

Exhibit B
(Adopted by Ordinance 3806)

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Section 1. Purpose and Intent:

The purpose of the Bush Central Station Planned Development Code, hereafter known as the BCS-PD Code, is to support development of the Bush Central Station into a pedestrian-oriented, mixed-use urban development environment, with convenient access to rail transit, shopping, employment, housing, and regional retail services. The goal of the Bush Central Station is to promote an efficient, compact land use pattern; encourage pedestrian activity; reduce the reliance on private automobiles; and provide a more functional and attractive community through the use of recognized principles of urban design.

- 1.1 Economic Development – The Bush Central Station and corresponding standards are created to support economic development, sustainable tax base, and job creation by establishing adjacency predictability of private development that supports and leverages investment in and around the Bush Central Station.
- 1.2 Implement the Design Goals of the Bush Central Station – The objective of the Bush Central Station is to foster a major regional employment center with significant regional retail and residential uses within convenient walking distance from the existing transit station and potential transfer station to the Cotton Belt rail line. The existing transit station provides a connection to 12 member cities of Dallas Area Rapid Transit (DART) including Dallas. Development within this area would be of high intensity, accommodating large scale office and retail users while providing for moderate scale mixed use and residential within portions of the Bush Central Station.
- 1.3 Establish Specific Development Standards – The BCS-PD Code implements the vision for the Bush Central Station as established in the City’s Comprehensive Plan and the Bush Central Station Regulating Plan, hereafter known as the Regulating Plan (Appendix A). The Regulating Plan shall provide guidance to property owners, developers, and the City on the form, character, and intensity of future development in the Bush Central Station. Creation of different Character Zones within Bush Central Station enables specific site and locational standards to be enumerated and applied. Clear graphic standards are provided for location, height, and building elements. Such standards promote sustainability, public welfare, walkable mixed use development, housing variety and transportation choice.

Section 2. Components of the Code:

- 2.1 This BCS-PD Code shall apply to the Bush Central Station unless otherwise specified in this Code. Development of property within the Bush Central Station shall comply with all applicable development standards set forth in this Code. The components of this BCS-PD Code consist of:
 - 2.1.1 Bush Central Station Regulating Plan: The Bush Central Station Regulating Plan, hereafter known as the “Regulating Plan”, is its official zoning map. It identifies the applicable character zones within the Bush Central Station including:
 - i. Character Zones – The Bush Central Station is divided into different “Character Zones”. A Character Zone creates a distinct urban form within that Zone which is different from urban forms in other Character Zones. Each Character Zone shall establish use and development standards including height, bulk, building and parking location, and functional design. The Regulating Plan classifies all lots within the Bush Central Station into one of four Character Zones.
 - ii. Special Frontage Standards – The Special Frontage Standards establish exceptions and special conditions for all buildings along designated frontages. Special Frontage Standards may be Mandatory or Non-Mandatory. Mandatory Special Frontage Standards shall apply in addition to the underlying Character Zone standards. Non-Mandatory Special Frontage Standards may be applied at the election of the developer and are not required.

- iii. Street Designations by Street Type– The Street Designations illustrate the design, configurations, and development context for all streets within the Bush Central Station. The street classification addresses vehicular lane widths, number of lanes, pedestrian accommodation, street tree requirements, on-street parking, and parkway and median standards (streetscape standards). In addition, streets are distinguished by the appropriate development context by denoting them on the Regulating Plan as Type ‘A’ or Type ‘B’ Streets.
- iv. Mandatory Street Network – The Mandatory Street Network specifies the future streets needed to implement the Bush Central Station Regulating Plan. The Mandatory Streets shall be required and shall generally meet the locational and connectivity goals of the Regulating Plan. Their design shall be guided by the Street Type Specifications.
- v. Mandatory Civic/Open Space – The Mandatory Civic/Open Space areas shown on the Regulating Plan designate the locations of proposed civic/open spaces (including parks, plazas, greens, and squares).
- vi. Non-Mandatory Street Network – The Non-Mandatory Street Network indicates locations of suggested, but not required streets within the Bush Central Station Regulating Plan. These streets are not mandatory, but at the election of the developer and their design shall be guided by the Street Type Specifications.
- vii. Non-Mandatory Civic/Open Space – The Non-Mandatory Civic/Open Space designation indicates the locations of desired, but not required civic/open spaces (including parks, plazas, greens, and squares) to implement the Regulating Plan. Non-Mandatory Civic/Open Spaces are not required, but at the election of the developer.
- viii. Special Destination – The Special Destination indicates an area on the Regulating Plan identified for specific uses with applicable standards as established in Section 5.7 of this Code.

2.1.2 Development Standards: The BCS-PD Code (the text portion of this Code) enumerates the development standards with text and graphics for Character Zones, Special Frontage, building form, civic open space, landscape, building design, signage, lighting, and all related standards for all streets, public and private development.

Section 3. Administration

This section sets forth the provisions for reviewing and approving development applications within the Bush Central Station. The intent is to ensure that all development is consistent with the provisions of this Code. All sections of this Code shall be applied during the review process.

3.1 The development standards under Articles XIII-A thru XVI and Article XXI-C of the City of Richardson Comprehensive Zoning Ordinance, as amended, shall not apply to the Bush Central Station except as specifically referenced herein. Development standards not addressed in this ordinance shall be governed by the City of Richardson Comprehensive Zoning Ordinance to the extent they are not in conflict with the intent or text of the BCS-PD Code.

3.2 Sign Standards under Chapter 18, as amended, of the City of Richardson Code of Ordinances, shall not apply to the Bush Central Station except as specifically referenced herein.

3.3 Using this Code:

The following basic steps should be followed to determine the uses and development standards applicable on property within the Bush Central Station:

3.3.1 Locate the subject property on the Bush Central Station Regulating Plan.

3.3.2 Identify:

- i. the Character Zone in which the property is located;

- ii. the Street Type designation along all its street frontages; and,
 - iii. any Special Frontage Requirements that may be applicable to the subject property.
- 3.3.3 Review the Schedule of Uses by Character Zone as listed in Table 6.1 to determine allowed uses.
- 3.3.4 Examine the corresponding zone standards in the Building Form and Development Standards in Section 7 to determine the applicable development standards.
- 3.3.5 Refer to Section 5.5 and 5.6 for Special Frontage Standards.
- 3.3.6 Refer to Section 8 for Building Design Standards.
- 3.3.7 Refer to Section 9 for Street Type and Streetscape Standards.

The information from the above listed steps explains where the building will sit on the lot, the limits on its three dimensional form, the range of uses, and the palette of materials that will cover it.

- 3.4 Development within the Bush Central Station that complies with the provisions of this Code shall follow the City's development process as outlined in Chapter 21, Article II of the City of Richardson's Code of Ordinances and shall be approved by the City Manager or designee (see Appendix C for flow chart of the review process). In addition to complying with applicable City regulations that are not in conflict with this Code, the applicant shall provide the information required to adequately show compliance with this Code.
- 3.5 Standard for Approval of development plans: If a development plan conforms to the standards set forth in this Code and applicable City regulations not in conflict with this Code, the development plan shall be approved. Upon request by an applicant, the authority charged with approving the development plan shall certify the reason(s) for an action taken on a development plan.
- 3.6 The City Manager or designee shall be responsible for the following:
- 3.6.1 Reviewing development plan applications for compliance with the requirements of BCS-PD Code.
 - 3.6.2 Approving development plan applications that are in compliance with the requirements of the BCS-PD Code.
 - 3.6.3 Making determinations on the applications and interpretations of standards in this Code.
 - 3.6.4 Approving revisions to previously approved development plans that comply with this Code and all applicable city ordinances.
 - 3.6.5 Approving any minor modifications to the approved Regulating Plan per Section 3.8.
 - 3.6.6 Recommendations on any SDP applications to the City Plan Commission (CPC) and City Council.
- 3.7 Special Development Plans: A request for a modification to any of the standards of this Code other than minor modifications permitted under Sections 3.8 shall be reviewed and processed as Special Development Plans.
- 3.7.1 Special Development Plans (SDP) are intended to allow applicants development flexibility to address specific market opportunities and/or contexts. An application for a Special Development Plan shall be processed as an amendment to the zoning ordinance under Article XXIX of the City of Richardson Zoning Ordinance and may only be considered by the City Council (CC) after the CPC has made a recommendation. The City Manager or designee shall review, make recommendations on any SDPs, and shall forward all SDP applications to the CPC. In evaluating an SDP, CC may consider the extent to which the application meets any of the following:
- i. provides an alternative "Master Plan" approach by consolidating multiple properties to create a predictable, market responsive development for the area; or
 - ii. fits the adjoining context by providing appropriate transitions.

3.8 Minor Modifications to the BCS-PD Code:

The City Manager or designee shall have the authority to approve a request for minor modifications to BCS-PD Code that:

- 3.8.1 Does not materially change the circulation and building location on the site;
- 3.8.2 Does not increase the building area permitted under this Code;
- 3.8.3 Does not change the relationship between the buildings and the street;
- 3.8.4 Does not allow a use not otherwise authorized in this Code;
- 3.8.5 Does not allow greater height of any building or reduction of any parking requirement established in this Code; or
- 3.8.6 Changes to established street cross sections per Table 3.1 below and Section 9 of this Code.

The City Manager or designee shall also have the authority to approve minor modifications outlined in Table 3.1. Any appeals to the decisions of the City Manager on minor modifications shall be heard by the City Council. Any City Council denials of minor modifications or any changes beyond those that meet the criteria above, the thresholds established in Table 3.1, shall be processed as an amendment to this Code under Article XXIX of the City of Richardson Comprehensive Zoning Ordinance.

Table 3.1 Minor Modifications Allowed

<i>Standard</i>	<i>Minor Modification Allowed</i>	<i>Comments</i>
Area/boundary of Character Zones (including any Mandatory Civic/Open Spaces)	No more than a 15% change (increase or decrease) in the area of any Character Zone (aggregate or per block)	<ul style="list-style-type: none"> • Shall not eliminate any Character Zone • 15% measurement shall be based on the total area of that specific Character Zone within the entire Bush Central Station • For the purposes of this code, any increase in any Character Zone that results from the elimination of a non-mandatory street shall not count towards the 15% threshold.
Location of any Mandatory Street	Location shall not move more than 100' in any direction	<ul style="list-style-type: none"> • Shall maintain the connectivity intended by the Regulating Plan
Building Form and Development Standards		
<ul style="list-style-type: none"> • Street Setback Line 	No more than a 10% change in the SSL as established in the corresponding Character Zone Building Form and Development Standards	<ul style="list-style-type: none"> • Changes to the Street Setback Lines may only be due to any changes to the street cross sections or change in the width of a sidewalk • In no case shall the sidewalk be less than 6 feet in width.
<ul style="list-style-type: none"> • Build to zones/setbacks 	No more than a 20% change in the maximum or minimum setback.	
<ul style="list-style-type: none"> • Building Frontage 	No more than a 15% reduction in the required building frontage along each block of a Type 'A' Street	<ul style="list-style-type: none"> • Any reduction in the required building frontage shall be to accommodate Porte-cochères for drop-off and pick-up.
<ul style="list-style-type: none"> • Street screen 	Waiver of Street screen requirement along a Type 'B' Street	<ul style="list-style-type: none"> • Requirement for a street screen may only be waived along a Type 'B' Street along the frontage of any interim surface parking lot (off-street) that is intended to be in-filled with a parking structure. • In no case shall any portion of the surface parking have frontage along a Type 'A' Street without a required street screen • In no case shall the (off-street) surface parking lot be located at a street intersection for a minimum depth of 30' along each street (regardless of the Street Type).
Street Cross Sections	Cross sections of new streets may be adjusted with respect to number of lanes, lane widths, on-street parking configuration, pedestrian accommodation, and street tree planting	<ul style="list-style-type: none"> • Any changes in the street cross sections shall be based on specific development context such as vegetation, natural features, drainage, and fire access and is subject to approval by the City.

3.9 Periodic Review Requirement: The City Manager or his/her designee shall convene an annual City Council work session with the property owners and/or the Property Owners Association (POA) of all the property within the planned development for the purpose of receiving project status updates on the development of the property. The project updates shall include, but are not limited to, total square footage of developed property, square footage of non-residential development, number of residential units, mix of land uses and traffic/transportation related items.

Section 4. Definitions

In addition to Definitions in Article I of the City of Richardson Comprehensive Zoning Ordinance, the following terms shall have the corresponding interpretations.

Arcade means a portion of the main façade of the building that is at or near the Street-Setback Line and a colonnade supports the upper floors of the building. Arcades are intended for buildings with ground floor commercial or retail uses and the arcade may be one or two stories.

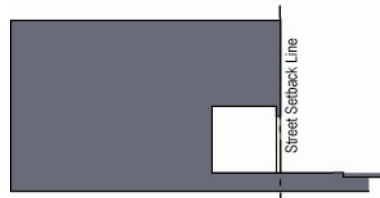


Image of an arcade

Attics/Mezzanines means the interior part of a building contained within a pitched roof structure or a partial story between two main stories of a building.

Auto-Related Sales and Service Uses means establishments that provide retail sales and services related to automobiles including, but not limited to, cars, tires, batteries, gasoline, etc.

Block Face Dimensions means the linear dimension of a block along one of its street frontages.

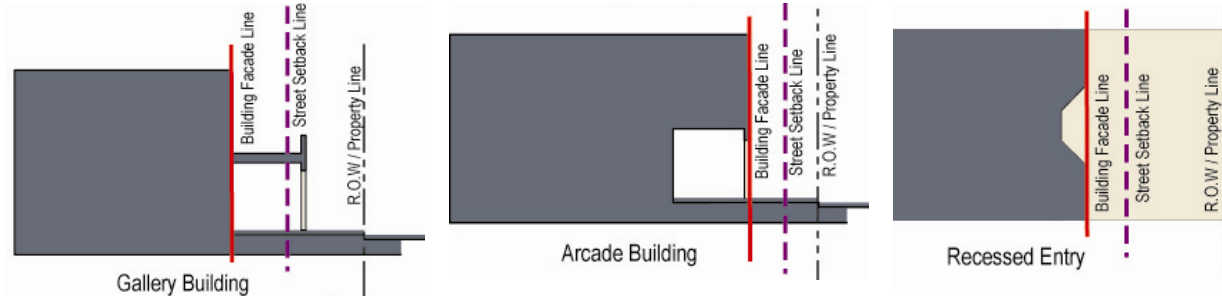
Block Perimeter means the aggregate dimension of a block along all of its street frontages.

Block means the aggregate of lots, pedestrian passages and rear alleys, circumscribed on all sides by streets.

Build-to Line means the line at which the principal building's front façade shall be built.

Build-to Zone (BTZ) means the area within which the principal building's front façade is to be built.

Building Façade Line means the vertical plane along a lot where the building's front façade is actually located.



Building Façade Line Illustrations

Building Form Standards means the standards established for each Character Zone that specify the height, bulk, orientation, and elements for all new construction and redevelopment.

Building Frontage means the percentage of the building’s front façade that is required to be located at the front Build-to Line or Zone as a proportion of that lot’s frontage along that public street. Parks, plazas, squares, improved forecourts, and pedestrian breezeway frontages shall be considered as buildings for the calculation of building frontage.

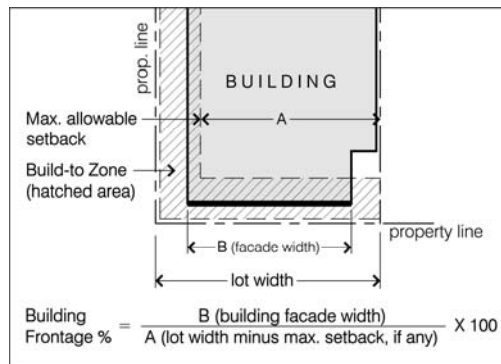


Image showing how a lot’s building frontage is calculated.

Character Zone means an area within the Bush Central Station that creates a distinct urban form different from other areas within the Bush Central Station. Character Zones are identified in the Regulating Plan.

City Manager means the City Manager of the City of Richardson or his/her designee.

Civic/Open Space means publicly accessible open space in the form of parks, courtyards, forecourts, plazas, greens, pocket parks, playgrounds, etc. They may be privately or publicly owned. For all residential uses, privately accessible open spaces such as courtyards, porches, and balconies may also be considered as Civic/Open Space for the purposes of this ordinance. Building façades facing a Civic/Open Space shall be treated as a Type ‘A’ Street frontage.

Green means a civic/open space intended for unstructured recreation, spatially defined by landscaping rather than building frontages.

Park means a civic/open space that is a preserve largely available for unstructured recreation.

Plaza means a primarily hardscaped civic/open space with formal landscaping, available for civic purposes and commercial activities. A plaza shall be spatially defined by buildings.

Playground is a civic/open space designed and equipped for children’s recreation. A playground may be fenced and may include an open shelter. Playgrounds may be located within residential areas and may be placed within a block. They may be included in other civic/open spaces.

Comprehensive Plan means the City of Richardson Comprehensive Plan that establishes the blueprint for the long-term growth and development of the City as adopted on the effective date of this Code.

Development Plan means any submittal as defined by Chapter 21, Article II of the City of Richardson’s Code of Ordinances.

Encroachment means any structural or non-structural element such as a sign, awning, canopy, terrace, or balcony that breaks the plane of a vertical or horizontal regulatory limit, extending into a Setback, into the Public R.O.W, or above a height limit.

Gallery means an extension of the main façade of the building that is at or near the front property line and the gallery may overlap the public sidewalk.

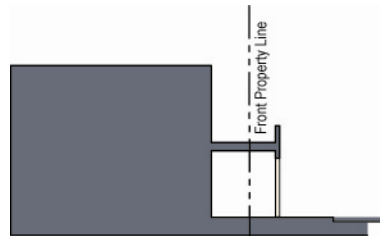


Image of a Gallery

Kiosk means a small temporary or permanent structure often open on one or more sides used for sales in civic/open spaces.

Live-Work Unit means a dwelling unit that is also used for work purposes, provided that the ‘work’ component is restricted to the uses of professional office, artist’s workshop, studio, or other similar uses and is located on the street level and constructed as separate units under a condominium regime or as a single unit. The ‘live’ component may be located on the street level (behind the work component) or any other level of the building. Live-work unit is distinguished from a home occupation otherwise defined by this ordinance in that the work use is not required to be incidental to the dwelling unit, non-resident employees may be present on the premises and customers may be served on site.

Living Screen means a Street Screen composed of landscaping in the form of vegetation.

Main Street Frontage means the special frontage requirement along identified Type A Street frontages as indicated in the Regulating Plan.

Master Sign Plan means a unique sign plan to implement a specific vision for a portion or all of the development that meets Section 10.2 of this Code.

Minor Modification means any changes to the BCS-PD Code that meet the threshold criteria established in Section 3.8 and Table 3.1.

Non-Mandatory Civic/Open Space means plaza, green, square, or park area designated on the Regulating Plan which is shown as a suggested feature within the Bush Central Station. The Non-Mandatory Plaza is not required and is at the election of the developer.

Non-Mandatory Neighborhood Frontage means the special frontage option to maintain a neighborhood compatible frontage as indicated in the Regulating Plan. The Non-Mandatory Neighborhood Frontage is not required and is at the election of the developer.

Non-Mandatory Pedestrian Passage means a pedestrian access area designated on the Regulating Plan which is shown as a suggested feature within the Bush Central Station. The Non-Mandatory Pedestrian Passage is not required and is at the election of the developer.

Non-Mandatory Street means a street that is designated on the Regulating Plan which is shown as a suggested street within the Bush Central Station. The Non-Mandatory Street is not required and is at the election of the developer.

Pedestrian Easement means a grant of use of private property for pedestrian access and use.

Regulating Plan means the Zoning Map attached hereto as Appendix A that shows the Character Zones, Civic Spaces, location of Special Frontages, Streets, and other Special Requirements applicable to the Bush Central Station subject to the standards in this Code. For the purposes of this Code, the Regulating Plan shall also be the Concept Plan for the Bush Central Station.

Residential Loft means a residential unit that is built to Retail Ready standards and adapted for residential uses.

Retail Ready means space constructed at a minimum ground floor height as established in each character zone which may be used for noncommercial uses and can be converted into retail/commercial use. Prior to the issuance of a certificate of occupancy for a retail/commercial use in a Retail-Ready space, the space must comply with all building and construction codes for that use. The intent of Retail-Ready space is to provide the flexibility of occupying a space in accordance with market demand and allowing the use in such space to change to retail/commercial uses accordingly.

Retail Sales Retail establishments are the final step in the distribution of merchandise. They are organized to sell in small quantities to many customers. Establishments in stores operate as fixed point-of-sale locations, which are designed to attract walk-in customers. Retail establishments often have displays of merchandise and sell to the general public for personal or household consumption, though they may also serve businesses and institutions. Some establishments may further provide after-sales services, such as repair and installation. Included in, but not limited to this category, are durable consumer goods sales and service, consumer goods, other grocery, food, specialty food, beverage, dairy, etc, and health and personal services.

Service Uses means a category for limited personal service establishments which offer a range of personal services that include (but not limited to) clothing alterations, shoe repair, dry cleaners, laundry, health and beauty spas, tanning and nail salons, hair care, etc.

Sign, Building Blade means a pedestrian-oriented sign that is affixed perpendicular to the corner of a building or to the front façade of a building above the ground floor to provide identification for the whole building.



Image of a Building Blade Sign

Sign, Marquee means a sign structure placed over the entrance to a theatre or other public gathering venue. It has signage stating either the name of the establishment or the name of the event, artist, and other details of the event appearing at that venue. The marquee is often identifiable by a surrounding cache of light bulbs, usually yellow or white, that flash intermittently or as chasing lights. Marquee signs may often be combined with Building Blade signs.



Image of a Marquee sign with a Building Blade Sign

Sign, Monument means any sign which is connected to the ground and which has no clear space for the full width of the sign between the bottom of the sign and the surface of the ground. A monument sign may include a sign face and sign structure, and may also include a sign base and sign cap.



Image of a Monument Sign

Sign, Sandwich Board means a portable sign consisting of two panels of equal size, which are hinged at the top or one panel with a support and placed on the ground or pavement so as to be self-supporting.



Images of sandwich board signs.

Sign, Tenant Blade means a smaller pedestrian-oriented sign that is affixed perpendicular to the building façade under a canopy or awning or immediately over a tenant space and provides identification for individual tenants within a building.



Image of a Tenant Blade Signs

Slip Road means a drive that extends within the property and parallel to either Renner Road or Plano Road. The Slip Road is generally located within close proximity to these main roads as identified on the Regulating Plan.

Slip Road Frontage means the frontage along a Slip Road.

Special Development Plan means a development application that meets Section 3.7 of this Code.

Special Frontage Requirements means standards applied to certain blocks as indicated in the Regulating Plan in order to address specific requirements and transitions based on street frontage and adjacency in addition to the underlying Character Zone standards.

Station Platform Frontage means the special frontage requirement on blocks with frontage along the rail station platform as indicated in the Regulating Plan.

Street-Setback Line means the imaginary line located at the outside edge of the sidewalk and measured from the center line of the street from which all build-to lines and build-to zones are measured from.

Street Screen means a freestanding wall or living screen built along the BTZ or in line with the building façade line along the street. It may mask a parking lot or a loading/service area from view or provide privacy to a side yard and/or strengthen the spatial definition of the public realm.



Image of a combination masonry and living street screen

Street Type means a specific designation for streets in the Bush Central Station that establishes a certain character and cross-sections to improve walkability within the Bush Central Station.

Street Network means the Mandatory and Non-Mandatory network for new and existing streets within the Bush Central Station as established in the Regulating Plan.

Transition Zones means the areas with specific adjacencies within which certain limitations on building heights apply as established for each Character Zone.

Tree Well means an unpaved area around the trunk of a tree within the sidewalk area that is either landscaped with ground cover or covered with a tree grate.



Example of a tree well with a tree grate



Example of a tree well with landscaping

Type ‘A’ Street means the streets identified as such on the Regulating Plan. Type ‘A’ Streets are the primary pedestrian streets and buildings along Type ‘A’ Streets shall be held to the highest standard of pedestrian-oriented design.

Type ‘B’ Street means the streets identified as such on the Regulating Plan. Type ‘B’ Streets are intended to primarily accommodate access to parking, service, and other auto-related functions.

Section 5. The Regulating Plan

5.1 The Regulating Plan (Appendix A) is hereby adopted as the official zoning map for the Bush Central Station.

5.2 Character Zones Established – the following Character Zones are established. The boundaries of the specific Character Zones shall be established in the Regulating Plan.

5.2.1 TOD Core: The TOD Core provides the most opportunity for the highest intensity development. It is the area that has significant development impact and the highest pedestrian activity due to its adjacency to the transit station. The TOD Core consists of the highest density

- and height, with the greatest variety of uses. Development within the TOD Core Zone shall meet the Building Form and Development Standards in Section 7.1 of this Code.
- 5.2.2 TOD Mixed Use: The TOD Mixed Use is the area adjacent to the TOD Core that is intended for high intensity commercial and residential uses in addition to supporting retail and restaurant uses. Development within the TOD Mixed Use Zone shall meet the Building Form and Development Standards in Section 7.2 of this Code.
- 5.2.3 Arterial Mixed Use: Arterial Mixed Use is intended to provide appropriate transitions to major regional roadways while taking advantage of the arterial frontage for limited auto-oriented sites. The Arterial Mixed Use frontage also acts as a gateway into the Bush Central Station by providing an attractive street frontage for the overall development. Development within the Arterial Mixed Use Zone shall meet the Building Form and Development Standards in Section 7.3 of this Code.
- 5.2.4 Highway Mixed Use: Highway Mixed Use is intended to provide an appropriate transition into the Bush Central Station from the President George Bush Turnpike (SH 190) access road. This area is also intended for high intensity development. Development within the Highway Mixed Use Zone shall meet the Building Form and Development Standards in Section 7.4 of this Code.
- 5.3 Street Designations By Street Type Established – The Regulating Plan shall establish the following Street Designations.
- 5.3.1 Type ‘A’ Streets Established – Type ‘A’ Streets are intended to be the primary pedestrian streets and buildings along Type ‘A’ Streets shall be held to the highest standard of pedestrian-oriented design. The Type ‘A’ Streets are TOD Main Street, TOD Street Type A, Parkway, Park Avenue and Court Street as identified in the Regulating Plan.
- 5.3.2 Type ‘B’ Streets Established – Type ‘B’ Streets are intended to balance pedestrian orientation with automobile orientation. Buildings along Type ‘B’ Streets may be permitted to accommodate some service and auto-related functions. The Type ‘B’ Streets are TOD Avenue and TOD Street Type B as identified in the Regulating Plan. In addition, Slip Roads provide a functional transition from the suburban arterials that bound the Bush Central Station to the urban street network offered within the development. The Slip Roads are designated along Plano Road and Renner Road and shall be considered as Type ‘B’ Streets for the purpose of development frontage standards.
- 5.4 Mandatory Streets by Street Type – The Street Network indicates Mandatory and Non-Mandatory streets needed to implement the Bush Central Station Regulating Plan. The Regulating Plan designates the type, classification, and location of streets. All new streets in the Bush Central Station shall meet the street design standards established in Section 9 herein.
- 5.5 Mandatory Special Frontage Requirements – In order to address specific requirements and transitions based on street frontage and adjacency, the following Special Frontage Requirements as established in the Regulating Plan shall apply:
- 5.5.1 Main Street Frontage: Ground floors of all buildings designated as Main Street Frontage on the Regulating Plan shall not be occupied by residential units and/or lodging rooms in hotels to a minimum depth of 30 feet as measured from the front building line.
- 5.5.2 Station Platform Frontage: Ground floors of all buildings designated as Station Frontage on the Regulating Plan shall provide shaded areas to a minimum depth of 6 feet. Shaded devices may include arcades, galleries, awnings, canopies, etc.
- 5.6 Non-Mandatory Neighborhood Frontage Requirements – In order to allow the development of a unique neighborhood enclave of brownstones, townhomes and live-work units within the Bush Central Station, the blocks along the Park Avenue have an optional Neighborhood Frontage designation for a minimum depth of 30 feet from the front building façade line. All standards of the

underlying Character Zone shall apply to the blocks designated as Neighborhood Frontage with the exception of height which shall be limited to a maximum of sixty (60) feet along this frontage. In addition, the requirement for ground floors to be retail ready along Type 'A' Street frontage shall not apply to the Neighborhood Frontage if developed as townhomes. If the Non-Mandatory Neighborhood Frontage is developed on any block or portion of a block designated as such, it shall then become Mandatory for those blocks that face the developed blocks and designated as Neighborhood Frontage on the Regulating Plan.

5.7 Special Destination – The following building and site standards shall apply to areas designated as Special Destination on the Regulating Plan:

5.7.1 Maximum permitted building height shall be 40 feet.

5.7.2 Only "Destination" uses shall be permitted. Destination uses shall include restaurants, cafes, entertainment venues and accessory retail with no drive through service.

5.7.3 Off-street parking shall not be permitted between the building and any Type 'A' Street. However, driveway access to parking may be permitted off Type 'A' or 'B' Streets.

5.7.4 Building pad and surface parking shall be located to maximize the preservation of trees.

5.7.5 Off-street parking may be met through a combination of on-site surface parking and shared parking within the Bush Central Station.

5.7.8 Valet parking circulation drives may be permitted from Type 'A' Streets.

5.7.9 Service areas shall not have frontage along Type 'A' Streets. Service areas shall be located along the back or side of the principal building.

5.7.10 All restaurant uses shall provide outdoor seating/patio.

5.7.11 The area occupied by any impervious surfaces (buildings or parking) shall not exceed the area identified on the regulating plan as Special Destination.

5.7.12 Uses permitted within the Special Destination area are:

1. Accessory Use only - Retail Sales or Service with no drive through facility (includes alcohol sales which shall meet Chapter 4, Alcoholic Beverages of the City of Richardson Code of Ordinances). Excluded from this category are Auto-Retail Sales and Service Uses (see Section 4 of the Code for Definition of Retail, Service uses, and Auto-related Sales and Service).
2. Food Service Uses such as full-service restaurants, cafeterias, bakeries and snack bars with no drive through facilities. Included in this category is café seating within a public or private sidewalk area with no obstruction of pedestrian circulation. Also included in this category is the sale of alcoholic beverages which shall meet Chapter 4, Alcoholic Beverages of the City of Richardson Code of Ordinances.
3. Theater, cinema, dance, music or other entertainment establishment
4. Sales from kiosks
5. Incidental Outdoor Display (subject to standards in Section 7 of the Code)
6. Special Events

5.8 Civic/Open Space – The Regulating Plan indicates Mandatory and Non-Mandatory Civic/Open Spaces. The specific standards for Civic/Open Space are established in Section 11.

Section 6. Schedule of Permitted Uses

6.1 Generally: Due to the emphasis on urban form over land uses in the Station Areas, general use categories have been identified by character zones. Uses not listed in the following schedule, but are substantially similar, may be permitted upon the approval of the City Manager or designee, subject to appeal directly to the City Council.

6.2 Schedule of Uses:

Table 6.1

Character Zone	TOD Core	TOD Mixed Use	Arterial Mixed Use & Highway Mixed Use
Land Use			
Commercial Uses (Office, Retail, Sales & Service Uses)			
Retail Sales or Service with <u>no drive through facility</u> (includes alcohol sales which shall meet Chapter 4, Alcoholic Beverages of the City of Richardson Code of Ordinances). Excluded from this category are Auto-Retail Sales and Service Uses (see Section 4 of the Code for Definition of Retail, Service uses, and Auto-related Sales and Service)	P	P	P
Finance, Insurance, and Real Estate establishments including banks, credit unions, real estate, and property management services, <u>with no drive through facility</u>	P	P	P
Offices for business, professional, and technical uses such as accountants, architects, lawyers, doctors, etc.	P	P	P
Research laboratory headquarters, laboratories and associated facilities	P	P	P
Food Service Uses such as full-service restaurants, cafeterias, bakeries and snack bars with <u>no drive through facilities</u> Included in this category is café seating within a public or private sidewalk area with no obstruction of pedestrian circulation. Also included in this category is the sale of alcoholic beverages which shall meet Chapter 4, Alcoholic Beverages of the City of Richardson Code of Ordinances.	P	P	P
Any use with a drive through facility	P/C	P/C	P/C
Auto-related Sales and Service	NP	NP	P/C
Arts, Entertainment, and Recreation Uses			
Art galleries	P	P	P
Art, antique, furniture or electronics studio (retail, repair or fabrication; excludes auto electronics sales or service)	P	P	P
Games arcade establishments	P	P	P
Theater, cinema, dance, music or other entertainment establishment	P	P	P
Museums and other special purpose recreational institutions	P	P	P
Fitness, recreational sports, gym, or athletic club	P	P	P
Parks, greens, plazas, squares, and playgrounds	P	P	P
Educational, Public Administration, Health Care and Other Institutional Uses			
Business associations and professional membership organizations	P	P	P
Child day care and preschools	P	P	P
Schools, libraries, and community halls	P	P	P
Universities and Colleges	P/C	P/C	P
Hospital	P	P	P
Civic uses	P	P	P
Social and fraternal organizations	P	P	P
Social services and philanthropic organizations	P	P	P
Religious Institutions	P	P	P
Funeral homes	P	P	P
Residential Uses			
Home Occupations	P/A	P/A	P/A
Multi-family residential			
Ground Floor	P/C	P	P/C
Upper Floors	P	P	P
Residential Lofts	P	P	P
Single-family residential attached dwelling unit (Townhomes)	P/C	P/C	P/C
Live-work unit	P	P	P
One- and two-story multi-family residential	NP	NP	NP
Other Uses			
Model homes for sales and promotion**	P	P	P
Hotels	P	P	P
Parking, surface (primary use of property)	NP	NP	P
Parking, surface (accessory use of property)	P	P	P
Parking, structured	P	P	P
Private attached garage	NP	NP	NP
Private detached garage	NP	NP	NP
Sales from kiosks	P	P	NP
Veterinary clinic	NP	P	P

Character Zone	TOD Core	TOD Mixed Use	Arterial Mixed Use & Highway Mixed Use
Community garden	P	P	P
Incidental Outdoor Display (subject to standards in Section 7 of the Code)	P/A	P/A	P/A
Antennas including cell, accessory, and mounted on top of buildings.	P/A/C	P/A/C	P/A/C
Wind energy equipment	P/A	P/A	P/A
Solar energy equipment	P/A	P/A	P/A
Special Event	P	P	P
Rain harvesting equipment	P/A/C	P/A/C	P/A/C
Utility equipment (includes electrical transformers, gas meters, etc)	P/A/C	P/A/C	P/A/C

** Model homes are limited to a time period until all the homes are sold in the neighborhood.

P= Permitted by right
 NP= Not Permitted
 A = Accessory use to not exceed 25% of the primary use building square footage
 P/C= Permitted with design criteria per Table 6.2
 P/A = Permitted Accessory Use
 NA= Not applicable

6.3 Use Criteria: All uses listed as P/C in Table 6.1 shall also meet the following standards in Table 6.2

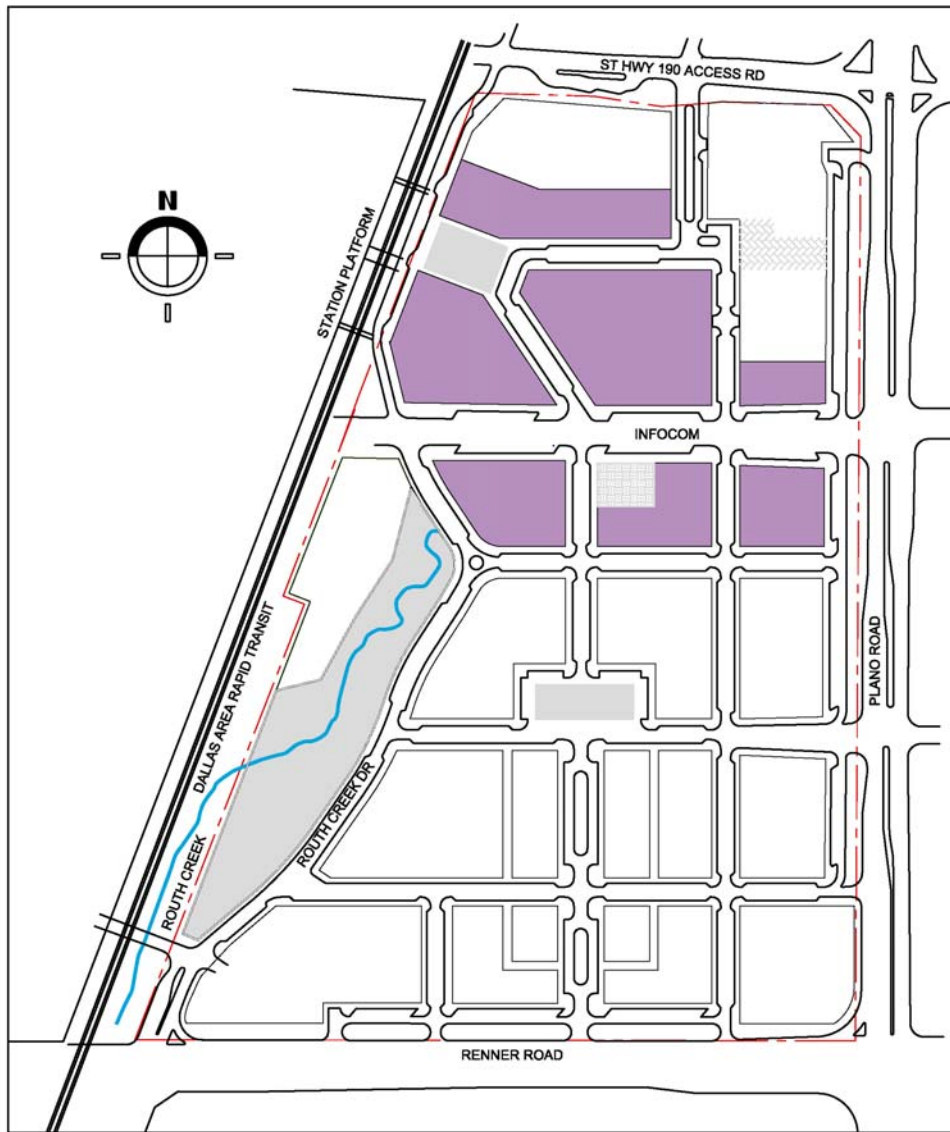
Use	Zone	Location & Design Criteria
Non-Residential Uses		
Any permitted use with a drive through facility	TOD Core, TOD Mixed Use & Arterial Mixed Use	<ul style="list-style-type: none"> All drive through access (driveways) shall be from Type 'B' Streets (including Slip Roads). Drive through lanes and/or canopies shall not have frontage along on or be located along any Type 'A' Streets. Drive through areas screened by a 4' high Street Screen.
Universities and Colleges	TOD Core & TOD Mixed Use	<ul style="list-style-type: none"> Shall be required to provide structured parking as part of the build-out for the university/college campus
Auto-related Sales and Service	Arterial Mixed Use & Highway Mixed Use	<ul style="list-style-type: none"> Gas pumps, canopies, and/or service bays shall not be located along any Type 'A' Street frontage. No more than 50% of a block's frontage along a Type 'B' Street shall be occupied by gas pumps, canopies, and/or service bays. Any buildings associated with the use shall also have a pedestrian entrance at a Type 'A' Street. No outdoor storage of vehicles or other products sold shall be permitted. All auto-related sales display shall be inside storefronts.
Residential Uses		
Multi-family residential Ground Floor	TOD Core and Arterial Mixed Use	<ul style="list-style-type: none"> All ground floors along all Type 'A' Streets and Slip Road frontages shall be built to Retail Ready standards. Ground floors may be occupied by residential uses unless designated as Main Street Frontage. Ground floors of all buildings designated as Main Street Frontage on the Regulating Plan shall not be occupied by residential units and lodging rooms to a minimum depth of 30 feet as measured from the front building line.
Single-family residential attached dwelling unit (Townhomes)	TOD Core, TOD Mixed Use, Arterial Mixed Use	<ul style="list-style-type: none"> Shall be permitted along Type 'B' Streets or along Type 'A' Streets which are also designated for Neighborhood Frontage on the Regulating Plan Frontages along street intersections shall be built to Retail Ready standards for a minimum of 30' along each street or the width of the lot, whichever is less.
Other Uses		
Antennas including cell, accessory and mounted (Excluded from this category are freestanding and commercial antennas and equipment buildings)	All Zones	<ul style="list-style-type: none"> Antennas shall be permitted on rooftops. Antennas shall be screened entirely with a screen of same color as the principal building. Antennas shall not be visible from adjacent Type 'A' Street.
Rain water harvesting equipment	All Zones	<ul style="list-style-type: none"> Rain water harvesting equipment may not be installed along Type 'A' Streets. On all other frontages, they shall be screened with a Street Screen at

Table 6.2 – Use Criteria		
<i>Use</i>	<i>Zone</i>	<i>Location & Design Criteria</i>
		least as high as the equipment being screened.
Utility equipment (includes electrical transformers, gas meters, etc)	All Zones	<ul style="list-style-type: none"> • Utility equipment shall not be installed with frontage on Type ‘A’ Streets. • On all other frontages, they shall be screened with a Street Screen at least as high as the equipment being screened.

Section 7. Building Form and Development Standards

The following section establishes the Building Form and Development Standards for all Character Zones within the Bush Central Station. Diagrams and reference letters are used for illustrations purposes only. Reference letters may not be in continuous sequence.

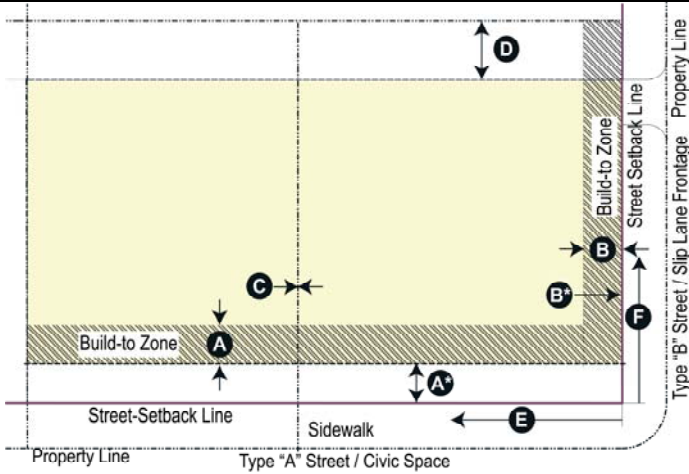
7.1 TOD Core Zone



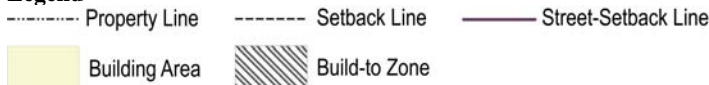
TOD Core Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements

7.1.1 Building Placement



Legend



Street-Setback Line (SSL)

(Distance from center line of street to edge of the BTZ)

TOD Main Street	61 feet
TOD Street Type A	35 feet
TOD Street Type B	31 feet
TOD Avenue	44 feet
Parkway	36 feet
Slip Road Frontage	85 feet (measured from the R.O.W of Plano Road)

Build-To Zone (BTZ)

(Distance from edge of SSL to edge of the BTZ)

Front (Type 'A' Street and Civic Space)	5 – 10 feet	A
Front (Type 'B' Street and Slip Road Frontage)	0 – 10 feet (see #1)	B

Setback

Front (Type 'A' Street and Civic Space – distance from SSL)	5 feet (min.) – 10 feet (max)	A*
Front (Type 'B' Street and Slip Road Frontage – distance from SSL)	0 feet (min.) – 10 feet (max.)	B*
Side (from property line)	0 feet (see #2)	C
Rear (from property line)	5 feet	D

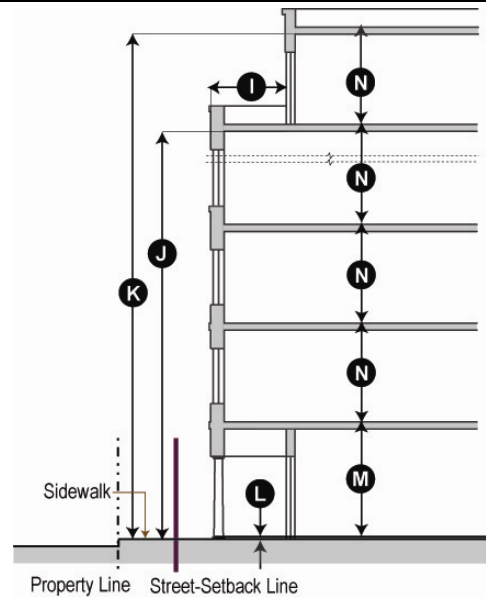
Building Frontage

Building Frontage required along Type 'A' Street/civic space BTZ	90% (min.) (see #3 and #7)	E
Building Frontage required along Type 'B' Street BTZ	0% (min.) (see #3 and #7)	F

7.1.2 Block Standards

Block face dimensions	250 – 400 feet (see #4)
Block perimeter	1400 feet (maximum) (see #4)

7.1.3 Building Height



Principal Building Standards

Building maximum	300 feet (see #8)	K
First floor to floor height	15 feet (min.) (see #5 and #15)	M
Ground floor finish level	12 inches max. above sidewalk (for ground floor Retail Ready buildings)	L
Upper floor(s) height (floor-to-ceiling)	10 feet min. (see #16)	N
Stepback height	Maximum 6 stories then stepback (see #13)	J
Stepback distance	10 feet min. (see #13)	I

7.1.4 Commercial Frontage Requirements

Ground floors of all buildings fronting on Type 'A' Streets and Plano Road shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

7.1.5 Special Frontage Requirements

Requirements Specific to Main Street Frontage

Ground floors of all buildings designated as Main Street Frontage on the Regulating Plan shall not be occupied by residential units and lodging rooms to a minimum depth of 30 feet as measured from the front building line.

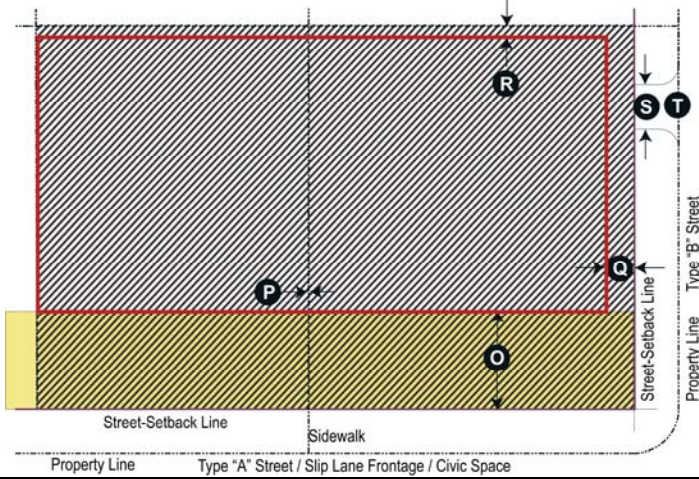
Requirements Specific To Station Frontage

Ground floors of all buildings designated as Station Frontage on the Regulating Plan shall provide shaded areas to a minimum depth of 6 feet. Shaded devices may include arcades, galleries, awnings, canopies, etc.

Notes

- #1 – The area between the building and the edge of the BTZ at the public sidewalk shall be paved flush with the sidewalk.
- #2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #3 – Corner building street facades shall be built to the BTZ for a minimum of 30' from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the build-to-zone standards.

7.1.6 Parking & Service Access



Legend

Property Line	Building Footprint
Surface Parking Area	Street-Setback Line
Above Grade Parking Area	

(i) Parking Location

Surface/At Grade Parking

Type 'A' Street, Slip Road Frontage and Civic Space setback	Shall be located behind the principal building	O
Type 'B' Street setback	Min. of 3 feet behind the building facade line along that street	Q
Side setback (distance from property line)	0 feet min.	P
Rear setback (distance from property line)	0 feet min.	R

Above Grade Parking

Setback along Type 'A' Street, Type 'B' Street, Slip Road Frontage and Civic Space (distance from SSL)	0 feet min.	O
Side and rear setbacks (distance from property line)	0 feet min.	
Upper Floors	May be built up to the building line	

(ii) Required Off-Street Parking Spaces (see #11 and #12)

Non-residential uses	1 space/300 square feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

Parking driveway width	20 feet max. (except when drives may need to be wider to address service access or fire lane standards)	S
Driveways and off-street loading and unloading shall not be located on a Type 'A' Streets. Porte cocheres may be permitted on Type 'A' Streets to provide drop-off and valet service. Shared driveways and cross access easements are encouraged between lots to minimize curb cuts. If driveway and/or off-street service loading and unloading access is provided from a Type 'A' Street, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.		T

7.1.7 Encroachments

Canopies, signs, awnings and balconies may encroach over the sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.1.8 Applicability

Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

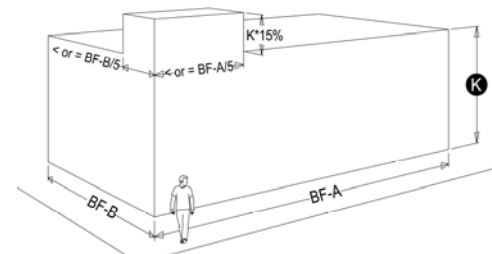
Notes

- #4 – Blocks exceeding the maximum block face and perimeter dimensions may be permitted as follows:
 - (i) No more than two adjacent blocks may be combined based on the Regulating Plan.
 - (ii) An increase in block dimensions shall not eliminate or significantly move a required street or required civic/open space. Required streets and required civic/open spaces may move a maximum of 100 feet.
 - (iii) The block shall maintain a continuous Type 'A' Street frontage with adjoining blocks.
 - (iv) Ground floor internal pedestrian connectivity shall be provided through the block. Connectivity may be provided inside the building, through a parking garage or outside by way of a pedestrian breezeway.

- #5 – First floor heights shall not apply to parking structures.
- #6 – All buildings in the TOD Core Zone shall meet the Building Design Standards in Section 8.

#7 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen that is at least as high as the service equipment being screened. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen composed of shrubs planted to be opaque at maturity. Species shall be selected from the Bush Central Station Planting List in Appendix B of this ordinance. The required Street Screen shall be located within the BTZ along the corresponding frontage.

#8 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.



#9 - Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.

#10 – Setbacks and build-to lines on recessed entries and arcade buildings shall be measured from the building façade line.

#11 – Required parking may be provided anywhere within the Bush Central Station.

#12 – Article III, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas.

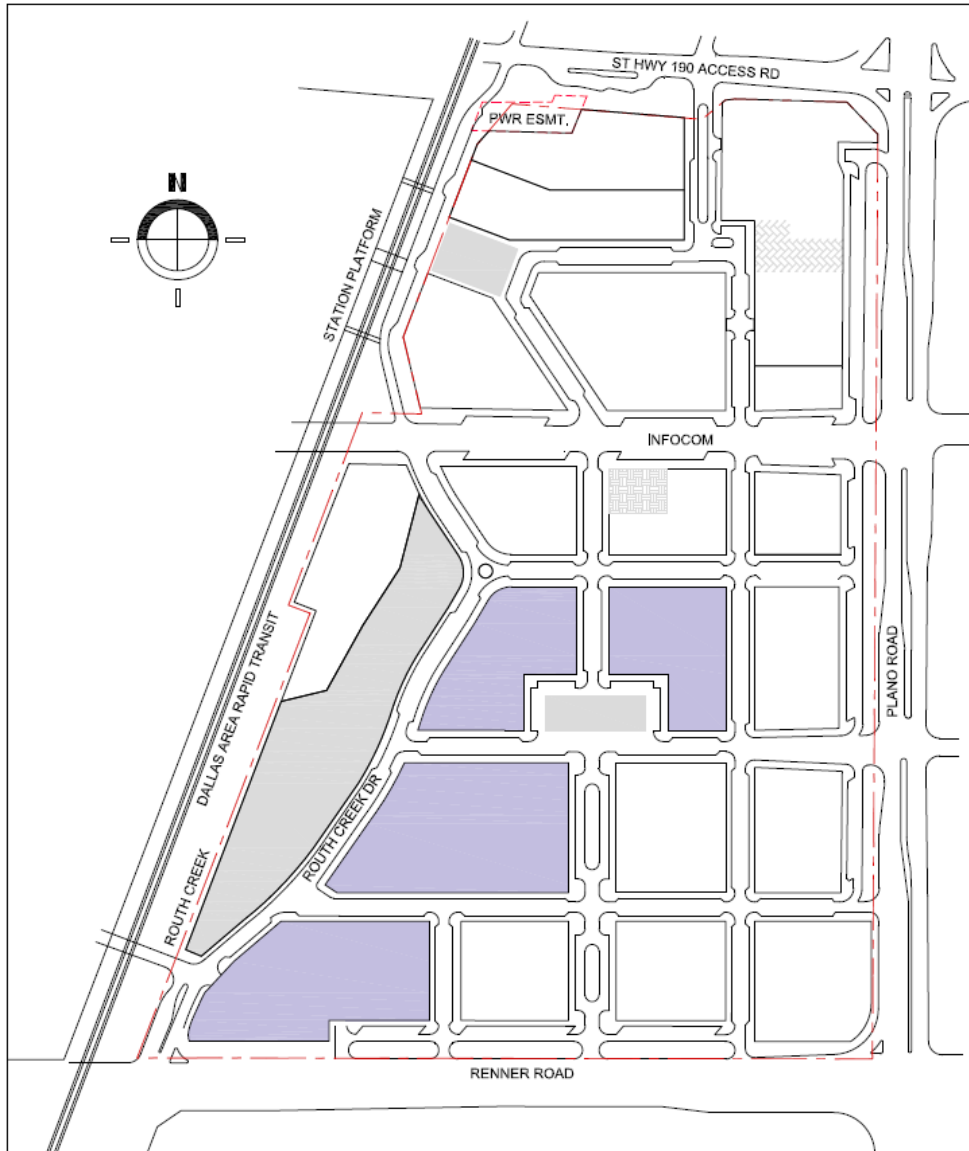
#13 – Stepback requirement shall not apply to any perimeter streets of the BCS-PD including S.H. 190 access road, Plano Road, Renner Road, Routh Creek Parkway, and DART R.O.W frontage.

#14 – Buildings within a minimum of 50% of the net developable land area (i.e., areas not including public street rights-of-way, sidewalks, required setbacks, open space, easements, and civic spaces) north of Infocom Drive, as identified in the Regulating Plan shall be concrete and steel construction.

#15 – Minimum floor-to-ceiling height (to the first obstruction at the ceiling level) for at least 50% of the linear feet of building frontage along Infocom Drive shall be 15'.

#16 – Minimum floor-to-ceiling height (to the first obstruction at the ceiling level) for upper floors shall only apply to a majority of the upper floors (over 50%).

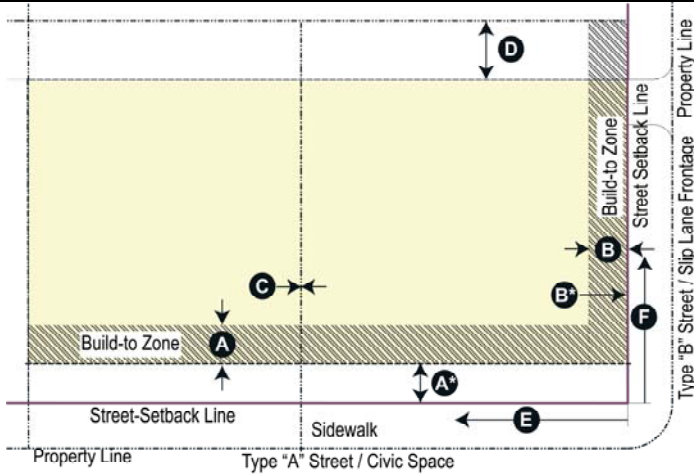
7.2 TOD Mixed Use Zone



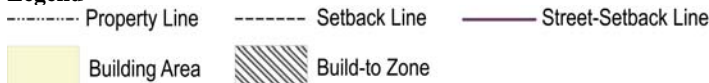
TOD Mixed Use Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements

7.2.1 Building Placement



Legend



**Street-Setback Line (SSL)
(Distance from center line of street cross section to edge of the BTZ)**

TOD Street Type A	35 feet
TOD Street Type B	31 feet
Parkway	36 feet
Court Street	26 feet
Slip Road Frontage	85 feet (measured from the R.O.W of Renner Road)

**Build-To Zone (BTZ)
(Distance from edge of SSL to edge of the BTZ)**

Front (Pedestrian Priority 'A' Street, Pedestrian Priority 'B' and Civic Space)		
Front (Type 'A' Street and Civic Space)	5 – 10 feet	A
Front (Type 'B' Street and Slip Road Frontage)	0 – 10 feet (see #1)	B

Setback

Front (Type 'A' Street and Civic Space – distance from SSL)	5 feet (min.) – 10 feet (max)	A*
Front (Type 'B' Street and Slip Road Frontage – distance from SSL)	0 feet (min.) – 10 feet (max)	B*
Side (distance from property line)	0 feet (see #2)	C
Rear (distance from property line)	0 feet (see #2)	

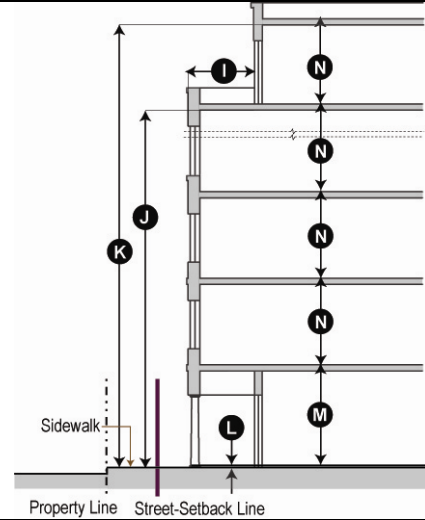
Building Frontage

Building Frontage required along Type 'A' Street/civic space BTZ	80% (min.) (see #3 and #7)	E
Building Frontage required along Type 'B' Street BTZ	0% (min.) (see #3 and #7)	F

7.2.2 Block Standards

Block face dimensions	250 – 400 feet (see #4)
Block perimeter	1500 feet (maximum) (see #4)

7.2.3 Building Height



Principal Building Standards

Building maximum	225 feet (see #8)	K
First floor to floor height	15 feet min. (see #5)	M
Ground floor finish level	12 inches max. above sidewalk (for ground floor Retail Ready buildings)	L
Upper floor(s) height (floor-to-ceiling)	10 feet min. (see #14)	N
Stepback height	Maximum 6 stories then stepback (see #13)	J
Stepback distance	10 feet min (see #13)	I

7.2.4 Commercial Frontage Requirements

Ground floors of all buildings fronting on Type 'A' Streets and Renner Road shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

7.2.5 Special Frontage Requirements

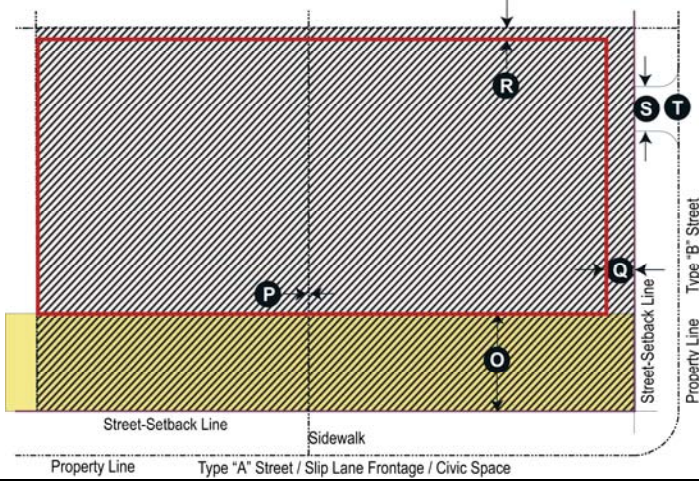
Requirements Specific to Neighborhood Frontage

All buildings designated Neighborhood Frontage on the Regulating Plan shall have a building height maximum of 4 stories to a minimum depth of 30 feet.

Notes

- #1 – The area between the building and the edge of the BTZ at the public sidewalk shall be paved flush with the sidewalk.
- #2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #3 – Corner building street facades shall be built to the BTZ for a minimum of 30 feet from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the build-to zone standards.
- #4 – Blocks exceeding the maximum block face and perimeter dimensions may be permitted as follows:
 - i. No more than two adjacent blocks may be combined based on the Regulating Plan.
 - ii. An increase in block dimensions shall not eliminate or significantly move a required street or required civic/open space. Required streets and required civic/open spaces may move a maximum of 100 feet.
 - iii. The block shall maintain a continuous Type 'A' Street frontage with adjoining blocks.
 - iv. Ground floor internal pedestrian connectivity shall be provided through the block. Connectivity may be provided inside the building, through a parking garage or outside by way of a pedestrian breezeway.

7.2.6 Parking & Service Access



- Legend**
- Property Line
 - Building Footprint
 - Surface Parking Area
 - Street-Setback Line
 - ▨ Above Grade Parking Area

(i) Parking Location

Surface/At Grade Parking

Type 'A' Street, Slip Road Frontage and Civic Space setback	Shall be located behind the principal building	O
Type 'B' Street setback	Min. of 3 feet behind the building facade line along that street	Q
Side setback (distance from property line)	0 feet min.	P
Rear setback (distance from property line)	0 feet min.	R

Above Grade Parking

Setback along Type 'A' Street, Type 'B' Street, Slip Road Frontage and Civic Space (distance from SSL)	0 feet min.	O
Side and rear setbacks (distance from property line)	0 feet min.	
Upper Floors	May be built up to the building line	

(ii) Required Off-Street Parking Spaces (see #10 and #11)

Non-residential uses	1 space/300 square feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

Parking driveway	20 feet max. (except when drives may need to be wider to address service access or fire lane standards))	S
Driveways and off-street loading and unloading	shall not be located on a Type 'A' Streets. Porte cocheres may be permitted on Type 'A' Streets to provide drop-off and valet service. Shared driveways and cross access easements are encouraged between lots to minimize curb cuts. If driveway and/or off-street service loading and unloading access is provided from a Type 'A' Street, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.	T

7.2.7 Encroachments

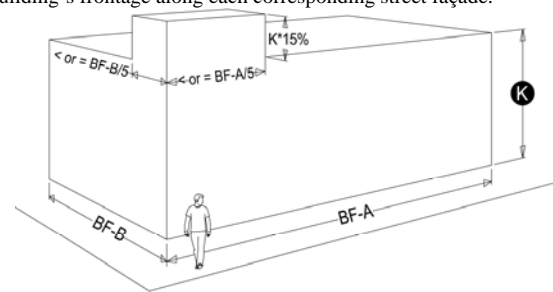
Canopies, signs, awnings and balconies may encroach over the sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.2.8 Applicability

Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

Notes

- #5 – First floor heights shall not apply to parking structures.
- #6 – All buildings in the TOD Mixed Use Zone shall meet the Building Design Standards in Section 8.
- #7 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen that is at least as high as the service equipment being screened. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen composed of shrubs planted to be opaque at maturity. Species shall be selected from the Bush Central Station Planting List in Appendix B of this ordinance. The required Street Screen shall be located within the BTZ along the corresponding frontage.
- #8 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.



#9 - Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.

#10 – Setbacks and build-to lines on recessed entries and arcade buildings shall be measured from the building façade line.

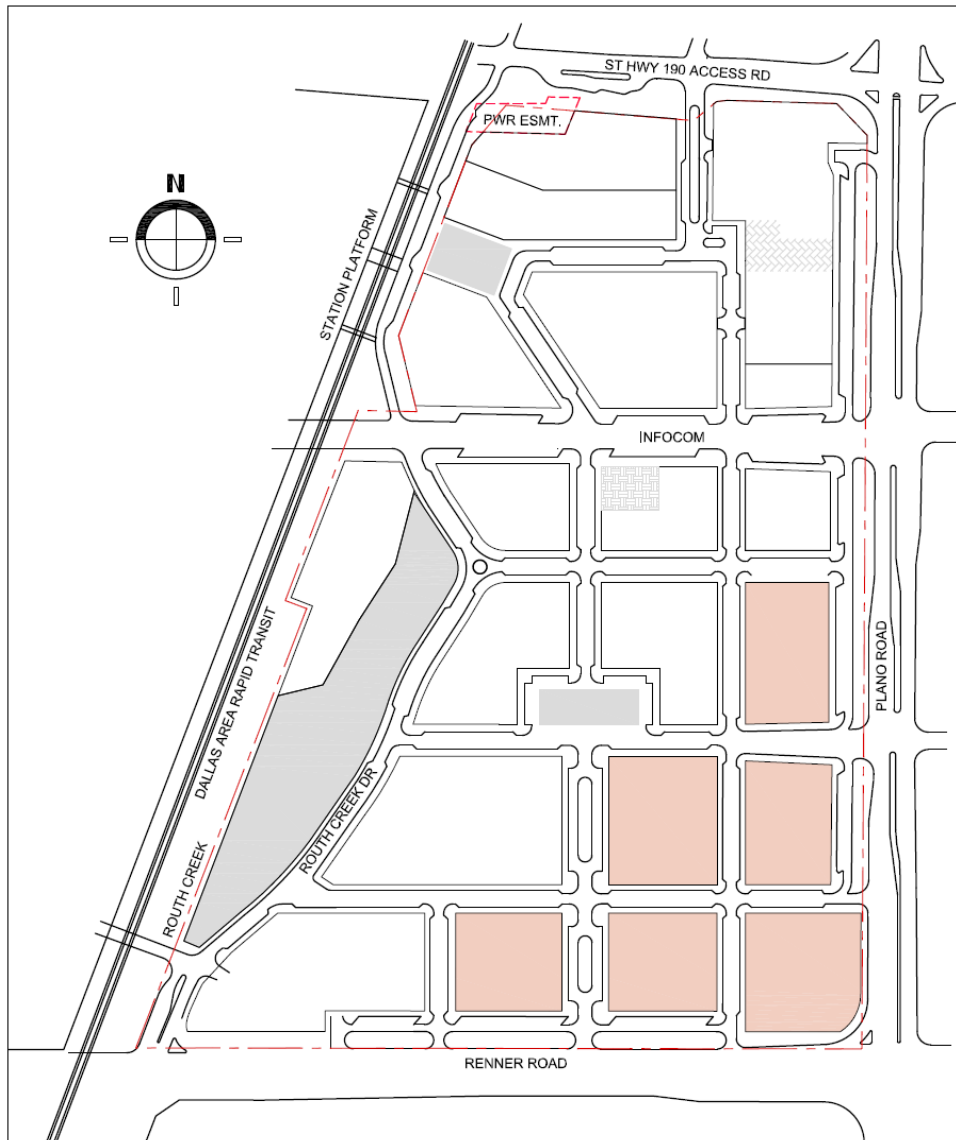
#11 – Required parking may be provided anywhere within the Bush Central Station.

#12 – Article III, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas.

#13 – Stepback requirement shall not apply to any perimeter streets of the BCS-PD including S.H. 190 access road, Plano Road, Renner Road, Routh Creek Parkway, and DART R.O.W frontage.

#14 - Minimum floor-to-ceiling height (to the first obstruction at the ceiling level) for upper floors shall only apply to a majority of the upper floors (over 50%).

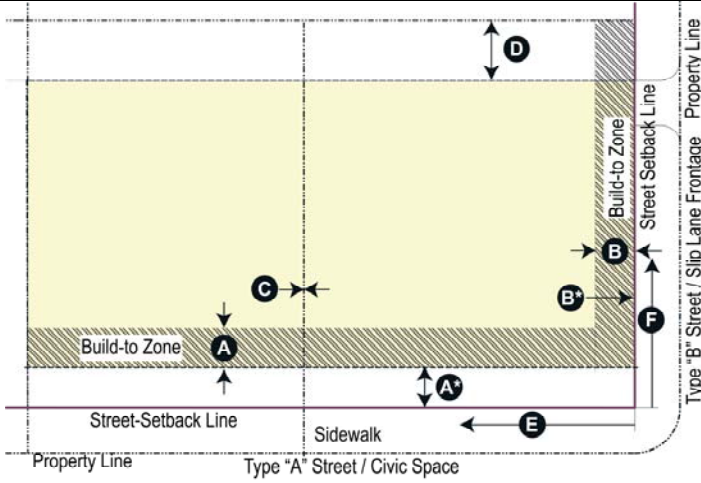
7.3 Arterial Mixed Use Zone



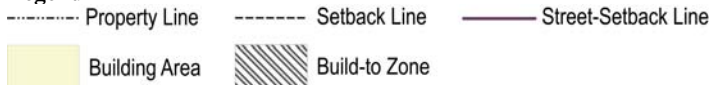
Arterial Mixed Use Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements

7.3.1 Building Placement



Legend



Street-Setback Line (SSL)

(Distance from center line of street cross section to edge of the BTZ)

TOD Street Type A	35 feet
TOD Street Type B	31 feet
TOD Main Street	61 feet
Park Avenue	51 feet
Slip Road Frontage (Plano Road)	85 feet (measured from the R.O.W of Plano Road)
Slip Road Frontage (Renner Road)	85 feet (measured from the R.O.W of Renner Road)

Build-To Zone (BTZ)

(Distance from SSL to edge of the BTZ)

Front (Type 'A' Street and Civic Space)	5 – 10 feet	A
Front (Type 'B' Street and Slip Road Frontage)	0 – 10 feet (see #1)	B

Setback

Front (Type 'A' Street and Civic Space – distance from SSL)	5 feet (min.) – 10 feet (max)	A*
Front (Type 'B' Street and Slip Road Frontage – distance from SSL)	0 feet (min.) – 10 feet (max)	B*
Side (distance from property line)	0 feet (see #2)	C
Rear (distance from property line)	0 feet	D

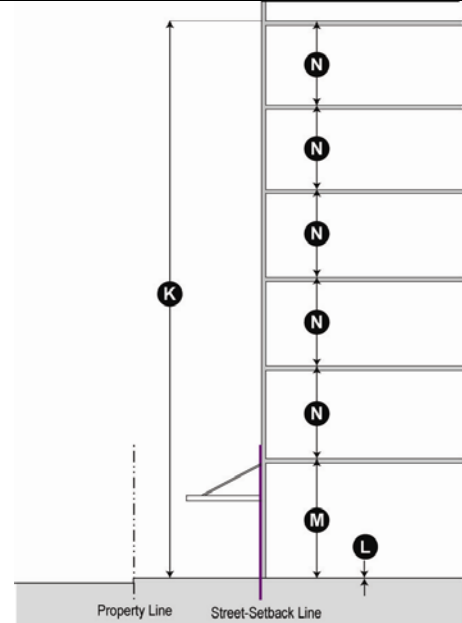
Building Frontage

Building Frontage required along Type 'A' Street/civic space BTZ	50% (min.) (see #3 and #7)	E
Building Frontage required along Type 'B' Street BTZ	0% (min.) (see #3 and #7)	F

7.3.2 Block Standards

Block face dimensions	250 – 400 feet (maximum) (see #4)
Block perimeter	1100 feet (maximum) (see #4)

7.3.3 Building Height



Principal Building Standards

Building maximum	100 feet (see #8)	K
First floor to floor height	15 feet (min.) (see #5)	M
Ground floor finish level	12 inches max. above sidewalk (for ground floor Retail Ready buildings)	L
Upper floor(s) height (floor-to-ceiling)	10 feet min. (see #13)	N

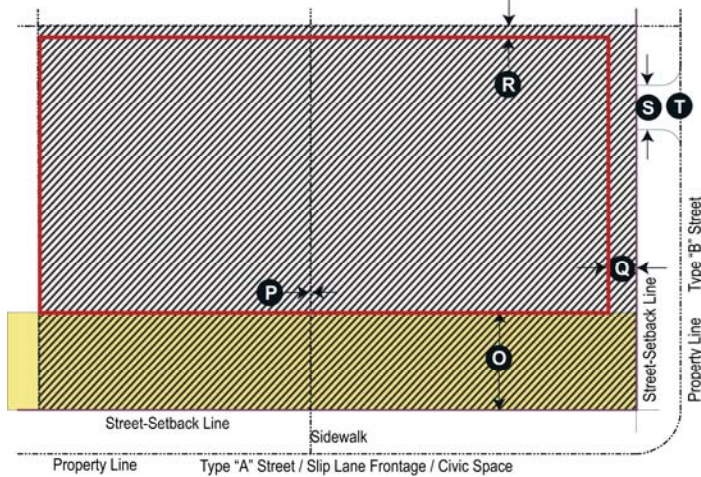
7.3.4 Commercial Frontage Requirements

Ground floors of all buildings fronting on Type 'A' Streets, Plano Road and Renner Road shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

Notes

- #1 – The area between the building and the edge of the BTZ at the public sidewalk shall be paved flush with the sidewalk.
- #2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #3 – Corner building street facades shall be built to the BTZ for a minimum of 15' from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the build-to zone standards.
- #4 – Blocks exceeding the maximum block face and perimeter dimensions may be permitted as follows:
 - i. Combined blocks shall be adjacent to one another based on the Regulating Plan.
 - ii. An increase in block dimensions shall not eliminate or significantly move a required street or required civic/open space. Required streets and required civic/open spaces may move a maximum of 100 feet.
 - iii. The block shall maintain a continuous Type 'A' Street frontage with adjoining blocks.
 - iv. Ground floor internal pedestrian connectivity shall be provided through the block. Connectivity may be provided inside the building, through a parking garage or outside by way of a pedestrian breezeway.

7.3.5 Parking & Service Access



Legend

Property Line	Building Footprint
Surface Parking Area	Street-Setback Line
Above Grade Parking Area	

(i) Parking Location

Surface/At Grade Parking

Type 'A' Street, Slip Road Frontage and Civic Space setback	Shall be located behind the principal building	O
Type 'B' Street setback	Min. of 3 feet behind the building facade line along that street	Q
Side setback (distance from property line)	0 feet min.	P
Rear setback (distance from property line)	0 feet min.	R

Above and Below Grade Parking (Distance from SSL)

Setback along Type 'A' Street, Type 'B' Street, Slip Road Frontage and Civic Space (distance from SSL)	0 feet min.	O
Side and rear setbacks (distance from property line)	0 feet min.	
Upper Floors	May be built up to the building line	

(ii) Required Off-Street Parking Spaces (see #11 and #12)

Non-residential uses	1 space/300 square feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

Parking driveway width	City standards on Arterial Roadways and 24 feet max. on all Bush Central Station Streets (except when drives may need to be wider to address service access or fire lane standards)	S
Driveways and off-street loading and unloading shall not be located on a Type 'A' Streets.		T
Porte cocheres may be permitted on Type 'A' Streets to provide drop-off and valet service.		
Shared driveways and cross access easements are encouraged between lots to minimize curb cuts.		
If driveway and/or off-street service loading and unloading access is provided from a Type 'A' Street, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.		

7.3.6 Encroachments

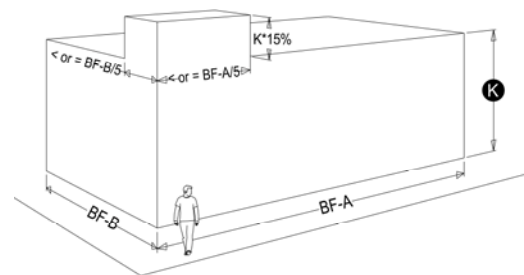
Canopies, signs, awnings and balconies may encroach over the sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.3.7 Applicability

Building Form and Development Standards in this Section shall apply to all development in this Character Zone.

Notes

- #5 – First floor heights shall not apply to parking structures.
- #6 – All buildings in the Arterial Mixed Use Zone shall meet the Building Design Standards in Section 8.
- #7 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen that is at least as high as the service equipment being screened. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen composed of shrubs planted to be opaque at maturity. Species shall be selected from the Bush Central Station Planting List in Appendix B of this ordinance. The required Street Screen shall be located within the BTZ along the corresponding frontage.
- #8 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.



#9 - Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.

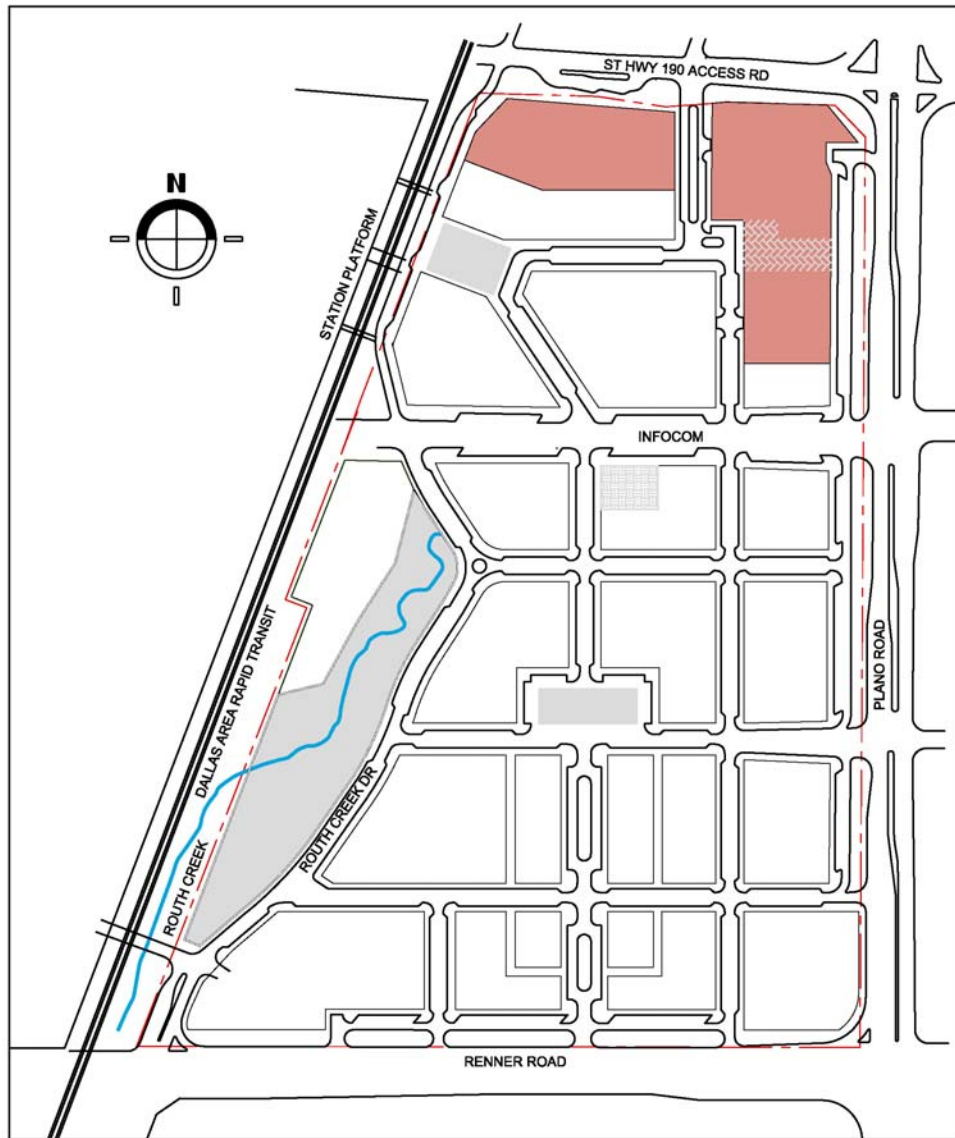
#10 – Setbacks and build-to lines on recessed entries and arcade buildings shall be measured from the building façade line.

#11 – Required parking may be provided anywhere within the Bush Central Station.

#12 – Article III, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas.

#13 - Minimum floor-to-ceiling height (to the first obstruction at the ceiling level) for upper floors shall only apply to a majority of the upper floors (over 50%).

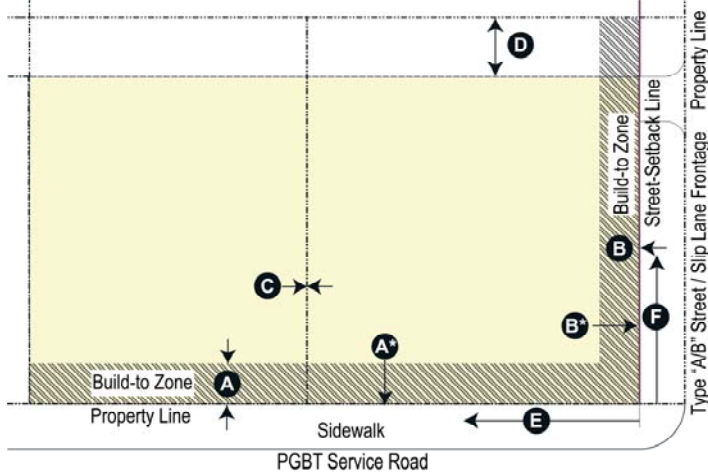
7.4 Highway Mixed Use



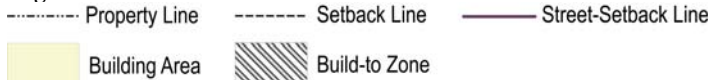
Highway Mixed Use Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements

7.4.1 Building Placement



Legend



Street-Setback Zone (SSL)
(Distance from center line of street cross section to edge of the BTZ)

TOD Avenue	44 feet
TOD Street Type B	31 feet
Slip Road Frontage	85 feet (measured from the R.O.W of Plano Road)

Build-To Zone (BTZ)
(Distance from Property Line to edge of the BTZ)

Front (PGBT Service Road)	None	A
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(Distance from SSL to edge of the BTZ)

Front (Type "A/B" Street and Slip Road Frontage)	0 – 10 feet (see #1)	B
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Setback

Front (PGBT Service Road – distance from property line or edge of utility easement)	15 feet (min.)	A*
Front (Type "A/B" Street and Slip Road Frontage – distance from SSL)	0 feet (min) – 10 feet (max)	B*
Side (distance from property line)	0 feet (see #2)	C
Rear (distance from property line)	0 feet	D

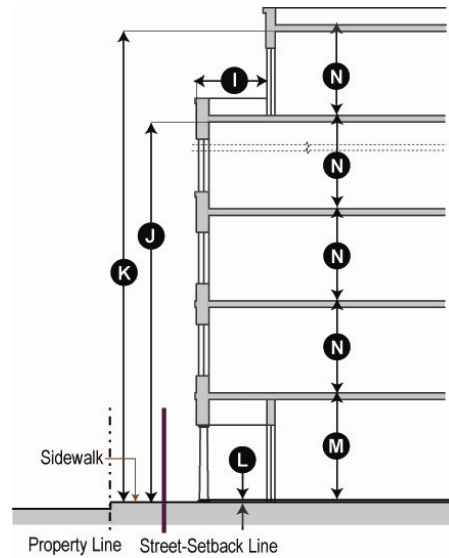
Building Frontage

Building Frontage required along Type 'A' Street BTZ	90% (min.) (see #3 and #7)	
Building Frontage required along PGBT Service Road	0% (min.) (see #3 and #7)	E
Building Frontage required along Type 'B' Street BTZ	0% (min.) (see #3 and #7)	F

7.4.2 Block Standards

Block face dimensions	250 – 400 feet (maximum) (see #4)
Block perimeter	1400 feet (maximum) (see #4)

7.4.3 Building Height



Principal Building Standards

Building maximum	300 feet (see #8)	K
Building minimum	50 feet (along PGBT Service Road frontage only)	
First floor to floor height	15 feet min. (see #5)	M
Ground floor finish level	12 inches max. above sidewalk (for ground floor Retail Ready buildings)	L
Upper floor(s) height (floor-to-ceiling)	10 feet min. (see #15)	N
Stepback height	Maximum 10 stories then stepback (see #13)	J
Stepback distance	10 feet min.	I

7.4.4 Commercial Frontage Requirements

Ground floors of all buildings fronting on President George Bush Turnpike (PGBT) service road and Plano Road shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

7.4.5 Special Frontage Requirements

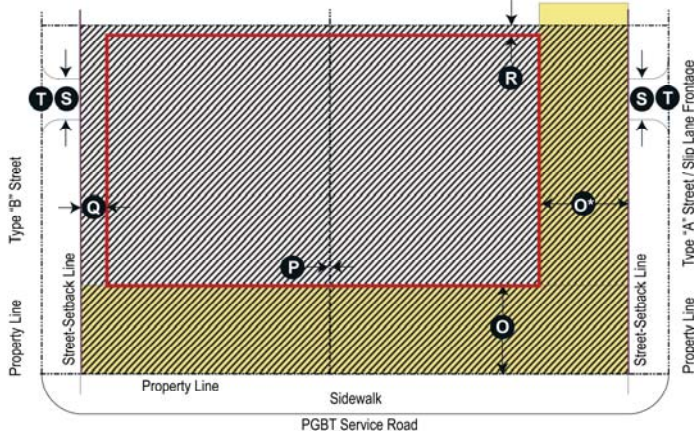
Requirements Specific To Station Frontage

Ground floors of all buildings designated as Station Frontage on the Regulating Plan shall provide shaded areas to a minimum depth of 6 feet. Shaded devices may include arcades, galleries, awnings, canopies, etc.

Notes

- #1 – The area between the building and the edge of the BTZ at the public sidewalk shall be paved flush with the sidewalk.
- #2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #3 – Corner building street facades shall be built to the BTZ for a minimum of 15 feet from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the build-to zone standards.

7.4.6 Parking & Service Access



(i) Parking Location

Surface/At Grade Parking

PGBT Service Road setback	Shall be located behind the principal building	O
Type 'A' Street and Slip Road Frontage	Shall be located behind the principal building	O*
Type 'B' Street setback	Min. of 3 feet behind the building façade line along that street	Q
Side setback (distance from property line)	0 feet min.	P
Rear setback (distance from property line)	0 feet min.	R

Above Grade Parking

Setback along Type 'A' Street, Type 'B' Street, Slip Road Frontage and Civic Space (distance from SSL)	0 feet min.	O
PGBT Service Road, side and rear setbacks (distance from property line)	0 feet min.	

Upper Floors May be built up to the building line

(ii) Required Off-Street Parking Spaces (see #11 and #12)

Non-residential uses	1 space/300 square feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

Parking driveway width	TXDOT standards on service road and 24 feet max on all Bush Central Station Streets (except when drives may need to be wider to address service access or fire lane standards)	S
Driveways and off-street loading and unloading shall not be located on PGBT Service Road. Porte cocheres may be permitted on Type 'A' Streets to provide drop-off and valet service. Shared driveways and cross access easements are encouraged between lots to minimize curb cuts. If driveway and/or off-street service loading and unloading access is provided from PGBT Service Road, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.		T

7.4.7 Encroachments

Canopies, signs, awnings and balconies may encroach over the sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.4.8 Applicability

Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

Notes

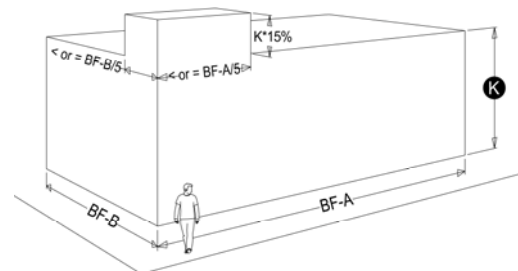
- #4 – Blocks exceeding the maximum block face and perimeter dimensions may be permitted as follows:
 - i. No more than two adjacent blocks may be combined based on the Regulating Plan.
 - ii. An increase in block dimensions shall not eliminate or significantly move a required street or required civic/open space. Required streets and required civic/open spaces may move a maximum of 100 feet.
 - iii. The block shall maintain a continuous Type 'A' Street frontage with adjoining blocks.
 - iv. Ground floor internal pedestrian connectivity shall be provided through the block. Connectivity may be provided inside the building, through a parking garage or outside by way of a pedestrian breezeway.

#5 – First floor heights shall not apply to parking structures.

#6 – All buildings in the Highway Mixed Use shall meet the Building Design Standards in Section 8.

#7 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen that is at least as high as the service equipment being screened. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen composed of shrubs planted to be opaque at maturity. Species shall be selected from the Bush Central Station Planting List in Appendix B of this ordinance. The required Street Screen shall be located within the BTZ along the corresponding frontage.

#8 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.



#9 – Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.

#10 – Setbacks and build-to lines on recessed entries and arcade buildings shall be measured from the building façade line.

#11 – Required parking may be provided anywhere within the Bush Central Station.

#12 – Article III, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas.

#13 – Stepback requirement shall not apply to any perimeter streets of the BCS-PD including S.H. 190 access road, Plano Road, Renner Road, Routh Creek Parkway, and DART R.O.W frontage.

#14 - Buildings within a minimum of 50% of the net developable land area (i.e., areas not including public street rights-of-way, sidewalks, required setbacks, open space, easements, and civic spaces) north of Infocom Drive, as identified in the Regulating Plan shall be concrete and steel construction.

#15 - Minimum floor-to-ceiling height (to the first obstruction at the ceiling level) for upper floors shall only apply to a majority of the upper floors (over 50%).

Section 8. Building Design Standards

The Building Design Standards and Guidelines for the Bush Central Station shall establish a coherent urban character and encourage enduring and attractive development. Development plans shall be reviewed by the City Manager or designee for compliance with the standards below.

The key design principles establish essential goals for development in the Bush Central Station to ensure the preservation, sustainability, and visual quality of this unique environment. Buildings shall be located and designed so that they provide visual interest and create enjoyable, human-scaled spaces. The key design principles are:

- a. New buildings shall utilize building elements and details to achieve a pedestrian-oriented public realm.
- b. Compatibility is not meant to be achieved through uniformity, but through the use of variations in building elements to achieve individual building identity.
- c. Building facades shall include appropriate architectural details and ornament to create variety and interest.
- d. Open space(s) shall be incorporated to provide usable public areas integral to the urban environment.

8.1 General to all Character Zones

8.1.1 Building Orientation

- i. Buildings shall be oriented towards Type 'A' Streets, where the lot has frontage along Type 'A' Streets. All other buildings shall be oriented towards Type 'B' Streets or Civic Spaces.
- ii. Primary entrance to buildings shall be located on the street along which the building is oriented. At intersections, corner buildings may have their primary entrances oriented at an angle to the intersection.
- iii. All primary entrances shall be oriented to the public sidewalk for ease of pedestrian access. Secondary and service entrances may be located from internal parking areas or alleys.

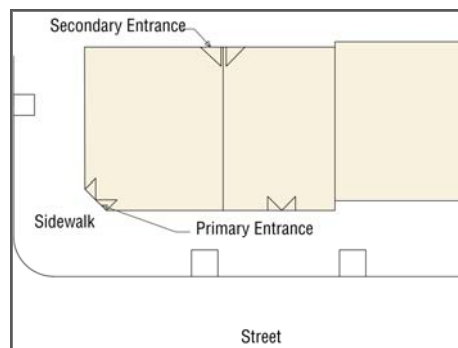


Figure showing required building orientation and location of primary entrances

8.1.2 Design of Parking Structures

- i. All frontages of parking structures located on Type 'A' Streets shall not have parking uses on the ground floor to a minimum depth of 30 feet along the Type 'A' Street frontage. If the frontage is along a designated Main Street Frontage, then the Main Street Frontage requirement shall supersede.
- ii. The amount of Type 'A' Street frontage devoted to a parking structure shall be minimized by placing the shortest dimension(s) along the Type 'A' Street edge(s).

- iii. Parking structure facades on all Type ‘A’ Streets shall be designed with both vertical (façade rhythm of 20 feet to 30 feet) and horizontal (aligning with horizontal elements along the block) articulation.
- iv. Where above ground structured parking is located at the perimeter of a building with frontage along a Type ‘A’ Street; it shall be screened in such a way that cars on all parking levels are completely hidden from view from all adjacent public streets. Parking garage ramps shall not be visible from any public street. Ideally, ramps should not be located along the perimeter of the parking structure. Architectural screens shall be used to articulate the façade, hide parked vehicles, and shield lighting.
- v. When parking structures are located at corners, corner architectural elements shall be incorporated such as corner entrance, signage and glazing.
- vi. Parking structures and adjacent sidewalks shall be designed so pedestrians are clearly visible to entering and exiting automobiles.



Images showing appropriate design of Parking Structures

8.1.3 Design of Automobile Related Building Site Elements

- i. Drive-through lanes for commercial uses shall not be located along any Type ‘A’ Street. Drive-through lanes shall be hidden behind a Street Screen along the Type ‘B’ Street frontage.
- ii. All off-street loading, unloading, and trash pick-up areas shall be located along alleys or Type ‘B’ Streets only unless permitted in the specific building form and development standards in Section 7. Any off-street loading, unloading, or trash pick-up areas shall be screened using a Street Screen that is at least as tall as the trash containers and/or service equipment it is screening at the BTZ. The Street Screen shall be made up of (i) the same material as the principal building or (ii) a living screen or (iii) a combination of the two.

8.1.4 Roof Form

- i. Buildings shall have simple, flat fronts with minimal articulations with flat or low pitched roofs with parapets. Corner hip roof elements and gable accents at the parapet may be permitted. Projecting mansard roofs shall not be permitted.



8.1.5 Façade Composition

- i. Buildings shall maintain a façade rhythm of 20 feet to 30 feet along all Type ‘A’ Streets.

- ii. This rhythm may be expressed by changing materials, or color, or by using design elements such as fenestration, columns and pilasters, or by varying the setback of portions of the façade.
- iii. Buildings shall be designed and built in tri-partite architecture so that they have a distinct Base, Middle and Cap.



Image of Tri-Partite Architecture

- iv. An expression line or equivalent architectural element shall delineate the Base and Cap of all buildings. A Cornice shall delineate the caps of facades that do not utilize a pitched roof.
- v. For retail storefront buildings, a transom, display window area, and bulkhead at the base shall be utilized.

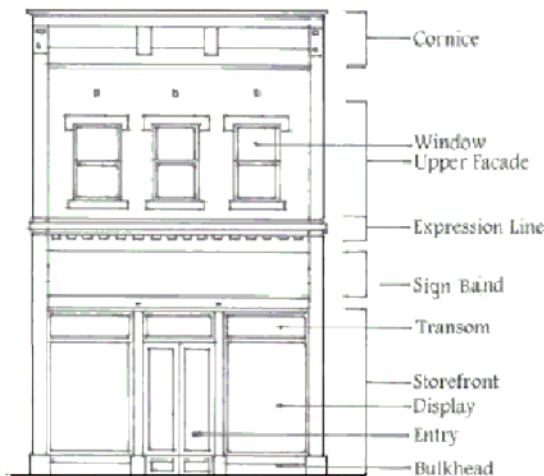


Image of a typical storefront

- vi. Storefronts on facades that span multiple tenants shall use architecturally compatible materials, colors, details, awnings, signage, and lighting fixtures.
- vii. Building entrances may be defined and articulated by architectural elements such as lintels, pediments, pilasters, columns, porticos, porches, overhangs, railings, balustrades, and others as appropriate. All building elements should be compatible with the architectural style, materials, colors, and details of the building as a whole. Entrances to upper level uses may be defined and integrated into the design of the overall building facade.
- viii. Buildings shall generally maintain the alignment of horizontal elements along the block.

- ix. Corner emphasizing architectural features, pedimented gabled parapets, cornices, awnings, blade signs, arcades, colonnades and balconies may be used along commercial storefronts to add pedestrian interest.



Buildings with architectural features and storefront elements that add interest along the street.

- x. Buildings which are located on axis with a terminating street or at the intersection of streets shall be considered as feature buildings. Such buildings shall be designed with features which take advantage of that location, such as an accentuated entry and a unique building articulation which is off-set from the front wall planes and goes above the main building eave or parapet line.

8.1.6 Windows and Doors

- i. Windows and doors on street (except alleys) fronting facades shall be designed to be proportional and appropriate to the specific architectural style of the building. First floor windows shall NOT be opaque, tinted or mirrored glass.
- ii. All ground floor front facades of buildings along Type 'A' Streets or Civic/Open Space shall have transparent storefront windows covering no less than 50% of the façade area. Each upper floor of the same building facades facing a Type 'A' Street or Civic/Open Space shall contain transparent windows covering at least 30% of the façade area. All other street facing facades (except alleys) shall have transparent windows covering at least 15% of the façade area for all floors.



Images showing appropriate storefront display and transparency

8.1.7 Commercial and Mixed Use Building Materials

- i. At least 85% of each building's façade (excluding doors and windows) along all Type 'A' Streets, Plano and Renner Roads shall be finished in one of the following materials:
- Masonry (brick, stone, stucco utilizing a three-step process, cast stone, rock, marble, granite, curtain glass, or glass block)

- ii. No more than 15% of each façade along all Type ‘A’ Streets, Plano, and Renner Roads shall use accent materials such as wood, architect metal panel, split-face concrete block, tile, or pre-cast concrete panels.



Images showing appropriate building materials within TOD Core, TOD Mixed Use, Arterial and Highway Mixed Use.

- iii. A minimum of 80% of all other building facades with the exception of Type ‘A’ Street facades shall be finished in one of the following materials:
 - Masonry (brick, stone, stucco utilizing a three-step process, cast stone, rock, marble, granite, curtain glass, or glass block)
- iv. No more than 20% of all other building facades with the exception of Type ‘A’ Street facades shall use accent materials such as wood, architect metal panel, split-face concrete block, tile, or pre-cast concrete panels.
- v. EIFS may only be used 8 feet above the ground floor and is prohibited on all building elevations with the exception of its use for exterior trim and molding features.
- vi. Cementitious-fiber clapboard (not sheet) with at least a 50-year warranty may only be used on the upper floors only of any commercial frontage on any street or alley façade and shall be limited to no more than 15% along Type ‘A’ Streets and 20% along Type ‘B’ Streets.
- vii. Roofing materials visible from any public right-of-way shall be copper, factory finished standing seam metal, slate, synthetic slate, or similar materials.

8.2 Standards Specific to Development in the TOD Core and Highway Mixed Use Zones

8.2.1 Buildings within a minimum of 50% of the net developable land area (i.e., areas not including public street rights-of-way, sidewalks, required setbacks, open space, easements, and civic spaces) north of Infocom Drive, as identified in the Regulating Plan shall be concrete and steel construction.

8.3 Standards Specific to Development with Neighborhood Frontage Designation:

8.3.1 Building Orientation

- i. Garages and carport for Residential Buildings shall be located on alleys at the rear of residential buildings.

8.3.2 Building Massing and Scale

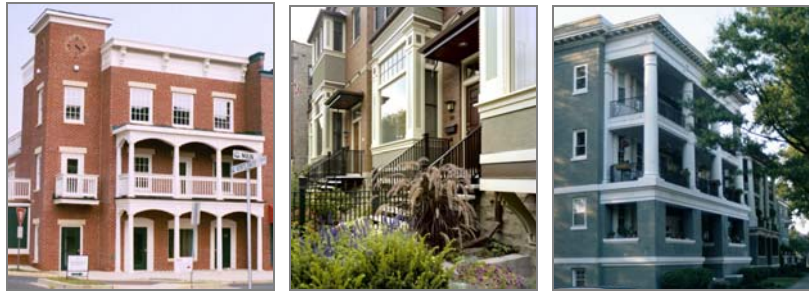
- i. Residential buildings shall have few, if any, articulations and simple roofs (gable, hip, combination) with most building wing articulations set at the rear of the structure. Window projections, bay windows, stoops, porches, balconies, and similar extensions shall be exempt from this standard.
- ii. Gable roofs, if provided for residential buildings, shall have a minimum pitch of 5/12. When hipped roofs are used, the minimum pitch shall be 5/12. Other roof types shall be appropriate to the architectural style of the building. Porch roofs may be a minimum pitch of 3/12.
- iii. Projecting mansard roofs shall be prohibited.



Images showing appropriate massing and scale for Residential Buildings

8.3.3 Façade Composition

- i. Buildings shall maintain a façade rhythm of 20 feet to 30 feet along Type ‘A’ Streets. This rhythm may be expressed by changing materials, or color, or by using design elements such as columns and pilasters, or by varying the setback of portions of the building façade.
- ii. At least one of the following -- porches, stoops, eaves or balconies -- shall be added along the front residential facades to add pedestrian interest along the street.
- iii. For residential buildings the grade of the slab or first floor elevation shall be elevated at least 18 inches above the grade of the sidewalk.
- iv. Alley and/or Type ‘B’ Street facing facades shall be of finished quality and of the same color that blend with the front of the building.



Residential buildings with porches, balconies, and stoops to add interest along the street.

8.3.4 Windows and Doors

- i. Windows and doors shall be designed to be proportional and appropriate to the architectural style of the building.
- ii. Windows may have jack arch, keystone arch, flat arch, or ornamental arches.
- iii. All building facades of residential buildings fronting on all streets or civic / open spaces, except alleys, shall have transparent windows covering at least 15% of each façade.



Images showing appropriate window designs and proportions.

8.3.5 Residential Building Materials

- i. At least 85% of the Type 'A' Street facing facades of all buildings (excluding doors and windows) shall be finished in one or more of the following materials. No more than three different materials shall be used on any single residential façade:
 - Cementitious-fiber clapboard (not sheet) with at least a 50-year warranty.
 - Masonry (brick; stone; man-made stone, or stucco utilizing a three-step process).
- ii. The following may only be allowed up to 15% as an accent material:
 - Architectural metal panels or similar material over a cementitious base, rock, glass block and tile.
- iii. EIFS shall not be permitted along any Type 'A' Street facades.
- iv. Side and rear facades shall be of finished quality and of the same color that blend with the front of the building. Ground floor side and rear facades shall not be EIFS. On upper floors of side and rear facades, EIFS is limited to no more than 10% of the façade area.
- v. Roofing materials (visible from any public right-of-way): copper, factory finished painted metal, slate, synthetic slate, terra cotta, and asphalt shingles (laminated and classified as being at least 300 pounds/100 sf).
- vi. An enclosed garage or carport shall be designed and constructed of the same material as the primary building.

Section 9. Street & Streetscape Design Standards

9.1 Generally: Streets in the Bush Central Station need to support the overall goal of a mixed use, compact, pedestrian oriented district. They should balance all forms of mobility while maximizing convenience for residents and visitors.

The Regulating Plan designates the required and recommended street network within the Bush Central Station. This section specifies the typical configuration of streets within the Bush Central Station. The specifications address vehicular lane width, parkway widths, R.O.W widths, number of travel lanes, on-street parking, and pedestrian accommodation. The character of streets in the Bush Central Station will vary based on the location. The service road of President George Bush Turnpike (State Highway 190) is under the purview of TxDOT while the remaining streets are city streets.

9.2 New Streets: This section specifies standards for all new streets in the Bush Central Station. New streets shall be based on the Mandatory or Non-Mandatory designation on the Regulating Plan.

9.3 Street Classification Established: Table 9.1 and associated cross sections shall establish the cross sections for each street type. The cross sections may be adjusted to fit existing contexts with the approval of the City Engineer. In addition, the proposed cross sections may be adjusted to meet the needs of the Uniform Fire Code as adopted by the City.

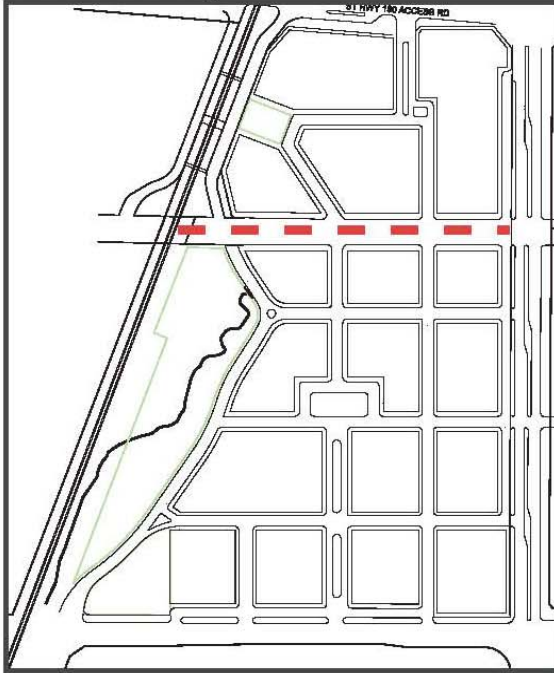
Table 9.1

Elements Street Classification	Street Width (Recommended minimum)		Number of Vehicular Lanes	Number of Bike Lanes	Vehicular Lane Widths	Bike Lane Width	On-Street Parking	Pedestrian Sidewalk Width (min.)	Parkway/ Tree Well
	R.O.W	Pedestrian Easement (on both sides)							
TOD Main Street Type 'A' Street	90 feet	16 feet	4	2	10.5 feet	5 feet	Yes, both sides, reverse angled	16 feet	Tree Well, 5 X 5 feet min.
Parkway Type 'A' Street	40 feet	16-18 feet	2	N/A	12 feet	N/A	Yes, both sides, parallel	16-18 feet	Tree Well, 5X5 feet
TOD Avenue Type 'B' Street	64 feet	12 feet	2	N/A	14 feet	N/A	Yes, both sides, parallel	12 feet	Tree Well, 5 X 5 feet min.
Park Avenue Type 'A' Street	74 feet	14 feet	2	N/A	14 feet	N/A	Yes, both sides, parallel	14 feet	Tree Well, 5 x 5 feet min. or Parkway, 6 feet min.
TOD Street Type 'A'	38 feet	16 feet	2	N/A	11 feet	N/A	Yes, both sides, parallel	16 feet	Tree Well, 5 X 5 feet min.
TOD Street Type 'B'	38 feet	12 feet	2	N/A	11 feet	N/A	Yes, both sides, parallel	12 feet	Tree Well, 5 X 5 feet min.
Court Street Type 'A' Street	20 feet	16 feet	1	N/A	12 feet	N/A	Yes, one side, parallel	16 feet	Tree Well, 5 X 5 feet min.
Plano Road Slip Road Type 'B' Street	60 feet	15 feet (adjacent to building)	1	N/A	16 feet One Way	N/A	Yes, one side, angled; other side parallel	15 feet sidewalk next to building and 6-foot trail along Plano Road	Tree Well, 5 X 5 feet min.
Renner Road Slip Road Type 'B' Street	60 feet	15 feet (adjacent to building)	1	N/A	16 feet One Way	N/A	Yes, one side, angled; other side parallel	15 feet sidewalk next to building and 12-foot trail along Renner Road	Tree Well, 5 X 5 feet min.
Commercial Alley	26 feet (16 feet of paving)	None	N/A	None	N/A	N/A	None	None	None

9.4 Street Cross Sections

9.4.1 TOD Main Street

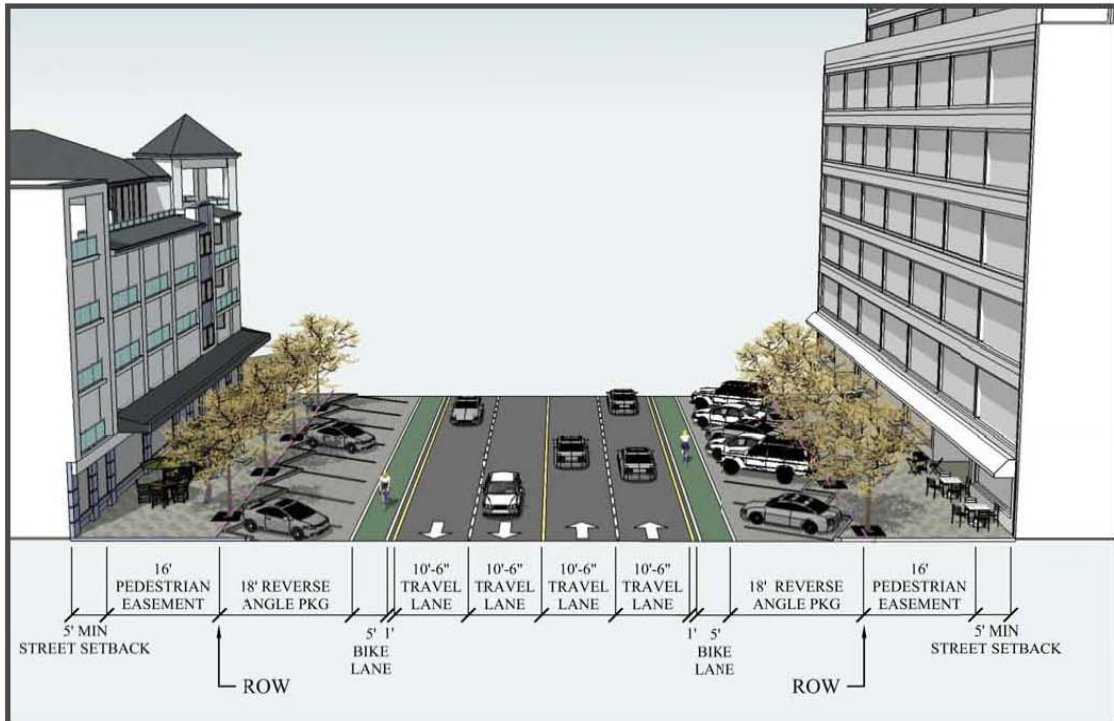
Locator Map



TOD Main Street

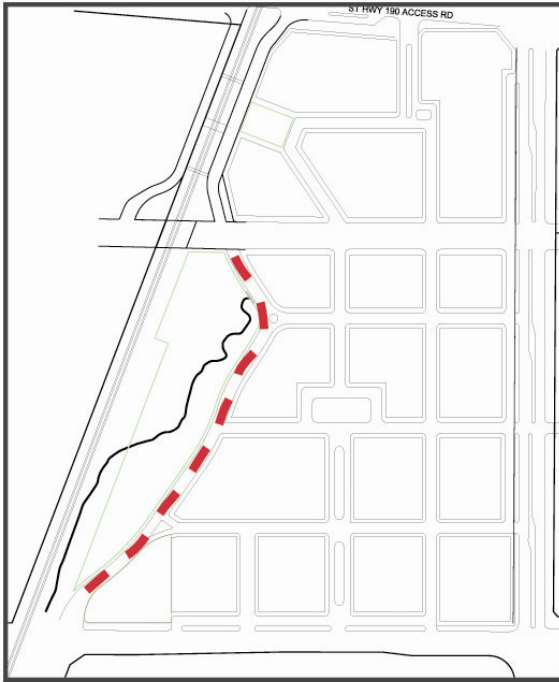
Street Type: A
ROW: 90'
Design Speed: 25 MPH
Parking: Reverse Angle on both sides
Number of Vehicular Lanes: 4
Number of Bike Lanes: 2
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.2 Parkway

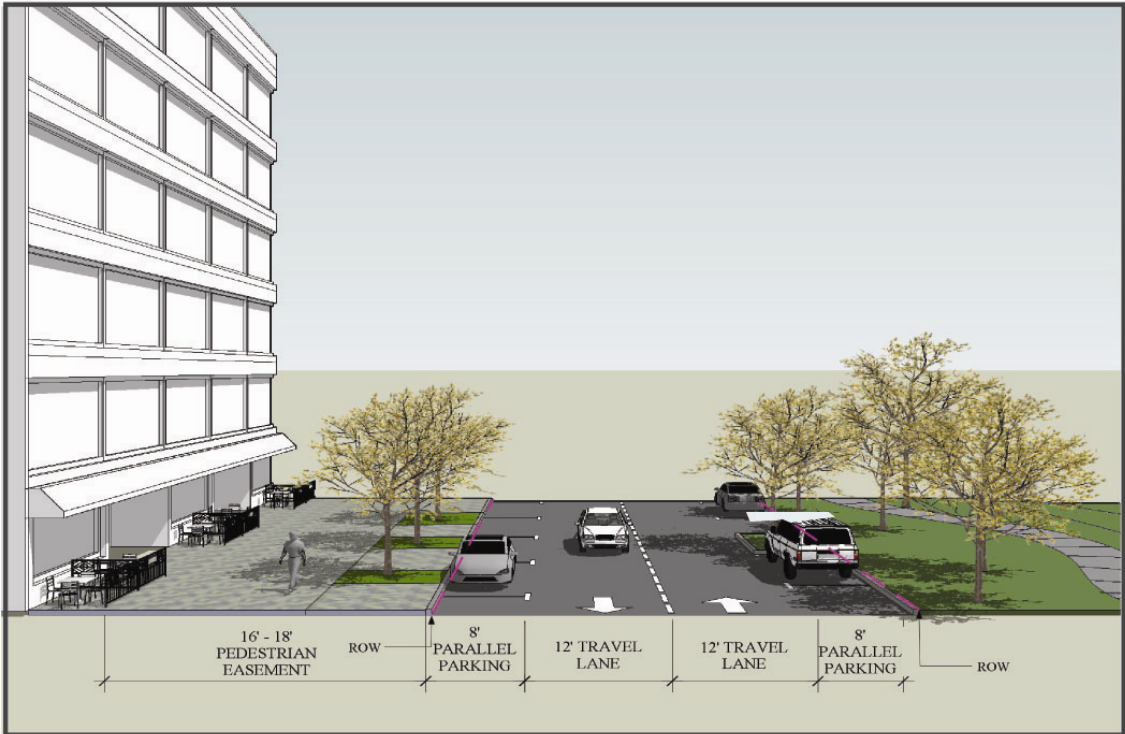
Locator Map



Parkway

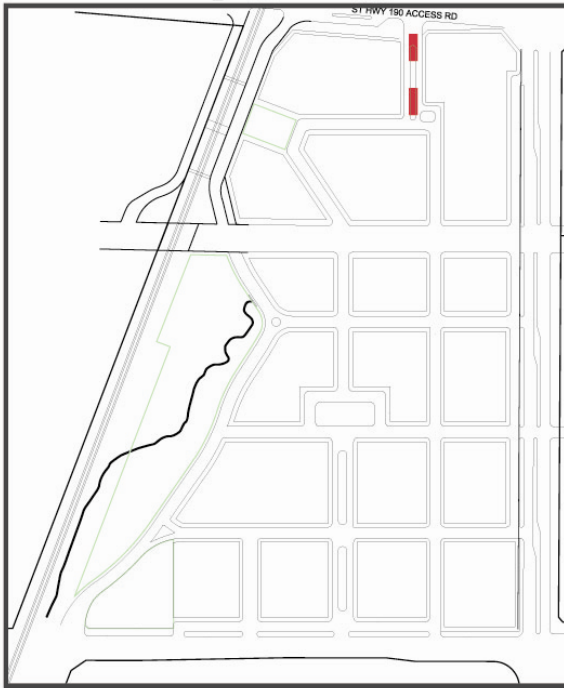
Street Type: A
ROW : 40'
Design Speed: 25 MPH
Parking: Parallel on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.3 TOD Avenue

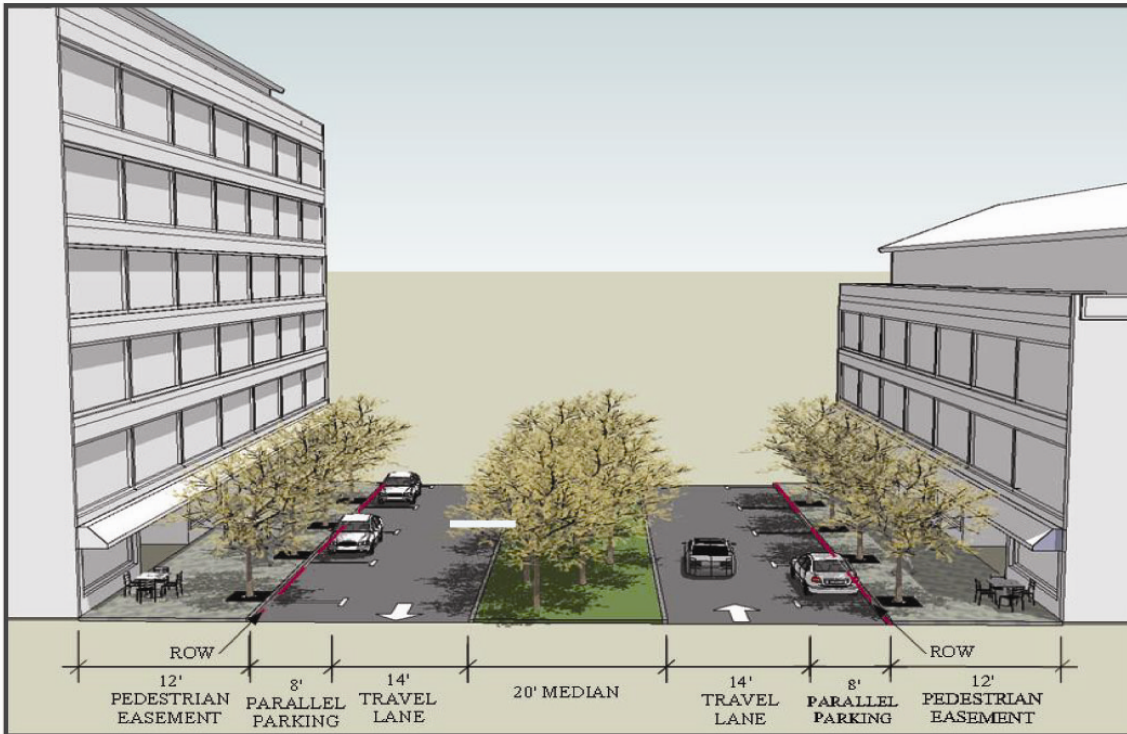
Locator Map



TOD Avenue

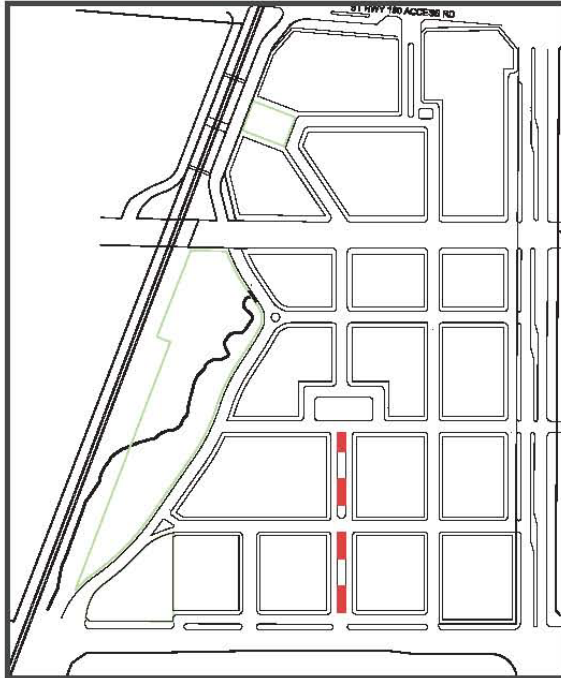
Street Type: B
ROW: 64'
Design Speed: 25 MPH
Parking: Parallel on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.4 Park Avenue

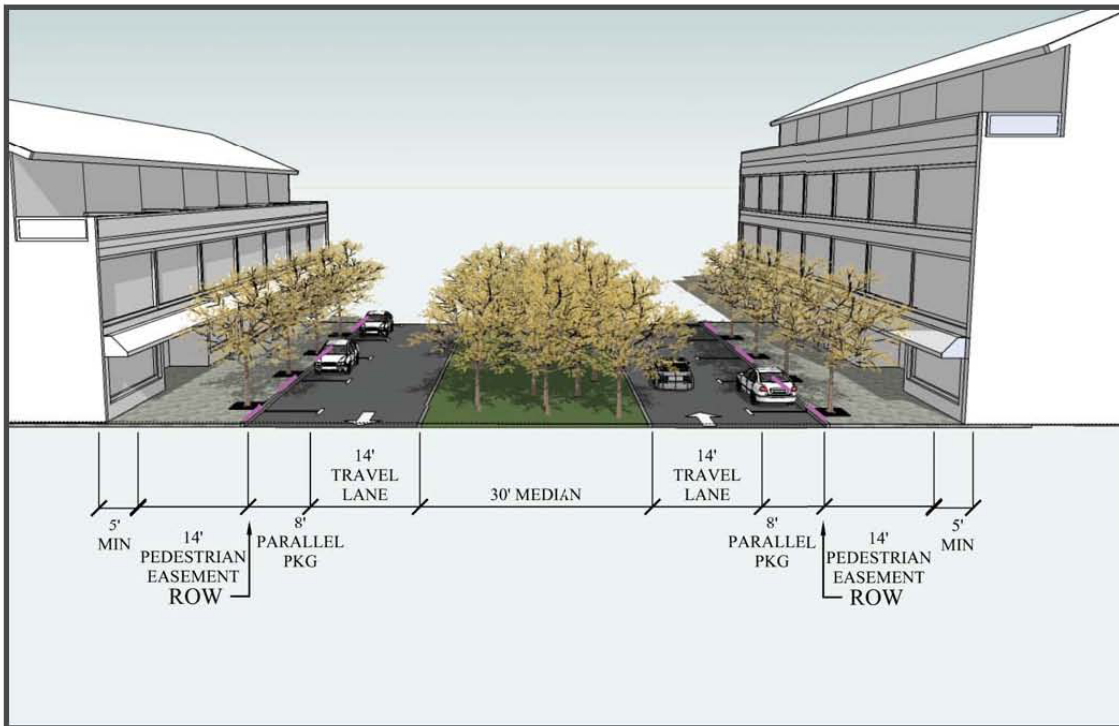
Locator Map



Park Avenue

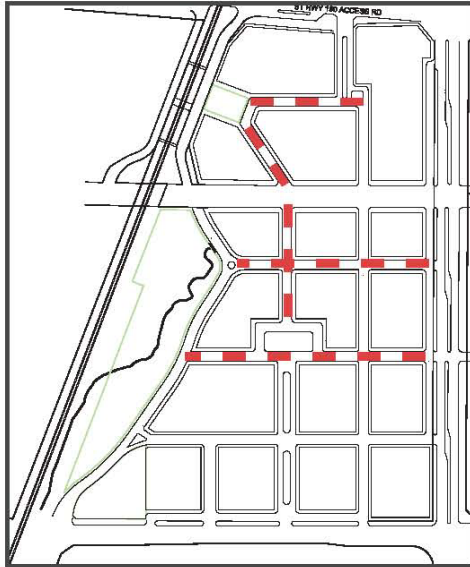
Street Type: A
ROW: 74'
Design Speed: 25 MPH
Parking: Parallel on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.5 TOD Street Type A

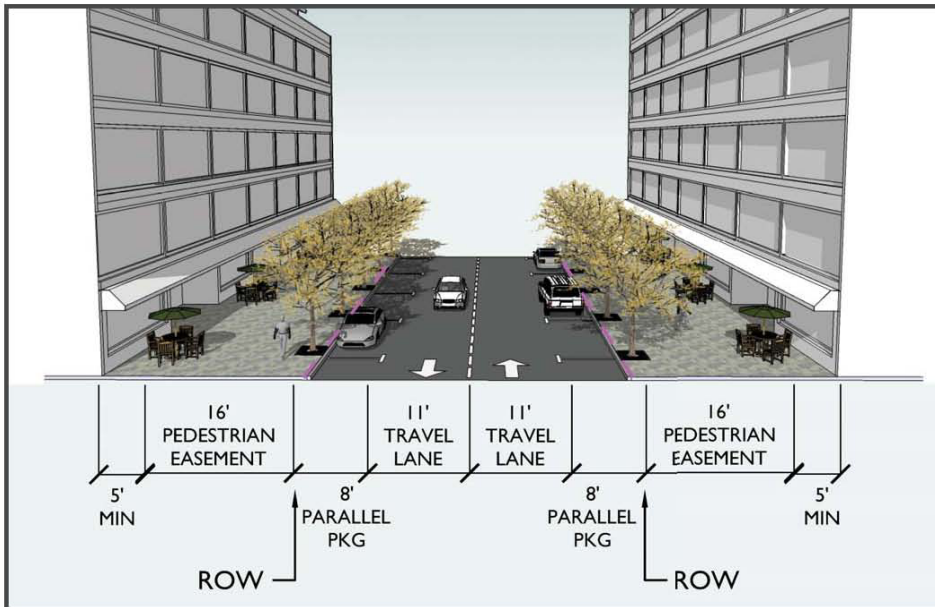
Locator Map



TOD Street Type A

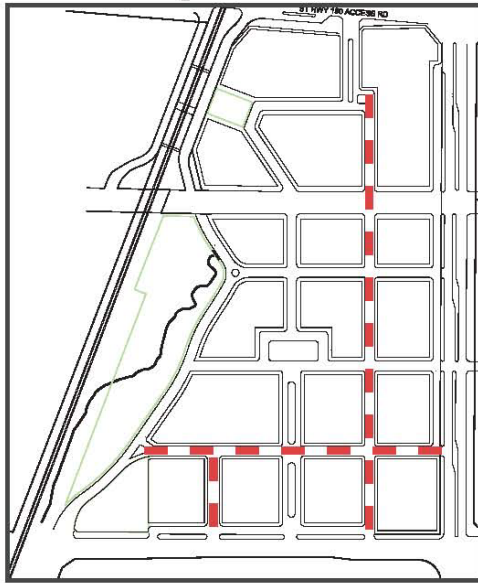
Street Type: A
ROW: 38'
Design Speed: 25 MPH
Parking: Parallel on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.6 TOD Street Type B

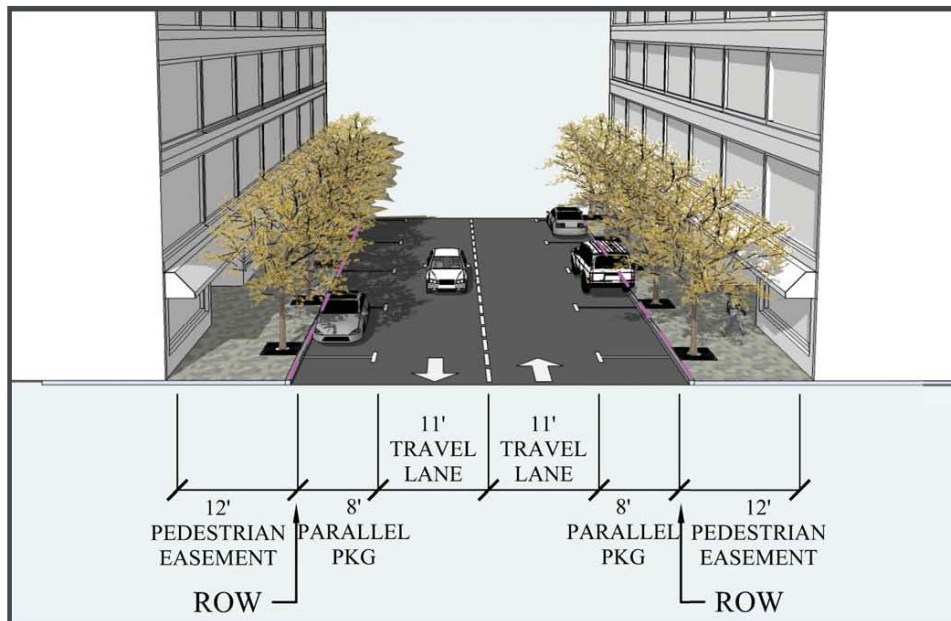
Locator Map



TOD Street Type B

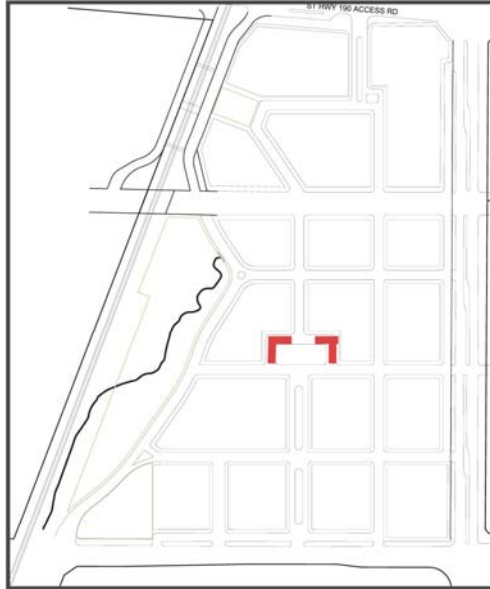
Street Type: B
ROW: 38'
Design Speed: 25 MPH
Parking: Parallel on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.7 Court Street

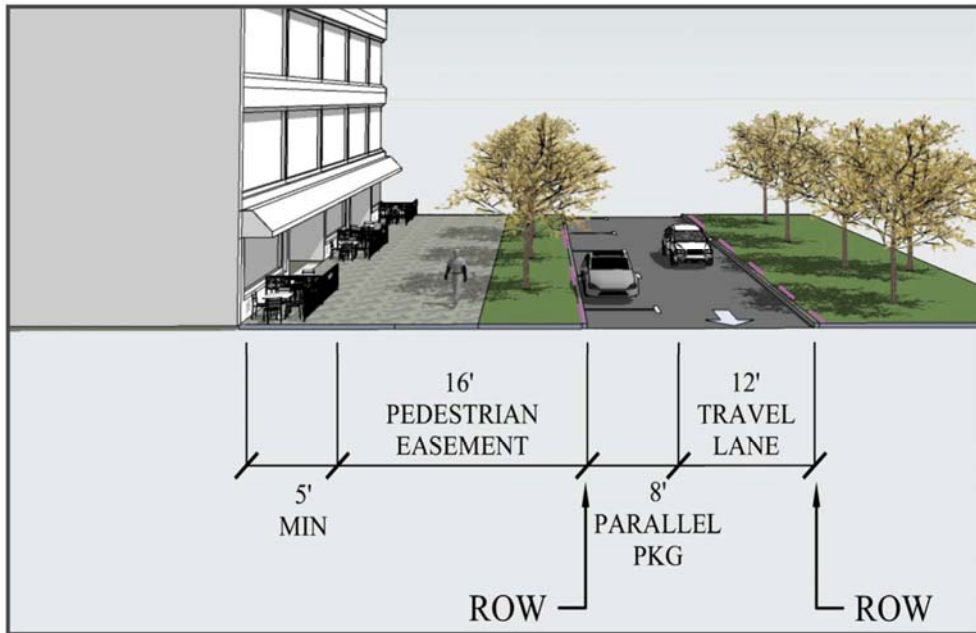
Locator Map



Court Street

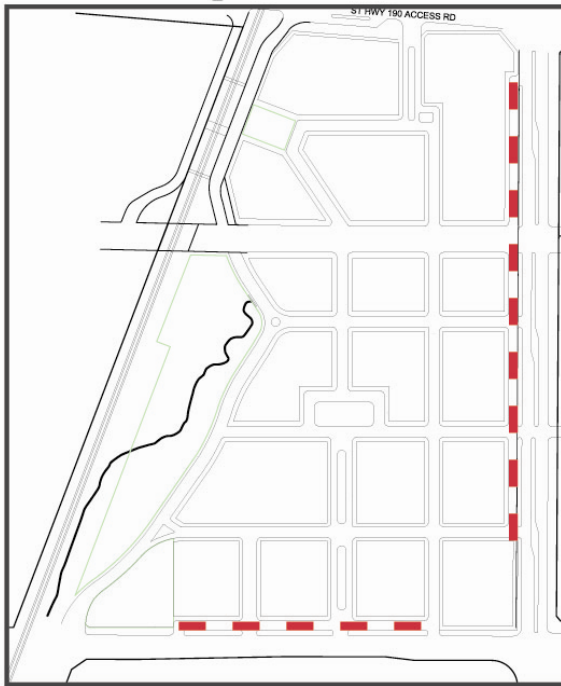
Street Type: A
ROW: 20'
Design Speed: 25 MPH
Parking: Parallel on one side
Number of Vehicular Lanes: 1
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.8 Plano/Renner Slip Road

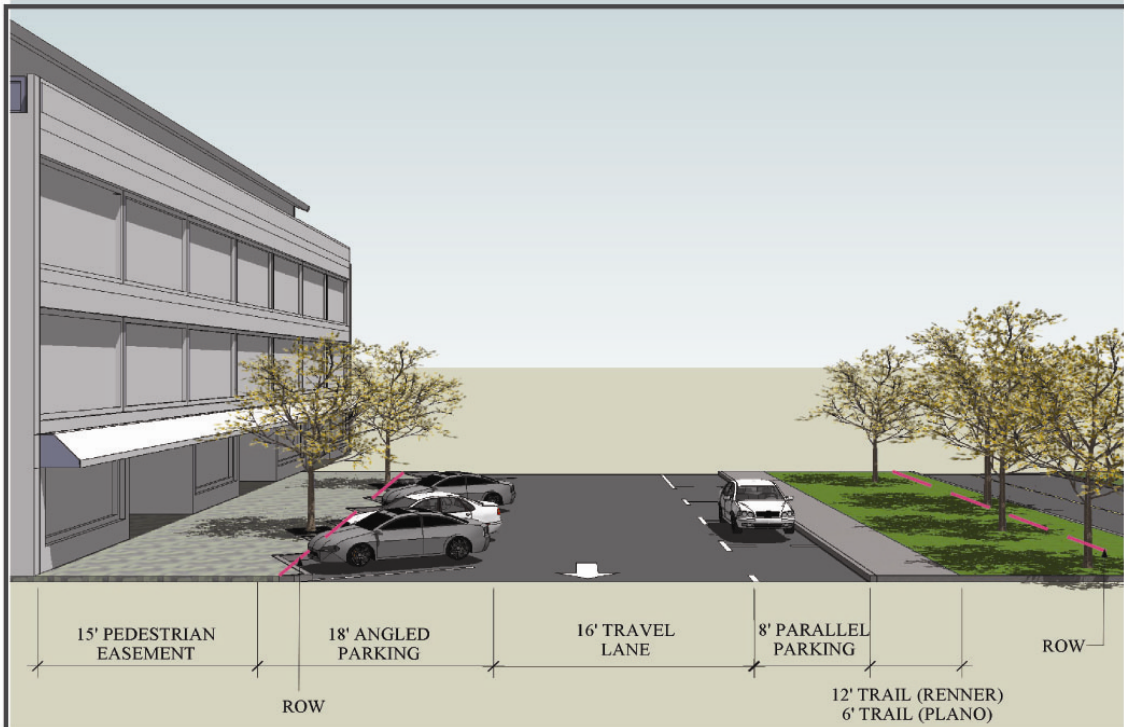
Locator Map



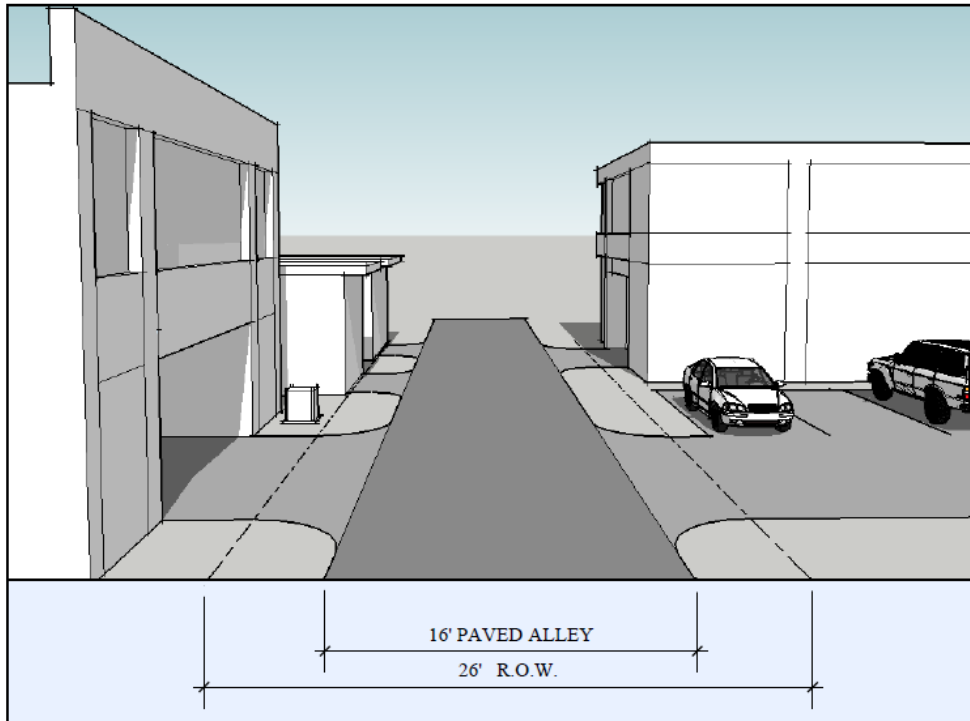
Slip Road (Plano / Renner)

Street Type: B
Slip Road Width: 60'
Design Speed: < 25 MPH
Parking: Head-in angled and parallel
Number of Vehicular Lanes: 1
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement / Trail
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.9 Commercial Alley



9.5 Streetscape & Landscape Standards: Streetscape standards shall apply to all streets within the Bush Central Station. Streetscape standards shall address all elements between the building face and edge of the curb. Typical streetscape elements addressed are street trees, lighting, street furniture and pedestrian amenities, and materials. Maintenance of all streetscape and landscape shall be by the property owners association established for the Bush Central Station. All dedicated public rights-of-way and landscaping within them shall be maintained by the City of Richardson.

9.6 Street Trees and Landscaping (within the pedestrian easement):

9.6.1 Street trees shall be required on all Bush Central Station Streets (except on alleys and PGBT access road.)

9.6.2 Street trees shall be planted approximately 3 feet behind the curb line.

9.6.3 Spacing shall be an average of 40 feet on center (measured per block face) along all streets except alleys.

9.6.4 The minimum caliper size for each tree shall be 3 in. and shall be a minimum of 12 feet in height at planting. Each tree shall be planted in a planting area no less than 36 sq. feet. However, the tree well area may be no smaller than 25 sq.ft.

9.6.5 Along the TOD Avenue and Park Avenue trees shall be required in the median and spacing and species shall be the same as the trees in the parkway.

9.6.6 Turf and groundcover: When clearly visible from the street and alleys, all unpaved ground areas shall be planted with low growing shrubs or ground cover, ornamental grasses, or a combination thereof. Turf grass must be installed as solid sod and not seeded on.

9.6.7 Species shall be selected from the Bush Central Station Planting List in Appendix B of this ordinance.

9.6.8 Maintenance of all landscape materials shall meet the requirements of the City of Richardson Landscape Ordinance Requirements.

9.6.9 Along arterials and highway access roads, street trees shall be planted within the required landscape parkway as per the City of Richardson Landscape Ordinance/Policies.

9.7 Street Furniture, Lighting, and Materials:

9.7.1 Pedestrian scale lighting shall be required along all Bush Central Station streets (except on alleys and PGBT access road). They shall be no taller than 20 feet.

9.7.2 Street lights shall be placed at 50 feet on center, approximately 3 feet behind the curb line.

9.7.3 The light standard selected shall be compatible with the design of the street and buildings.

9.7.4 Trash receptacles and bike racks shall be required along all Type 'A' Streets. A minimum of one each per block face shall be required.

9.7.5 Street furniture and pedestrian amenities such as benches are recommended along all Type 'A' Streets.

9.7.6 All street furniture shall be located in such a manner as to allow a clear sidewalk passageway of a minimum of 6 feet.

9.7.7 Materials selected for paving and street furniture shall be of durable quality and require minimal maintenance.

Section 10. Signage

Except as specifically listed below, all other signage and sign standards must comply with Chapter 18 of the City of Richardson Code of Ordinances, as amended.

10.1 For new signs, the standards in Table 10.1 shall apply and sign permits shall be approved administratively by the City of Richardson Building Official unless specifically noted in this section.

Table 10.1

Character Zone	TOD Core & Special Destination	TOD MU	Arterial MU	Highway MU	Standard
Sign Type					
Wall (Building) Signs	P	P	P	P	<ul style="list-style-type: none"> For all ground floor commercial uses (retail, office, and restaurant): One sign per tenant space; area to be calculated at 1.5 sq. feet per linear foot of public street frontage for the tenant space with a maximum of 100 sq. ft per tenant. Second and upper floor commercial uses may also be permitted one second floor wall sign per tenant space per public street frontage; area to be calculated at 1.5 sq. feet per linear foot of second or upper floor frontage along that public street with a maximum of 125 sq. feet. Institutional uses (non-profits and churches): One sign per tenant space; area to be calculated at 1.5 sq. feet per linear foot of public street frontage with a maximum of 100 sq. feet. Live-Work and Home occupations: One sign limited to an area of 20 sq. feet max. Building sign may encroach a maximum of 12" on to a sidewalk while maintaining a vertical clearance of 8 feet from the finished sidewalk. Building signs may be internally or externally lit. Marquee signs as only permitted as specified below.
Monument Signs	NP	NP	P	P	<ul style="list-style-type: none"> One monument sign per lot per lot street frontage (no more than 2 per lot separated by at least 100 feet) limited to a maximum of 50 sq. feet per sign face and 6 feet in height. Permitted only along PGBT access road, Type 'B' Streets and Slip Road Frontages.
Window Signs	P	P	P	P	<ul style="list-style-type: none"> Limited to 10% of the window area. <p>The following shall be exempt from this limitation:</p> <ul style="list-style-type: none"> Addresses, closed/open signs, hours of operation, credit card logos, real estate signs, and now hiring signs; Mannequins and storefront displays of merchandise sold; and Interior directory signage identifying shopping aisles and merchandise display areas.
Building Blade Signs	P	P	P	P	<ul style="list-style-type: none"> One per building (commercial and mixed use buildings only). Area = 30 sq. feet maximum per sign face. May encroach a maximum of 6 feet over a sidewalk, but shall not encroach over any parking or travel lane. Building blade signs may be attached to the building at the corners of building or along any street facing façade above the first floor facade.
Tenant Blade Signs	P	P	P	P	<ul style="list-style-type: none"> One per commercial tenant space (retail, office, or restaurant use). Area = 16 sq. feet maximum per sign face. May encroach a maximum of 4 feet over a public sidewalk, but shall not encroach over any parking or travel lane. Tenant blade signs shall be oriented perpendicular to the building façade and hung under the soffit of an arcade or under a canopy/awning or attached to the building façade immediately over the ground floor tenant space while maintaining a vertical clearance of 8 feet from the finished sidewalk.
Marquee Signs	P	P	P	P	<ul style="list-style-type: none"> Permitted for theatres, auditoriums, and other public gathering venues of 100 persons or more. Marquee signs shall be attached to the building or located above or below a canopy only. Area = 100 sq. feet maximum. Message board may be changeable copy (electronic and non-electronic). Electronic message boards shall be non-flashing.
For sale/for lease signs	P	P	P	P	<ul style="list-style-type: none"> Size is limited to 32 sq. feet per sign face. All other standards are the same as City or Richardson Sign Regulations.
Address signs	P	P	P	P	Same as City of Richardson Sign Regulations.
Temporary construction signs	P	P	P	P	1 free standing sign per lot during construction only; limited to 32 sq. feet.
Banners	P	P	P	P	Same as City of Richardson Sign Regulations.

Character Zone	TOD Core & Special Destination	TOD MU	Arterial MU	Highway MU	Standard
Sign Type					
Sandwich board signs	P	P	P	NP	<ul style="list-style-type: none"> Permitted only for retail, service, or restaurant uses. Limited to 12 sq. feet per sign face per storefront. Sign may not exceed 4 feet in height. A minimum of 6 feet of sidewalk shall remain clear. Chalkboards may be used for daily changing of messages. Readerboards (electronic and non-electronic) shall be prohibited. Sign shall be removed every day after the business is closed.
Light Pole Banners	P	P	P	P	<ul style="list-style-type: none"> Permitted only with approval of the City. Max. 10 sq. feet per sign face. Limited to one per light pole. All light pole banners shall be approved by the appropriate utility company prior to consideration by the City. Light pole banners shall be limited to publicize community-wide events, holiday celebrations, public art, and other city sponsored events.
Directory signs	P	P	P	P	<ul style="list-style-type: none"> Shall be allowed for all multi-tenant commercial and mixed use buildings only. One directory sign per multi-tenant building limited to 12 sq. feet in area. Design of the sign shall be integral to the façade on which the sign is to be affixed.
Pole signs	NP	NP	NP	NP	<ul style="list-style-type: none"> May only be permitted as part of a Master Sign Plan.
LED Signs	P	P	P	P	<ul style="list-style-type: none"> Shall be covered by a lens or diffuser. May only be permitted where electronic message boards are permitted within Bush Central Station. Or, may only be permitted as part of a Master Sign Plan.
Off-premise signs	NP	NP	NP	NP	

10.2 An applicant has the option to establish unique sign standards including size, color, type, design, and location. Such applications shall be reviewed as “Master Sign Plans” by the City of Richardson Building Official and are subject to approval of the Sign Control Board. In evaluating a Master Sign Plan, the Sign Control Board shall consider the extent to which the application meets the proposed Sign Plan:

- 10.2.1 Promotes consistency among signs within a development thus creating visual harmony between signs, buildings, and other components of the property;
- 10.2.2 Enhances the compatibility of signs with the architectural and site design features within a development;
- 10.2.3 Encourages signage that is in character with planned and existing uses thus creating a unique sense of place; and
- 10.2.4 Encourages multi-tenant commercial uses to develop a unique set of sign regulations in conjunction with development standards.

Section 11. Civic/Open Space Standards

11.1 The design of Civic/Open Space shall be regulated by the Civic/Open Space standards herein which shall create a network of open spaces that recognizes the natural qualities of the area while providing a range of both passive and active recreational opportunities. These opportunities may be accommodated in a variety of spaces ranging from large regional parks to neighborhood-scaled greens to urban squares and plazas. The open space network will be serviced by an interconnected network of trails and paths for pedestrians and bicyclists alike. Standards in Article 13-A, Section 5 of the City of Richardson Zoning Ordinance shall not apply to any development in the Bush Central Station.

11.2 Park Standards



The required park, as designated on the Regulating Plan, will create an important public space that connects the community within the Bush Central Station and allows for passive recreation. Parks shall primarily be naturally landscaped with many places to sit on benches or low walls. Passive recreation activities in parks may include grassy lawns for unstructured and informal active recreational activities. Appropriate civic elements, fountains or open shelters may be included. TOD Core and TOD Mixed Use buildings shall front onto and activate this space.

Typical Characteristics

General Character

- Large, open space
- Spatially defined by landscaping and building frontages
- Paths, trails, open shelters, lawns, trees and shrubs naturally disposed
- May be lineal, following the trajectories of natural corridors

Location and Size

Location and size shall be as shown on the Regulating Plan (Appendix A).

Typical Uses

- Unstructured and passive recreation
- Casual seating/picnicking

11.3 Green Standards



The required green, as designated on the Regulating Plan, will serve as an important public space for the Bush Central Station. The green will be available for civic purposes, commercial activity, unstructured recreation and other passive uses. Greens shall primarily be naturally landscaped with many shaded places to sit. Appropriate paths, civic elements, fountains or open shelters may be included and shall be formally placed within the green. TOD Mixed Use buildings, Arterial Mixed Use buildings or Neighborhood Frontage buildings shall front onto and activate this space.

Typical Characteristics

General Character

- Open space
- Spatially defined by landscaping and building frontages
- Lawns, trees and shrubs naturally disposed
- Open shelters and paths formally disposed

Location and Size

Location and size shall be as shown on the Regulating Plan (Appendix A) and shall range between 0.38 – 0.75 acres.

Typical Uses

- Unstructured and passive recreation
- Casual seating
- Commercial and civic uses
- Residential address

11.4 Square Standards



The required square provides an important terminated vista and sight line from the station platform. The square will serve as an open space available for civic purposes, commercial activity, unstructured recreation and other passive uses. The square should have a more urban, formal character and be defined by the surrounding building frontages and adjacent tree-lined streets. All buildings adjacent to the square shall front onto the square. Adjacent streets shall be lined with appropriately scaled trees that help to define the square. The landscape shall consist of lawns, trees, and shrubs planted in formal patterns and furnished with paths and benches. Shaded areas for seating should be provided. A civic element or small structure such as an open shelter, pergola, or fountain may be provided within the square.

Typical Characteristics

General Character

- Formal open space
- Spatially defined by buildings and tree-lined streets.
- Open shelters, paths, lawns, and trees formally arranged
- Walkways and plantings at all edges
- Located at important intersection

Location and Size

Location and size shall be as shown on the Regulating Plan (Appendix A).

Typical Uses

- Unstructured and passive recreation
- Casual seating
- Commercial and civic uses

11.5 Plaza Standards



Plazas add to the vibrancy of streets within the more urban zones and create formal open spaces available for civic purposes and commercial activity. Building frontages shall define these spaces. The landscape should consist primarily of hardscape. If trees are included, they should be formally arranged and of appropriate scale. Casual seating, along with tables and chairs, should be provided. Plazas typically should be located at the intersection of important streets.

Typical Characteristics

General Character

- Formal open space
- Primarily hardscape surfaces
- Trees and shrubs optional
- Spatially defined by building frontages

Location and Size

- Location and size shall be as shown on the Regulating Plan (Appendix A).
- Shall front on at least one (1) street.

Typical Uses

- Commercial and civic uses
- Casual seating
- Tables and chairs for outdoor dining
- Retail and food kiosks

11.6 Pedestrian Passage Standards



Pedestrian passages create intimate passageways through buildings at designated locations. These paths provide direct pedestrian access to residential addresses and create unique spaces for frontages to engage and enter off of. Pedestrian passages allow for social and commercial activity to spill into the public realm. Pedestrian passages should consist of a hardscape pathway activated by frequent entries and exterior stairways. The edges may simply be landscaped with minimal planting and potted plants.

Typical Characteristics

General Character

- Hardscape pathway
- Frequent entries and frontages
- Exterior stairways
- Defined by building frontages
- Minimal planting and potted plants
- Maintain the character of surrounding buildings

Standards

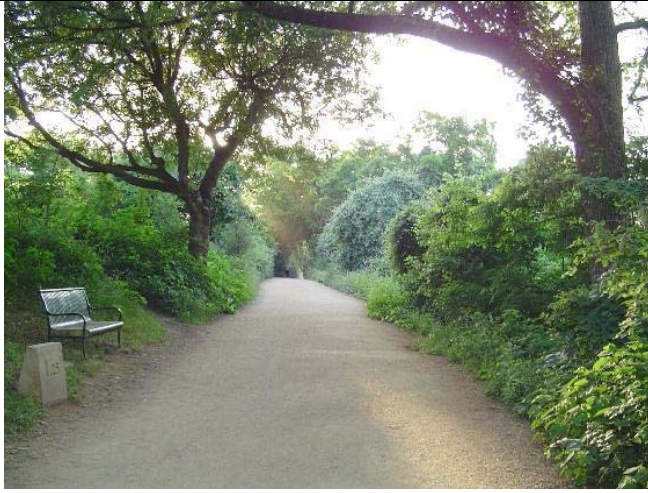
Min. Width 12 feet

Location shown on the Regulating Plan (Appendix A).

Typical Uses

- Pedestrian connection and access
- Casual seating

11.7 Multi-Use Trail Standards



The multi-use trail provides an important place for active recreation and creates a connection to regional paths and biking trails. The multi-use trail will run continuously from the station platform, through the required park, and along Renner Road. The multi-use trail will help activate connections between the open spaces and the uses throughout the Bush Central Station. The multi-use trail may have a different character as it passes through the park and as it is adjacent to Renner Road. Within the required park, the trail shall be naturally disposed with low impact paving materials so there is minimal impact to the existing creek bed and landscape. Along Renner Road, the trail shall have a more formal disposition with a paved trail and regular landscaping.

Typical Characteristics

General Character

Park Multi-Use Trail:

Naturally disposed landscape

Low impact paving

Trees lining trail for shade

Appropriately lit for safety

Renner Road Multi-Use Trail:

Hardscape Path

Formally disposed pedestrian furniture, landscaping and lighting

Trees lining trail for shade

Standards

Min. Width 12 feet

Location shown on the Regulating Plan (Appendix A).

Typical Uses

Active and passive recreation

Casual seating

11.8 Playground Standards



Playgrounds shall be permitted in parks and greens to provide open space designed and equipped for the recreation of children. These playgrounds should serve as quiet, safe places -- protected from the street and typically located where children do not have to cross major to access. Playgrounds may be fenced. An open shelter, play structures or interactive art and fountains may be included with landscaping between. Shaded areas and seating shall be provided. Playground equipment and design must be reviewed and approved by the City prior to installation.

A larger playground may be incorporated into the park, whereas a more intimate playground may be incorporated into the green.

Typical Characteristics

General Character

- Focused toward children of all ages
- Fenced with minimal exits (non mandatory)
- Open shelter
- Shade and seating provided
- Play structure, interactive art or fountains

Standards

- Min. Size N/A
- Max. Size N/A
- As described by civic space type in which playground is located
- Protected from traffic
- No service or mechanical equipment

Typical Uses

- Active and passive recreation
- Unstructured recreation
- Casual seating

11.9 Ancillary Structure Standards



Ancillary structures should be formal in character and generally related to but clearly subordinate to surrounding buildings. Each individual structure should keep in character with the style of nearby buildings. Typically, these structures are located at prominent locations within an appropriate civic space. Ancillary structures located in more urban zones may have minor commercial uses, such as small food or news vendors, but may also serve as civic elements for general public use with more passive activities.

Typical Characteristics

General Character

- Formal character
- Relating to style of surrounding buildings
- One or more open sides
- Covered or providing shade
- Small, stand alone structure
- Located within Park, Green, Square or Plaza

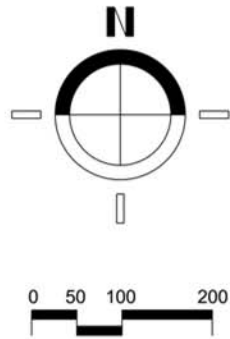
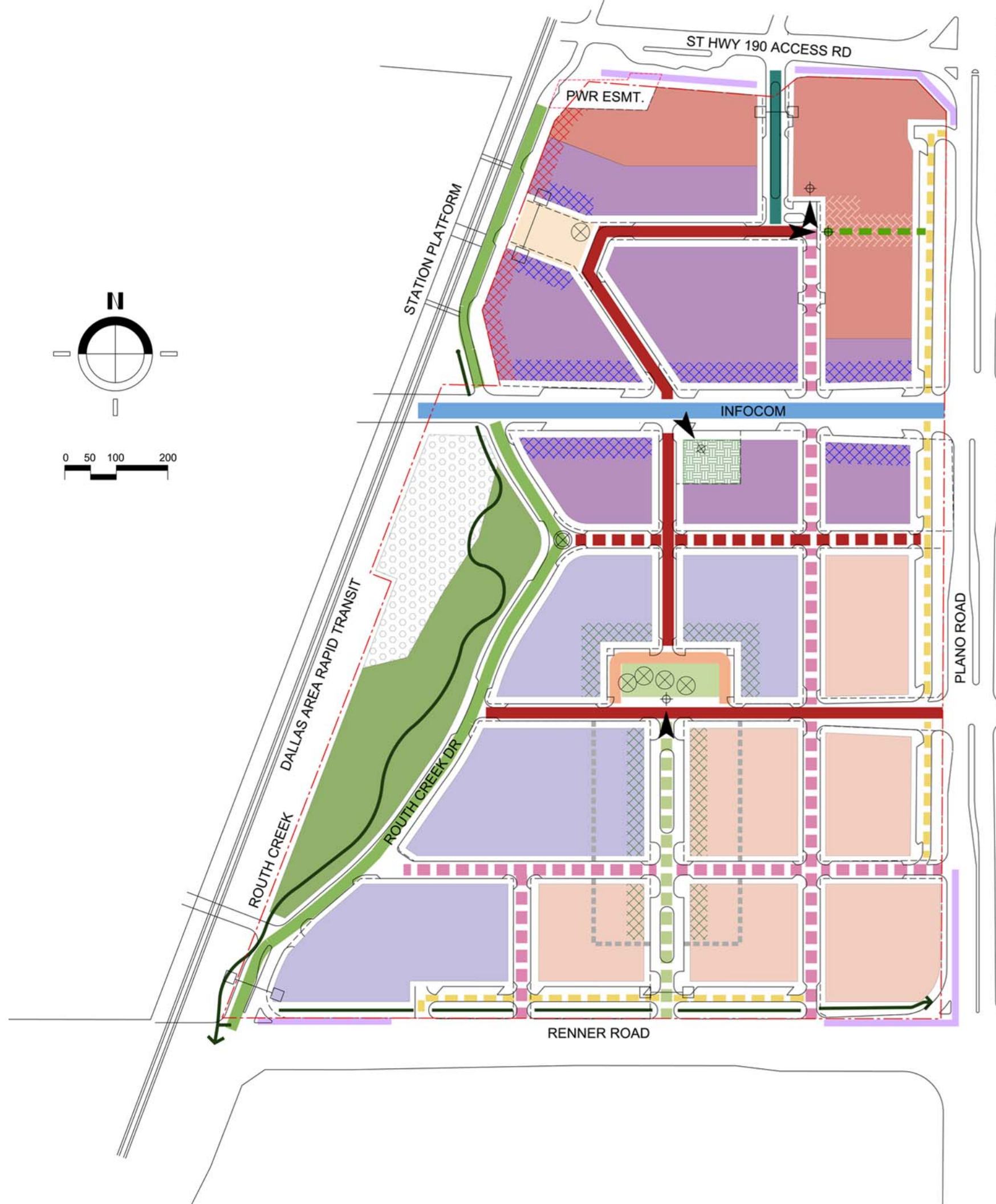
Standards

- Min. Size N/A
- Max. Size N/A

Typical Uses

- Civic purposes
- Minor commercial uses
- Casual seating/picnicking

APPENDIX A



LEGEND

CHARACTER ZONES	
Character Zone	Approx. Area (acres)
TOD Core	8.22
TOD Mixed Use	8.29
Highway Mixed Use	4.34
Arterial Mixed Use	7.76

OPEN SPACE TYPES	
Mandatory Park	3.65
Mandatory Green	0.38
Mandatory Square	0.28
Mandatory Plaza	0.39
Non-mandatory Plaza	0.39
Non-mandatory Streets	5.1

FRONTAGE TYPES	
Mandatory Main Street Frontage	
Mandatory Station Platform Frontage	
Non-mandatory Neighborhood Frontage	

STREET DESIGNATIONS BY STREET TYPE	
TYPE 'A' STREETS	
TOD Main Street	
TOD Street type 'A'	
Parkway	
Park Avenue	
Court Street	
TYPE 'B' STREETS	
TOD Street Type 'B'	
TOD Avenue	
Slip Lane Frontage	
'B' Street Frontage	
Solid line denotes mandatory street	
Dashed line denotes non-mandatory street	
OTHER	
Approximate District Boundary	
Non-mandatory Alley	
Non-mandatory Pedestrian Passage	
Mult-Use Trail	
Existing tree to remain - non mandatory	
Non-mandatory Gateway Element	
Non-mandatory Vista Terminus	
Special Destination	

REGULATING PLAN

Bush Central Station Planned Development Code

February 14, 2011

**Appendix B
Planting List**

The following lists contain all species approved for use in the Bush Central Station. It contains native and acceptable adapted species. Other species that are drought tolerant and adaptive may be used for planting within the Bush Central Station. The use of alternative species may be permitted with the approval of the City.

CANOPY/STREET TREE LIST

<u>Common Name</u>	<u>Botanical Name</u>
Live Oak	<i>Quercus virginiana</i>
Red Oak	<i>Quercus shumardi</i>
Bald Cypress	<i>Taxodium distichum</i>
Sweetgum	<i>Liquidambar styraciflua</i>
Cedar Elm	<i>Ulmus crassifolia</i>
Lacebark Elm	<i>Ulmus parvifolia</i>
Bigtooth Maple	<i>Acer grandidentatum</i>
Caddo Maple	<i>Acer saccharum 'Caddo'</i>
Texas Ash	<i>Fraxinus velutina 'Rio Grande'</i>
Bur Oak	<i>Quercus macrocarpa</i>
Chinquapin Oak	<i>Quercus muhlenbergii</i>
Escarpment Live Oak	<i>Quercus fusiformis</i>
Ginkgo	<i>Ginkgo biloba</i>

ORNAMENTAL TREE LIST

<u>Common Name</u>	<u>Botanical Name</u>
Yaupon Holly	<i>Ilex vomatoria</i>
Crape Myrtle	<i>Lagerstromia indica</i>
Deciduous Yaupon	<i>Ilex decidua</i>
Southern Crabapple	<i>Malus app.</i>
Chinese Pistache	<i>Pistacia chinensis</i>
Mexican Plum	<i>Prunus Mexicana</i>
Wax Myrtle	<i>Myrica carifera</i>
Chitalpa	<i>Chitalpa tashkentensis</i>
Deciduous Holly	<i>Ilex decidua</i>
Desert Willow	<i>Chilopsis linearis</i>
Eve's Necklace	<i>Sophora affinis</i>

SHRUBS LIST

<u>Common Name</u>	<u>Botanical Name</u>
Dwarf Nandina	<i>Nandina domestica 'nana'</i>
Dwarf Burford Holly	<i>Ilex cornuta 'burfordi nana'</i>
Abelia Grandiflora	<i>Abelia grandiflora</i>
Barberry	<i>Barberry spp.</i>
Yucca (Red, Yellow or Soft Tip)	<i>Hesperaloe parviflora</i>
Texas Sage	<i>Leucophyllum frutescans</i>
Indian Hawthorn	<i>Raphiolepis indica</i>
Dwarf Crape Myrtle	<i>Lagerstromia indica 'nana'</i>
Dwarf Yaupon Holly	<i>Ilex vomitorria 'nana'</i>

Pampas Grass	<i>Cortaderia selloana</i>
Black-Eyed Susan	<i>Rudbeckia hirta</i>
Dwarf Wax Myrtle	<i>Myrica pusilla</i>
Needlepoint Holly	<i>Ilex cornuta 'Needle Point'</i>
Knockout Rose	<i>Rosa 'Knock Out'</i>
Rosemary	<i>Rosmarinus officinalis</i>

GROUND COVER/VINES LIST

<u>Common Name</u>	<u>Botanical Name</u>
Asian Jasmine	<i>Trachelosperum Asiaticum</i>
Big Blue Liriope	<i>Liriope muscari</i>
Mondograss	<i>Ophiopogon japonicus</i>
Purple Winter Creeper	<i>Euonymum coloratus</i>
Santolina	<i>Santolina virens</i>
Trumpet Vine	<i>Campsis radicans</i>
Virginia Creeper	<i>Parthenocissus quinquefolia</i>
Lady Banks Rose	<i>Rosa banksiaw lutea</i>
Confederate Jasmine	<i>Trachelospermum jasminoides</i>
Crossvine	<i>Bignonia capreolata</i>
Evergreen Wisteria	<i>Millettia reticulata</i>
Lantana 'New Gold'	<i>Lantana camara 'New Gold'</i>
Liriope 'Silver Dragon'	<i>Liriope muscari 'Silver Dragon'</i>
Prostrate Rosemary	<i>Rosmarinus officinalis prostrata</i>
Sweet Autumn Clematis	<i>Clematis terniflora</i>

ORNAMENTAL GRASSES LIST

<u>Common Name</u>	<u>Botanical Name</u>
Dwarf Fountain Grass 'Little Bunny'	<i>Pennisetum alopecuroides 'Little Bunny'</i>
Dwarf Maiden Grass	<i>Miscanthus sinensis 'Adagio'</i>
Fountain Grass	<i>Pennisetum alopecuroides</i>
Inland Sea oats	<i>Chasmanthium latifolium</i>
Maiden Grass	<i>Miscanthus sinensis 'Gracillimus'</i>
Mexican Feather Grass	<i>Stipa tenuissima</i>
Muhly Grass	<i>Muhlenbergia capillaris</i>
Weeping Lovegrass	<i>Eragrostis curvula</i>

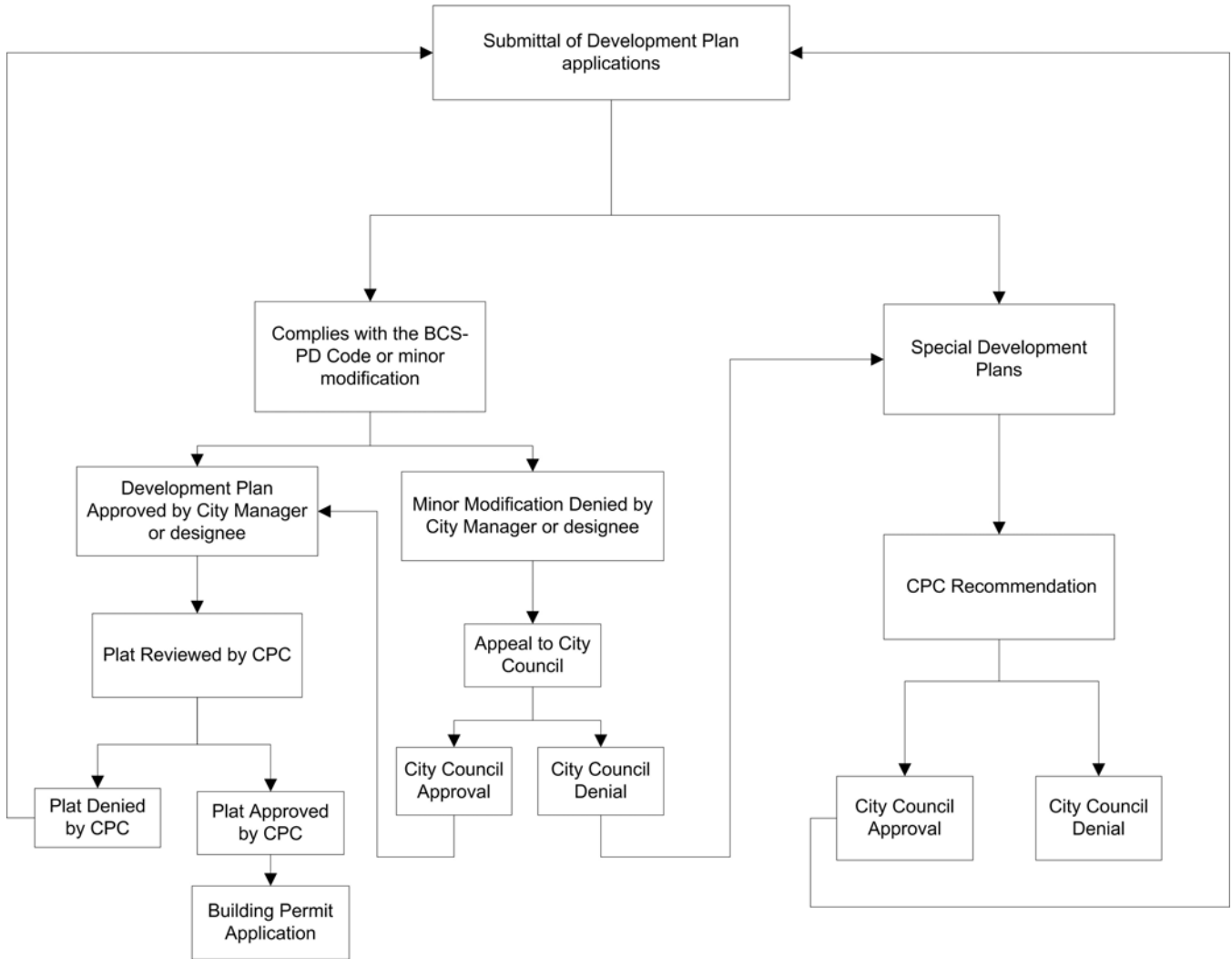
TURF

<u>Common Name</u>	<u>Botanical Name</u>
Bermuda	<i>Cynodon dactylon</i>
St. Augustine	<i>Stenotaphrum secundatum</i>
Zoysia	<i>Zoysia tenuifolia</i>

These plantings may be placed in Civic/Open Spaces or used to meet the private landscaping requirements of the Code. The applicant shall select drought tolerant, low maintenance, and adaptable shrubs and ground cover based on the placement on the site subject to approval by the City.

Appendix C

**Bush Central Station
Development Review Process**



BCS-PD: Bush Central Station Planned Development
 CPC: City Plan Commission
 CC: City Council
 SDP: Special Development Plan

Note: Development Plans include site plans, building elevations, landscape plans, civil engineering plans, and all plats