubs naturally disposed
- May be lineal, following the trajectories of natural corridors

Location and Size

Location and size shall be as shown on the Regulating Plan (Appendix A).

Typical Uses

- Passive, and unstructured active recreation
- Casual seating/picnicking

11.3 Plaza Standards



Plazas add to the vibrancy of streets within the more urban zones and create formal open spaces available for civic purposes and commercial activity. Building frontages shall define these spaces. The landscape should consist primarily of hardscape. If trees are included, they should be formally arranged and of appropriate scale. Casual seating, along with tables and chairs, should be provided. Plazas typically should be located at the intersection of important streets.

Typical Characteristics

General Character

- Formal open space
- Primarily hardscape surfaces
- Trees and shrubs optional
- Spatially defined by building frontages

Location and Size

Location and size shall be as shown on the Regulating Plan (Appendix A).

Typical Uses

- Commercial and civic uses
- Casual seating
- Tables and chairs for outdoor dining

11.4 Pedestrian Passage Standards



Pedestrian passages create intimate passageways through buildings at designated locations. These paths provide direct pedestrian access to residential addresses and create unique spaces for frontages to engage and enter off of. Pedestrian passages allow for social and commercial activity to spill into the public realm. Pedestrian passages should consist of a hardscape pathway activated by frequent entries and exterior stairways. The edges may simply be landscaped with minimal planting and potted plants.

Typical Characteristics

General Character

- Hardscape pathway
- Frequent entries and frontages
- Exterior stairways
- Defined by building frontages
- Minimal planting and potted plants
- Maintain the character of surrounding buildings

Standards

Min. Width 12 feet

Typical Uses

- Pedestrian connection and access
- Casual seating

11.5 Multi-Use Trail Standards



The multi-use trail provides an important place for active recreation and creates a connection to regional paths and biking trails. The multi-use trail will help activate connections between the open spaces and the uses throughout the C-TOPD - East. The multi-use trail may have different character as it passes through Spring Creek Corridor and as it aligns with public streets. Within Spring Creek Corridor, the trail shall be naturally disposed with low impact paving materials so there is minimal impact to the existing creek bed and landscape. Along public streets, the trail shall have a more formal disposition with a paved trail and regular landscaping.

Typical Characteristics

General Character

Spring Creek Corridor Multi-Use Trail:

Naturally disposed landscape

Low impact paving

Trees lining trail for shade

Appropriately lit for safety

Public Street Multi-Use Trail:

Hardscape Path

Formally disposed pedestrian furniture, landscaping and lighting

Trees lining trail for shade

Standards

Min. Width 12 feet

Location shown on the Regulating Plan (Appendix A).

Typical Uses

Active and passive recreation

Casual seating

11.6 Playground Standards



Playgrounds shall be permitted in parks to provide open space designed and equipped for the recreation of children. These playgrounds should serve as quiet, safe places – protected from the street and typically located where children are not required to cross major roads to access. Playgrounds may be fenced. An open shelter, play structures or interactive art and fountains may be included with landscaping between. Shaded areas and seating shall be provided.

A large playground may be incorporated into the park.

Playground equipment shall serve all ages, based on City of Richardson Parks and Recreation Standards, as amended. Playground equipment and design shall be reviewed and approved by the City of Richardson, including the Parks and Recreation Department.

Typical Characteristics

General Character

- Focused toward children
- Fenced with minimal exits (non-mandatory)
- Open shelter
- Shade and seating provided
- Play structure, interactive art or fountains

Standards

- Min. Size N/A
- Max. Size N/A
- As described by civic space type in which playground is located
- Protected from traffic
- No service or mechanical equipment

Typical Uses

- Active and passive recreation
- Unstructured recreation
- Casual seating

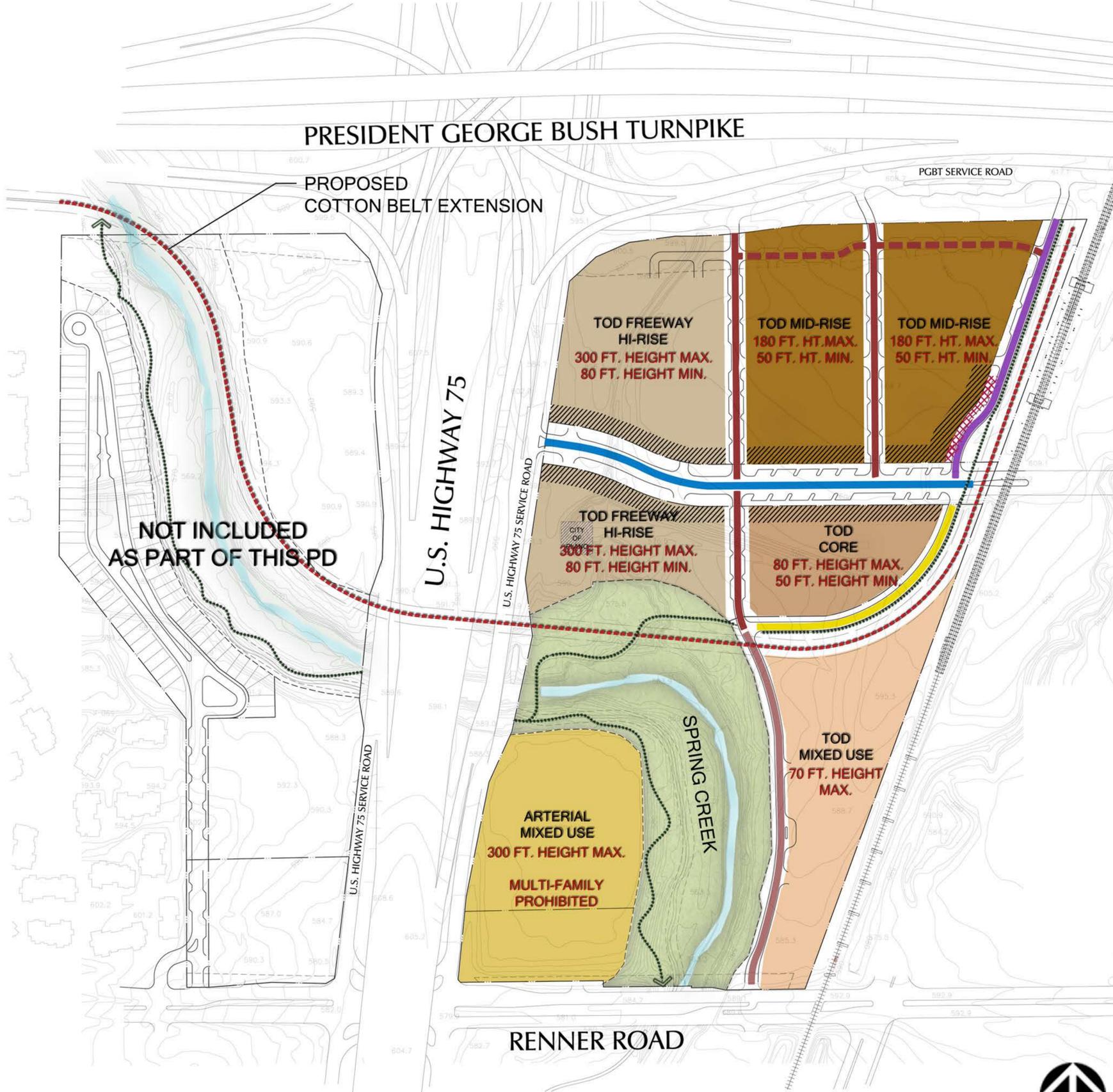
11.7 Ancillary Structure Standards



Ancillary structures should be formal in character and generally related to but clearly subordinate to surrounding buildings. Each individual structure should keep in character with the style of nearby buildings. Typically, these structures are located at prominent locations within an appropriate civic space. Ancillary structures located in more urban zones may have minor commercial uses, such as small food or news vendors, but may also serve as civic elements for general public use with more passive activities.

Typical Characteristics	
General Character	
Formal character	
Relating to style of surrounding buildings	
One or more open sides	
Covered or providing shade	
Small, stand alone structure	
Located within Park, Green, Square or Plaza	
Standards	
Min. Size	N/A
Max. Size	N/A
Typical Uses	
Civic purposes	
Minor commercial uses	
Casual seating/picnicking	

Please see following page attached.

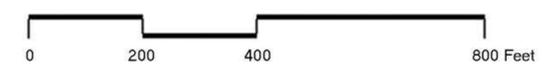


LEGEND	
CHARACTER ZONES	STREETS - GENERAL
<ul style="list-style-type: none"> TOD Core -- 3.2 Acres TOD Mixed Use -- 5.3 Acres TOD Freeway Hi-Rise -- 16.1 Acres Arterial Mixed Use -- 6.7 Acres TOD Mid-Rise -- 9.1 Acres <p>Note: Acreages are approximate.</p>	<ul style="list-style-type: none"> Solid line denotes mandatory street Dashed line denotes non-mandatory street
	STREET TYPES
	<ul style="list-style-type: none"> Street Type "A" - TOD Main Street Street Type "A" - TOD Station Street Type "B" - TOD Trail Connector Street Type "B" - Parkway Street Type "B" - TOD Street
SPECIAL REQUIREMENTS	
<ul style="list-style-type: none"> Station Platform Frontage Ground Floor Retail Ready <p>NOTE: For blocks with a minimum height requirement, a minimum of 10,000 sf of floor area per block must achieve the minimum height.</p>	
	OTHER
	<ul style="list-style-type: none"> Multi-Use Trail
	OPEN SPACE TYPES
	<ul style="list-style-type: none"> Mandatory Open Space

APPENDIX A: REGULATING PLAN

Caruth Properties

Transit Oriented Planned Development - East



Appendix B**Planting List**

The following lists contain all species approved for use in the C-TOPD - East District. It contains native and acceptable adapted species. Other species that are drought tolerant and adaptive may be used for planting within the C-TOPD - East District. The use of alternative species may be permitted with the approval of the Zoning Administrator.

CANOPY/STREET TREE LIST

<u>Common Name</u>	<u>Botanical Name</u>
Live Oak	<i>Quercus virginiana</i>
Red Oak	<i>Quercus shumardi</i>
Bald Cypress	<i>Taxodium distichum</i>
Sweetgum	<i>Liquidambar styraciflua</i>
Cedar Elm	<i>Ulmus crassifolia</i>
Lacebark Elm	<i>Ulmus parvifolia</i>
Bigtooth Maple	<i>Acer grandidentatum</i>
Caddo Maple	<i>Acer saccharum 'Caddo'</i>
Texas Ash	<i>Fraxinus texensis</i>
Bur Oak	<i>Quercus macrocarpa</i>
Chinquapin Oak	<i>Quercus muhlenbergii</i>
Escarpment Live Oak	<i>Quercus fusiformis</i>
Ginkgo	<i>Ginkgo biloba</i>

ORNAMENTAL TREE LIST

<u>Common Name</u>	<u>Botanical Name</u>
Yaupon Holly	<i>Ilex vomatoria</i>
Crape Myrtle	<i>Lagerstromia indica</i>
Deciduous Yaupon	<i>Ilex decidua</i>
Southern Crabapple	<i>Malus app.</i>
Chinese Pistache	<i>Pistacia chinensis</i>
Mexican Plum	<i>Prunus Mexicana</i>
Wax Myrtle	<i>Myrica carifera</i>
Chitalpa	<i>Chitalpa tashkentensis</i>
Deciduous Holly	<i>Ilex decidua</i>
Desert Willow	<i>Chilopsis linearis</i>
Eve's Necklace	<i>Sophora affinis</i>

SHRUBS LIST

<u>Common Name</u>	<u>Botanical Name</u>
Dwarf Nandina	<i>Nandina domestica 'nana'</i>
Dwarf Burford Holly	<i>Ilex cornuta 'burfordi nana'</i>
Abelia Grandiflora	<i>Abelia grandiflora</i>
Barberry	<i>Barberry spp.</i>
Yucca (Red, Yellow or Soft Tip)	<i>Hesperaloe parviflora</i>
Texas Sage	<i>Leucophyllum frutescans</i>
Indian Hawthorn	<i>Raphiolepis indica</i>

Dwarf Crape Myrtle	<i>Lagerstromia indica 'nana'</i>
Dwarf Yaupon Holly	<i>Ilex vomitoria 'nana'</i>
Pampas Grass	<i>Cortaderia selloana</i>
Black-Eyed Susan	<i>Rudbeckia hirta</i>
Dwarf Wax Myrtle	<i>Myrica pusilla</i>
Needlepoint Holly	<i>Ilex cornuta 'Needle Point'</i>
Knockout Rose	<i>Rosa 'Knock Out'</i>
Rosemary	<i>Rosmarinus officinalis</i>

GROUND COVER/VINES LIST

<u>Common Name</u>	<u>Botanical Name</u>
Asian Jasmine	<i>Trachelosperum Asiaticum</i>
Big Blue Liriope	<i>Liriope muscari</i>
Mondograss	<i>Ophiopogon japonicus</i>
Purple Winter Creeper	<i>Euonymum coloratus</i>
Santolina	<i>Santolina virens</i>
Trumpet Vine	<i>Campsis radicans</i>
Virginia Creeper	<i>Parthenocissus quinquefolia</i>
Lady Banks Rose	<i>Rosa banksiaw lutea</i>
Confederate Jasmine	<i>Trachelospermum jasminoides</i>
Crossvine	<i>Bignonia capreolata</i>
Evergreen Wisteria	<i>Millettia reticulata</i>
Lantana 'New Gold'	<i>Lantana camara 'New Gold'</i>
Liriope 'Silver Dragon'	<i>Liriope muscari 'Silver Dragon'</i>
Prostrate Rosemary	<i>Rosmarinus officinalis prostrata</i>
Sweet Autumn Clematis	<i>Clematis terniflora</i>

ORNAMENTAL GRASSES LIST

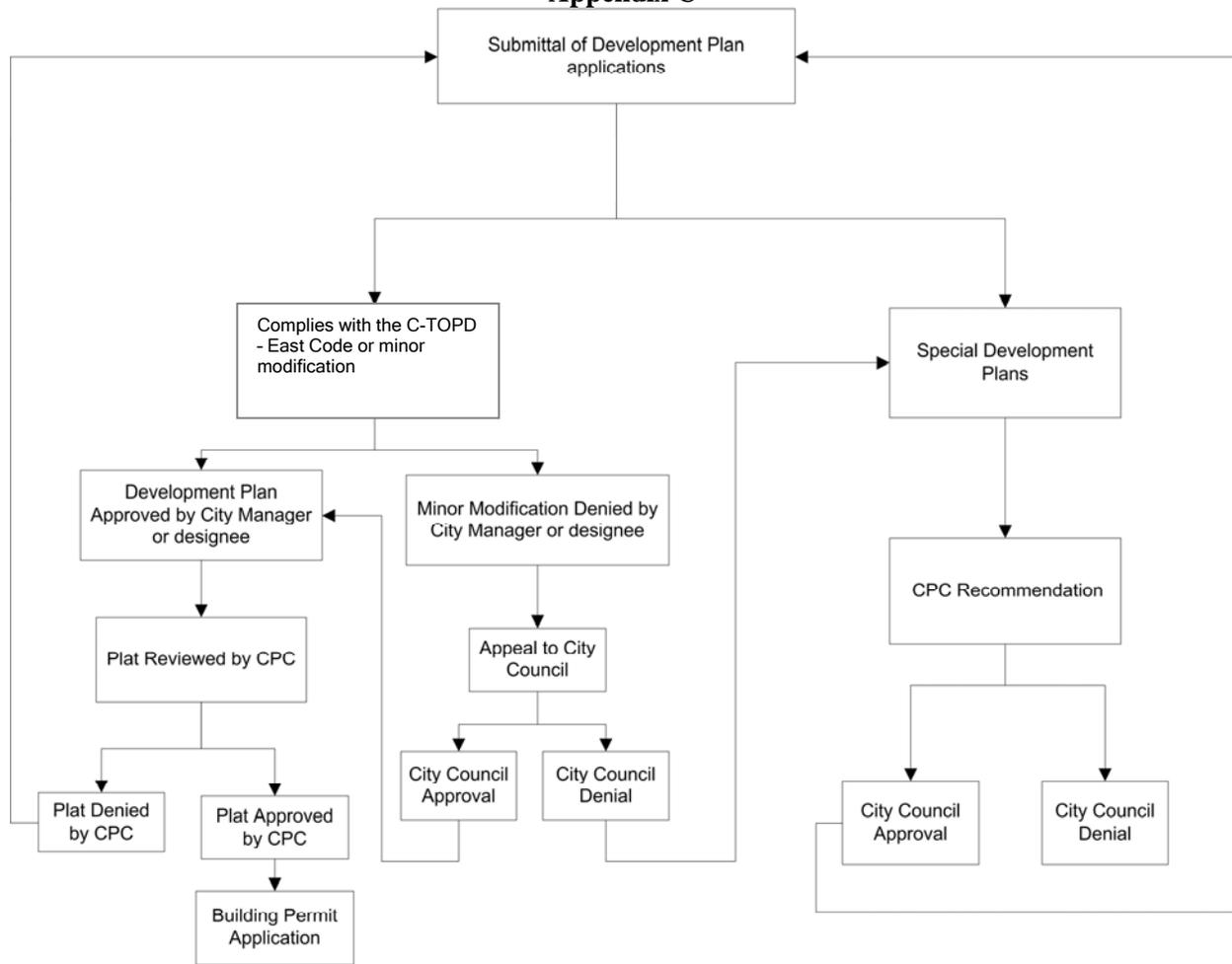
<u>Common Name</u>	<u>Botanical Name</u>
Dwarf Fountain Grass 'Little Bunny'	<i>Pennisetum alopecuroides 'Little Bunny'</i>
Dwarf Maiden Grass	<i>Miscanthus sinensis 'Adagio'</i>
Fountain Grass	<i>Pennisetum alopecuroides</i>
Inland Sea oats	<i>Chasmanthium latifolium</i>
Maiden Grass	<i>Miscanthus sinensis 'Gracillimus'</i>
Mexican Feather Grass	<i>Stipa tenuissima</i>
Muhly Grass	<i>Muhlenbergia capillaris</i>
Weeping Lovegrass	<i>Eragrostis curvula</i>

TURF

<u>Common Name</u>	<u>Botanical Name</u>
Bermuda	<i>Cynodon dactylon</i>
St. Augustine	<i>Stenotaphrum secundatum</i>
Zoysia	<i>Zoysia tenuifolia</i>

These plantings may be placed in Civic/Open Spaces or used to meet the private landscaping requirements of the Code. The applicant shall select drought tolerant, low maintenance, and adaptable shrubs and ground cover based on the placement on the site subject to approval by the City.

Appendix C



C-TOPD - East: Caruth Properties Transit Oriented Planned Development

CPC: City Plan Commission

CC: City Council

SDP: Special Development Plan

Note: Development Plans include site plans, building elevations, landscape plans, civil engineering plans, and all plats